

# GRAIN DEALERS JOURNAL

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& Co. PROVISION  
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Established 1877  
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## Two Things—

—are of paramount importance in the grain business. *First*, you must choose a logical market wherein to buy and sell, and *second*, you must deal with reliable grain men.

You are always seeking those two important factors, yet you can always find them at Little Rock where the reputable grain men have formed the Little Rock Grain Exchange, and are seeking to render an ever increasing service to the grain trade of the entire country. Give any of the members mentioned here a trial and you will be more than satisfied.

# LITTLE ROCK

## ARKANSAS

These Members of the Grain Exchange at Little Rock will see to it that you get satisfaction in your grain transactions:

Cunningham Commission Co.,  
Grain, corn products

Caple & Stockton,  
Hay, grain, feed

Cochran Co., H. K.,  
Grain dealers

Daniel Mill & Elevator Co., Joe,  
Grain, hay, feed

Darragh Company,  
Hay, grain, mixed feeds

Davis, S. P.,  
Dealer, consignments

Farmer Co., E. L.,  
Brokers, hay, grain, mill feed

Gordy Co., C. L.,  
Grain brokers, hay, grain, mill feed

Hayes Grain & Commission Co.,  
Dealers in grain, hay

Munn Brokerage Co.,  
Grain, hay, mill feed

Niemeyer Grain Co., George,  
Grain, hay, mill feed

Weinmann Milling Co.,  
Grain, hay, mixed feeds

Wilson Co., John R.,  
Grain brokers

Wilson & Co.,  
Hay, grain, feed brokers



## Directory of the Grain Trade

In Organized Markets Only Members of the Local Grain Exchange Will Be Listed

*HAVING YOUR name in this directory will introduce you to many old and new firms during the year, whom you do not know or could not meet in any other way. Many new concerns are looking for connections, seeking an outlet or an inlet, possibly in your territory. It is certain that they turn to this recognized Directory, and act upon the suggestions it gives them.*

### ANNISTON, ALA.

Woodruff Feed Co., feed manufacturers.

### ATCHISON, KANS.

Blair Elevator Co., receivers and shippers.\*

### ATHENS, GA.

Eppes, Wm. E., grain, hay, flour, produce.  
Callaway Grocery Co., The, whole. gro., grain, feeds.  
Hinton & Co., O. R., mdse. and grain brokers.  
O'Farrell Brokerage Co., flour, grain, hay, mxd. fds.\*  
Pitner & Beusse, grain and merchandise brokers.\*  
Steedman, Wm. B., grain, feed broker.\*  
Webb Brokerage Co., grain, flour, feed specialty.  
Wier Feed & Grain Co., whole. grain, feed, flour.

### ATLANTA, GA.

Commercial Exchange Members.  
Atlanta Milling Co., buyers of soft mlg. wheat.\*  
Brooke & Co., T. H., grain, hay, flour, c/s pdts.  
Fain Grain Co., W. L., flour, feed, field seeds.\*  
Gregg & Son, J. T., feedstuffs, mdse. brokers.  
Leonard & Sons, J. T., feedstuffs, mdse. brokers.  
Martin, Theo. W., broker, grain, flour, hay, mxd. feed.  
Smith, Edward B., broker, grain, hay, flour, mxd. feed.  
Taylor Commission Co., c/s meal, c/s hulls, fertilizer.\*

### BALTIMORE, MD.

Chamber of Commerce Members.  
Baltimore Grain Co., grain receivers.\*  
Baltimore Pearl Hominny Co., corn products.\*  
Blackburn & Co., G. F., grain receivers, exporters.\*  
Dennis & Co., grain merchants.\*  
England & Co., Chas., grain, hay.\*  
Fahy & Co., John T., grain receivers and exporters.\*  
Hammond, Snyder & Co., Inc., receivers, exporters.\*  
Hax & Co., G. A., grain, hay, seeds.\*  
Lederer Bros., grain receivers.\*  
Manger & Co., J. A., grain, seeds, hay.\*  
Robinson & Jackson, grain commission merchants.\*  
Steen & Bro., E., grain receivers and exporters.\*  
Wack & Co., Henry E., grain, hay, feeds.\*

### BEAUMONT, TEX.

Archer Brokerage Co., grain brokers.

### BESSEMER, ALA.

Curry & Co., J. C., feedstuffs, flour, meal.

### BIRMINGHAM, ALA.

Birmingham Grain Co., grain, feed, flour.\*  
Cecil Brokerage Co., grain, hay, millfeed brokers.\*  
Cooley Flour & Grain Co., W. M., grn., flour crn. ml.\*  
Cushy, Edward T., flour, grain, feed broker.\*  
Hemphill & Co., R. C., mdse. & grain brokers.  
Montgomery Brokerage Co., grain, gr. pdts., hay, mdse.  
Morgan & Co., B. C., broker, grain, feed, flour, hay.\*  
Ramsey & Co., J. E., mdse., grain, feed, flour broker.\*  
Southeastern Brokerage Co., grain, hay, feed.  
Sunny South Grain Co., mfrs. mxd. fd., ctn. sd. ml.  
Tennessee Mill & Feed Co., feedstuffs.\*  
Western Grain Co., mfrs. mxd. feed, crn. meal, grits.\*  
Wood-Crabbe Grain Co., mfrs. crn. ml., grts., gr., hay.\*

### BLOOMINGTON, ILL.

Hasenwinkle Grain Co., cash and futures.  
Hick, L. E., grain.  
Worth-Gyles Grain Co., cash and future grain.\*

### BOSTON, MASS.

Chamber of Commerce Members.  
Benzaquin, Matthew D., grain brokerage, commission.\*  
Jaquith, Parker, Smith & Co., wheat, barley, milo.\*

### BUFFALO, N. Y.

Corn Exchange Members.  
Armour Grain Co., grain merchants.\*  
Burns, Berend J., grain and feed merchant.  
Burns Grain Co., grain commission.\*  
Churchill Grain & Seed Co., receivers, shippers.\*  
Doorty-Ellsworth Co., Inc., brokerage commission.\*  
Electric Grain Elevator Co., consignments.\*  
Gee-Lewis Grain Co., grain consignments.\*  
Harold, A. W., grain, barley a specialty.  
Irwin, Dudley M., barley.\*  
McConnell Grain Corporation, commission merchants.\*  
Kennedy & Co., Chas., wheat a specialty.\*  
Pratt & Co., receivers, shippers of grain.\*  
Ratcliffe, S. M., commission merchant.  
Taylor & Bourneque Co., grain merchants.\*  
Urmston Grain Co., grain commission.\*  
Whitney & Gibson, consignments.\*

### CAIRO, ILL.

Board of Trade Members.  
Hulliday Elevator Co., grain dealers.\*  
Magee-Lynch Grain Co., grain.\*  
Thistlewood & Co., grain and hay.

### CEDAR RAPIDS, IOWA.

Cedar Rapids Grain Co., corn and oats.\*  
Gifford-Matthews Co., grain and grain products.\*  
King Wilder Grain Co., grain shippers.\*

### CHATTANOOGA, TENN.

Grain Exchange Members.  
Bagley & Semmes, hay, grain and feed brokers.  
Harbin, A. D., hay, grain and mill feeds.  
Tennessee River Mfg. Co., corn buyers, corn meal mfrs.  
Thomasson & Co., J. T., grain, hay, feed.  
Winer Feed Co., food, feed, seed.

### CHICAGO, ILL.

Board of Trade Members.  
Anderson & Co., W. P., grain commission merchants.\*  
Armour Grain Co., grain buyers.\*  
Bailey & Co., E. W., grain commission merchants.\*  
Bartlett-Frazier Co., grain merchants.\*  
Bennett & Co., Jas. E., commission merchants.\*  
Breanan & Co., John B., grain commission merchants.\*  
Bridge & Leonard, commission merchants.\*  
Carhart Code Harwood Co., grain commission.\*  
Dole & Co., J. H., grain and seeds.\*  
Freeman & Co., Henry H., grain, hay, straw.  
Gerstenberg & Co., commission merchants.\*  
Hales & Edwards Co., grain merchants.\*  
Harris, Winthrop & Co., grain commission.\*  
Hitch & Carder, commission merchants.\*  
Holt & Co., Lowell, commission, grain and seeds.  
Lamson Bros. & Co., consignments solicited.\*  
Lewis & Co., F. S., grain and provisions.\*  
Lowitz & Co., E., grain commission.\*  
McKenna & Dickey, commission merchants.\*  
Mumford & Co., W. R., grain, hay, millstuffs.\*  
Norris Grain Co., grain merchants.\*  
Northern Grain & Warehouse Co., grain receivers.  
Paynter, H. M., grain and field seeds.  
Perrine & Co., W. H., grain and commission.\*  
Pope & Eckhardt Co., grain and seeds.  
Press & Co., W. G., grain, provisions, stocks, etc.  
Quaker Oats Co., buyers of grain.  
ReQua Bros., wheat a specialty.\*  
Rosenbaum Bros., receivers, shippers.\*  
Rosenbaum Grain Co., J. J., grain merchants.\*  
Rothschild Co., Moses, receivers and shippers.  
Rumsey & Co., grain commission.\*  
Sawers Grain Co., grain commission.\*  
Schiffin & Co., P. H., commission.\*  
Shaffer & Co., J. C., grain merchants.\*  
Simons, Day & Co., grain merchants.\*  
Somers, Jones & Co., grain and field seeds.\*  
Thayer & Co., C. H., receivers-shippers.\*  
Udike Grain Co., consignments.\*  
Ware & Leland, grain and seeds.\*  
Zweig & Co., Harry A., cash grain only.

### CINCINNATI, O.

Grain & Hay Exchange Members.  
Early & Daniel Co., grain, hay, feed.\*  
Mutual Commission Co., hay, grain and feed.\*  
Perin Bros., want corn.\*

### CLEVELAND, O.

Grain & Hay Exchange Members.  
Bailey, E. I., grain and millfeed.\*  
Cleveland Grain Co., The, receivers and shippers.\*  
Gates Elevator Co., The, receivers and shippers.\*  
Lake Shore Elevator Co., grain and feed.\*  
Sheets Elevator Co., The, grain, hay, straw.\*  
Shepard, Clark & Co., grain merchants.\*  
Star Elevator Co., receivers, grain, hay, straw.\*  
Strauss & Co., H. M., receivers, shippers hay & grain.\*  
Union Elevator Co., The, grain and hay.\*

### CULLMAN, ALA.

Ponder & Kelley, grain, feed, gen'l mdse.

### DALLAS, TEX.

E. A. Johnson Co., grain and flour brokers.  
\*Member Grain Dealers National Association.

### DAVENPORT, IOWA.

Davenport Elevator Co., receivers and shippers.\*

### DECATUR, ALA.

Lyle-Taylor Grain Co., whole. grain, hay, feeds.

### DECATUR, ILL.

Baldwin & Co., H. I., grain brokers.\*

### DENVER, COLO.

Grain Exchange Members.  
Ady & Crowe Merc. Co., The, grain and hay.  
Conley-Ross Grain Co., The, grain and beans.  
Crescent Flour Mills, The, wheat, corn oats.\*  
Gallagher Grain Co., grain merchants.\*  
Denver Elevator, We buy and sell grain and beans.\*  
Hungarian Flour Mills, wheat, corn, oats, etc.  
Kellogg Grain Co., O. M., receivers and shippers.\*  
McCaull-Dinsmore Co., wholesaler and commission.\*  
O'Donnell Grain Co., wholesale grain.\*  
Phelps Grain Co., T. D., wholesale grain.\*  
Scott-George Grain Co., receivers and shippers.\*  
Summit Grain & Coal Co., wheat, corn, oats, rye, barl.\*  
Thompson Merc. Co., The W. F., wholesale hay.  
Warwick Grain Co., buyers & sellers all kinds of grain.

### DES MOINES, IOWA.

Board of Trade Members.  
Anderson Co., D. L., grain merchants.\*  
Bartz & Co., W. H., grain merchants.\*  
Des Moines Elevator & Grain Co., oats a specialty.  
Harper & Sons, commission merchants.\*  
Iowa Grain Co., receivers and shippers.\*  
Lockwood, Lee, grain, millfeed broker.  
Marshall Hall Grain Co., grain commission.\*  
Mid-West Consumers Grain Co., grain merchants.\*  
Perrine & Co., W. H., commission merchants.\*  
Taylor & Patton Co., corn and oats.  
Tower, C. A., grain broker.

### DETROIT, MICH.

Board of Trade Members.  
Carson & Co., H. C., corn, oats, rye.\*  
Dumont, Roberts & Co., receivers, shippers.\*  
Huston, C. R., grain, hay consignments a specialty.\*  
Lichtenberg & Son, oats, corn, hay, straw.  
Simmons & Co., F. J., grain and hav.\*

### FORT WORTH, TEX.

Grain and Cotton Exchange Members.  
Dorsey Grain Co., receivers, shippers.

### GADSDEN, ALA.

Alabama Brokerage Co., grain, feed, hay, fr., mdse.  
Gadsden Brokerage Co., feed, fr., hay c/s pdts.

### GREAT FALLS, MONTANA.

Barkemeyer Grain & Seed Co., grain dealers.\*

### GREENVILLE, MISS.

Lyle & Lyle, whole. grain, hay, feedstuffs.

### GREENSBORO, N. C.

Moon-Taylor Co., grain and hay brokers.\*

### GREENVILLE, O.

Grubbs Grain Co., E. A., track buyers.\*

### GULFPORT, MISS.

Corso & Runfalo, gro., grain, feed, flour, hay.  
Gulfport Grocery Co., gro., grain, fd., dr., etc.\*  
Howie & Co., J. B., bkrs., grain, fd., dr., hay.

### HAMMOND, IND.

Nowak Milling Corp., "Domino Feeds."

### HASTINGS, NEBR.

Elder, Fred W., wholesale grain, hay and mill pdts.\*  
Koehler-Twidale Elevator Co., grain dealers.\*  
Moritz Grain Co., Chas., wholesale grain.  
Sexson, C. R., grain.

### HATTIESBURG, MISS.

Bobo & Co., W. E., bkrs., mdse., grain, hay, fd., dr.  
McLain & Co., A. S., grain, feed, mdse. broker.  
Merchants Grocery Co., whole. grocers, grain, fd., dr.

### HOUSTON, TEX.

Beatty-Archer Co., grain brokers.\*  
Gulf Grain Co., grain, hay, millfeed.\*  
(Continued on next page.)



# Directory of the Grain Trade

In Organized Markets Only Members of the Local Grain Exchange Will Be Listed

## HUTCHINSON, KANS.

**Board of Trade Members.**  
Farmers Co.-Op. Com. Co., commission merchants.  
Gano Grain Co., grain merchants.  
Hausam-Bateman & Co., grain buyers and sellers.  
Hayes Grain Co., John, grain merchants.  
Hutchinson Grain Co., grain merchants.  
Kelly Milling Co., Wm., millers of hard wheat.  
Jay Hausam & Company, grain merchants.  
McClure Grain Co., J. B., buyers and sellers.\*  
Pettit Grain Co., L. H., grain merchants.  
Reno Flour Mills Co., millers and grain dealers.  
Rock Milling & Elevator Co., receivers and shippers.  
Russell Grain Co., commission merchants.  
Southwest Grain Co., receivers and shippers.  
The Security Ele. Co., receivers, shippers mto kafir.  
Union Grain Co., grain merchants.

## HUNTSVILLE, ALA.

Huntsville & Decatur Bkg. Co., dlsr. gr. & gr. pdts.  
Lyle & Lyle, whse, grain, hay, feedstuffs.

## INDIANAPOLIS, IND.

**Board of Trade Members.**  
Belt Elevator & Feed Co., receivers and shippers.  
Bingham-Hewett-Scholl Co., grain merchants.\*  
Boyd Grain Co., Bert A., strictly brokerage & com.\*  
Cleveland Grain Co., grain commission.  
Evans Grain Co., W. R., commission and brokerage.\*  
Goldberg Grain Co., consignments.\*  
Hayward-Rich Grain Co., grain commission.\*  
Hill Grain Co., The Lew, commission & brokerage.  
Hoosier Grain Co., consignments only.  
Kinney Grain Co., H. E., receiver and shipper.\*  
McCord-Black Co., grain merchants.  
Menzie Grain & Bkg Co., Carl D., grain commission.\*  
Merchants Hay & Grain Co., hay, grain, mill feed.\*  
Minor & Son, B. B., grain consignments solicited.  
Montgomery & Tompkins, receivers and shippers.  
Sawers Grain Co., consignments.  
Stetwell & Co., C. A., grain & hay.  
Steinhart Grain Co., commission and brokerage.  
Urmston Grain Co., receivers and shippers.\*  
Witt, Frank A., grain commission and brokerage.\*

## JACKSON, MICH.

Bartlett & Co., J. E., feed, grain, salvage.\*  
Stockbridge Elvtr., track buyers, sellers, gr. & sds.  
Wagter-White Co., track buyers-sellers, grain-feed.

## JACKSON, MISS.

Aviston Flour Co., feed mfrs. whl. flour, grain.\*  
Brittain & Co., P. L., mer. bkrs., dlsr. hay, gr., mll pdts.  
Green, R. H., whse. grocers, pdce, grain, mill feed.  
Nall & Co., A. S., grain brokers.  
Royal Feed & Mfg. Co., mixed feed mfrs.\*

## JASPER, ALA.

Acuff, J. D., buyer white milling corn.  
Robins, J. H., grain, hay, feed, flour broker.

## KANSAS CITY, MO.

**Board of Trade Members.**  
Addison Grain Co., consignments.  
Beyer Grain Co., consignments and mill orders.  
Bruce Bros. Grain Co., consignments.  
Christopher & Co., B. C., kafir, feterita, milo.\*  
Croydsdale Grain Co., grain commission.  
Davis Grain Co., A. C., grain commission.  
Denton Kuhn Grain Co., consignments.\*  
Dilts & Morgan, consignments.\*  
Ernst Davis Grain Co., commissions.\*  
Federal Grain Co., receivers, shippers.\*  
Fisher Grain Co., C. V., receivers & shippers of grain.\*  
Frisco Elevators Co., grain merchants.\*  
Goffe & Carkeuer, receivers and shippers of grain.\*  
Hall-Baker Grain Co., consignments.  
Hinds Grain Co., The receivers, shippers.\*  
Hipple Grain Co., feterita, kafir, milo.  
Larabee Flour Mills Corp., The, mfrs. "Larabee Best."  
Masters Brokerage Co., H. S., flour, millfeed, grain.  
Mensendieck Grain Co., consignments.\*  
Moore-Lawless Grain Co., grain receivers.\*  
Moore-Seaver Grain Co., receivers and shippers.\*  
Nellis-Witter Grain & Mfg. Co., grain and feed.\*  
Norris Grain Co., grain merchants and exporters.  
Orthwein Matchette Co., consignments, buying orders.  
Parker Corn Co., corn, oats, kafir, milo.\*  
Roehen Grain Co., E. E., consignments.  
Root Grain Co., consignments and futures.\*  
Shannon Grain Co., consignments.  
Stevenson Grain Co., buyers and sellers.\*  
Terminal Elevators, receivers, shippers.\*  
Twidale-Wright Grain Co., consignments futures.  
Thresher Fuller Grain Co., grain commission.\*  
Western Grain Co., shippers (a specialty).\*

## KNOXVILLE, TENN.

Davis & Susong Co., hay, grain, feed.\*  
Levy & Co., R. T., merchandise and grain brokers.  
Security Mills & Feed Co., flour and feed.\*  
Smith & Co., J. Allen, flour, meal, feed.\*

## LAKE CHARLES, LA.

Beatty Brokerage Co., grain brokers.\*

## LITTLE ROCK, ARK.

**Grain Exchange Members.**  
Cunningham Commission Co., grain, corn products.\*  
Caple & Stockton, hay, grain, feed.  
Cochran Co., H. K., grain dealers.

## LITTLE ROCK (Continued).

Daniel Mill & Elevator Co., Joe, grain, hay, feed.  
Darragh Company, hay, grain, mixed feeds.\*  
Davis, S. P., dealer, consignments.  
Farmer Co., E. L., brokers, hay, grain, mill feed.\*  
Gordy Co., C. L., grain brok., hay, grain, mill feed.\*  
Hayes Grain & Commission Co., dealers in grain, hay.\*  
Munn Brokerage Co., grain, hay, mill feed.\*  
Niemeyer Grain Co., George, grain, hay, mill feed.\*  
Weinmann Milling Co., grain, hay, mixed feeds.  
Wilson Co., John R., grain brokers.  
Wilson & Co., hay, grain, feed brokers.

## LIMA, O.

Riddle & Co., T. P., hay and grain.

## LINCOLN, NEBR.

**Grain Exchange Members.**  
Barstow Grain Co., W. T., recvrs. and shprs. all grs.  
Central Granaries Co., wheat, corn, oats, rye barley.  
Cummings Grain Co., M. T., grain, seed, millfeed.  
DeWitt Grain Co., grain.  
Ewart Grain Co., wheat, corn, oats, rye, barley.\*  
Foster Grain Co., receivers and shippers.  
Gooch Milling & Elvtr. Co., flour, feed, macaroni pdts.  
Lincoln Grain Co., grain merchants.\*  
Nebraska Corn Mills, meal, pearl hominy, corn, flour.  
Paul, John M., grain broker.  
Western Feed Dealers Supply Co., hay, gr., millfeeds.  
Wright-Leet Grain Co., receivers and shippers.

## LOS ANGELES, CALIF.

**Grain Exchange Members.**  
Pacific Grain & C. S. P. Co., grain & c/s meal.

## LOUISVILLE, KY.

**Board of Trade Members.**  
Bingham-Hewett Gr. Co., receivers-shippers of grain.\*  
Edinger & Co., grain, hay, flour.  
Farmer & Sons, Oscar, hay, grain and feeds.\*  
Kentucky Public Elevator Co., storers and shippers.\*  
Schuff & Co., A. C., specialty white mlg. corn, wh.  
Verhoeff & Co., H., receivers and shippers.\*  
Zorn & Co., S., receivers and shippers.\*

## LYNCHBURG, VA.

Moon-Taylor Co., grain and hay brokers.\*

## MACON, GA.

Dorsett Co., W. C., brokers, grain feed, flour.  
McCord, Chas. R., flour, grain, feed, broker.  
McRae, D. L., flour, grain, hay, feed, broker.\*  
Middlebrooks Bros., brokers.\*  
National Milling Co., mfrs. mixed feed.  
Pitner, Beusse & Morgan, brokers.\*  
Quinby, Edmund B., hay, grain, flour, feed broker.

## MEMPHIS, TENN.

**Merchants Exchange Members.**  
Anderson, Embrey E., grain, hay, ear corn specialty.\*  
Brown, Walter M., broker & com., consignments.\*  
Buxton, E. E., broker and commission merchant.\*  
Bluff City Grain Co., all grains, oats a specialty.\*  
Cereal Byproducts Co., everything for mixed feeds.  
Clark-Burkle & Co., grain and hay.\*  
Cook, L. P., receiver and shipper.  
Davis & Andrews Co., grain, mixed feed.\*  
Denyven & Co., brokers and commission.\*  
Edgar-Morgan Co., mixed feed manufacturers, grain.  
Hasenwinkle, H. J., consignments.  
Horton & Co., J. B., grain dealers.  
International Sugar Feed No. 2 Co. mfrs. swt. mx. fd.  
Jones, Lee D., grain dealers.\*  
Lovitt & Co., L. B., cotton seed and peanut products.  
Marks & Anderson, wholesale grain, hay, mixed feed.  
Memphis Milling Co., high grade mixed feed.\*  
National Brokerage Co., flour, grain, feed brokers.\*  
Mississippi Elevator Co., grain dealers, feed mfrs.\*  
Nessly Co., J. L., broker, com., alf. meal, molasses.\*  
Patterson & Co., G. E., mfrs. mixed feed, grain.\*  
Pease & Dwyer, grain, mixed feed.\*  
Riverside Elevator & Warehouse, broker & whse'man.  
Royal Feed & Milling Co., mixed feed manufacturers.  
Scruggs-Robinson Co., brok. com. mer., consignments.\*  
Sessum Coal & Grain Co., grain, mixed feed.  
Stout-Hunt Milling Co., flour and corn meal.  
Tate & Co., J. E., wholesale grain dealers.  
U. S. Feed Co., grain, hay, mill feed.\*  
Wade & Sons, Inc., John, grain, feed, flour.\*

## MERCER, MO.

Alley Grain Co., oats, corn, wheat, seeds.\*  
Alley, A. A., dealers & broker, grain and seeds.

## MERIDIAN, MISS.

**Board of Trade Members.**  
Anderton & Co., W. A., whse brokers, gr., hay, fd.  
Elkin-Henson Grain Co., gr. mxd. fd., hay, ctn. sd. pts.  
George Co., The A. H., grain dlsr., mxd. fd. mfrs.\*  
Gibson Brokerage Co., J. A., grain & mdse. brokers.  
Harris, John H., grain and mdse. brokers.  
Hayward & Scott, grain brokers & mfrs. agents.  
Lyle Grocery Co., The Tom, who. gro., grain & hay.  
Lyon & Co., A. J., whse gro., grain, feed.  
Meridian Grain & Elvtr. Co., gr. dlsr., mxd. fd. mfrs.  
Meyer Bros., wholesale groc., grain, feed.  
Smith Brokerage Co., grn., gr. pdts., hay, ctn., sd. pts.  
Sturgis Co., grain dealers, mixed feed mfrs.\*  
Threefoot Bros. & Co., whse grain, feed, flr., gro.  
Snowden Com. Co., grain, hay, rice brokers.  
Tutt Grain Co., J. B., grain, fd., ctn. sd. pdts., dlsr.  
Queen City Feed Co., grain, hay, flour, corn meal.  
Winner-Klein & Co., whse gro., grain, feed.

## MIDDLETOWN, CONN.

Meech & Stoddard, Inc., grain, feed, hay, flour.\*

## MILWAUKEE, WIS.

**Chamber of Commerce Members.**  
Bacon Co., E. P., grain and seed.\*  
Franke Grain Co., feeds, grain, hay.  
Godfrey-Blanchard Co., grain receivers.\*  
Kamm Company, P. C., barley and rye.\*  
Quinn Shepherdson Co., receivers & shippers.  
Rankin, M. G., & Co., grain and feed.  
Rialto Elvtr. Co., grain receivers and shippers.\*  
Taylor & Bourneau Co., grain merchants.\*  
Thayer & Co., C. H., receivers & shippers.\*

## MINNEAPOLIS, MINN.

**Chamber of Commerce Members.**  
Banner Grain Co., grain receivers.  
Benson, Staback Co., grain commission.\*  
Cargill Commission Co., grain commission.  
Carter, Sammis & Co., grain commission.  
Cereal Grading Co., grain merchants.\*  
Chambers-Mackay Co., screenings & mill feed.  
Dalrymple Co., William, grain commission.\*  
Davies & Co., F. M., grain commission.\*  
Gee Grain Co., G. E., receivers and shippers.  
Getchell-Tanton Co., grain commission.  
Godfrey-Blanchard Co., grain receivers-shippers.\*  
Gould Grain Co., receivers and shippers.\*  
Hankinson & Co., H. L., grain commission.  
Lewis & Co., Chas. E., consignments.\*  
Malmquist & Co., C. A., receivers & shippers.  
Marfield Grain Co., grain commission.\*  
McCaull Dinsmore Co., consignments solicited.\*  
Quinn-Shepherdson Co., receivers and shippers.  
Scroggins Grain Co., corn and oats.\*  
Van Dusen-Harrington Co., grain merchants.\*  
Welch Co., E. L., mill oats & screenings.\*  
Wernli-Anderson Co., grain commission, screenings.  
Woodward Newhouse Co., grain merchants.  
Zimmerman, Otto A., barley and oats my specialty.

## MOBILE, ALA.

Butler & Clark, mdse., grain, ctn. sd. pdts., brokers.  
Cleveland Co., Frank, bkrs. & com. merchants.  
Hopper & Co., H. M., grain, mlfds., hay brokers.\*  
King & Co., John R., bkrs., grn., mlfds., hay, beans.  
Meador & Co., W. M., mdse. & grain brokers.  
Minnis & Co., A. R., bkrs., grain, feed, ctn. sd. pdts.  
Suttle, J. L., grain, gr. pdts., feed, mdse. broker.\*  
Turner-Young Grain Co., dlsr., gr., gr. pdts., mfrs. fd.  
Vass & Co., H. P., mdse. & grain brokers.  
Wilson Brokerage Co., expt. freight bkrs., fwdg. agts.  
Ziliak & Schafer Mlg. Co., grain & gr. pdts., feed.  
Zimmern's Co., J., mxd. fd. mfrs., dlsr. grain & hay.

## MONTGOMERY, ALA.

Alabama Black Belt Co., gr. bkrs., dlsr. in Ala. pdts.  
American Mlg. & Feed Co., mfrs. crn. mlg. gr. dl. fd.  
Browder Bros., grain, feed, flour, mdse. bkrs.  
Cody-Craig Co., grain, feed, flour, mdse., brokers.  
Hobbie Grocery Co., H. M., opertrs. "Hobbie Elvtr."\*  
Holland & Co., O. C., mdse. & grain brokers.  
Lutz & Co., E. G., bkrs., grain, feed, flour, mdse.  
Mitchell & Co., Chas. E., dlsr., hay, grain, cow peas.  
Richardson Co., O. A., grain, feed, flour brokers.  
Shank & Copeland, bkrs., grain, fd., flr., hay, mll pdts.  
Smith Brokerage Co., grain, feed, flour, hay.  
Winter, Loeb Grocery Co., grain, feed, flour, mdse.

## NASHVILLE, TENN.

**Grain Exchange Members.**  
Allen Grain Co., receivers and shippers.\*  
Bennett & Co., John C., brokers.\*  
Crozier & Co., W. H., receivers and shippers.\*  
Daugherty-Vincent Grain Co., receivers and shippers.\*  
Gillette Grain Co., The, operators steel elevator.\*  
Hale & Sons, J. R., receivers and shippers.\*  
Hermitage Elvtr. & Warehouse Co., public storage.\*  
Jones & Co., Chas. D., receivers and shippers.\*  
Kerr, S. S., receiver and shipper.\*  
Liberty Mills, flour and feed.\*  
Mero Mills, FLavo FLour, feeds.\*  
Nashville Grain & Feed Co., receivers and shippers.  
Nashville Roller Mills, self rising flour mfrs.\*  
Nashville Warehouse & Elvtr. Co., public storage.\*  
Neil Shofner Grain Co., receivers and shippers.\*  
Rex Mill & Feed Co., grain and feeds.\*  
Tate, W. R., receiver and shipper.\*  
Tennessee Grain Co., receivers and shippers.\*  
Tyner & Co., John A., corn, wheat, oats, seeds.\*  
Wilkes & Co., J. H., grain, flour, feeds.\*  
Worke & Co., R. H., grain, feed, hay.

## NEWARK, N. J.

Smith & Wallace Co., J. C., receivers, shippers.

## NEW CASTLE, PA.

Hamilton Co., grain, feed, flour, hay, potatoes.\*

## NEW ORLEANS, LA.

**Board of Trade Members.**  
Anderson & Jackson, Inc., exporters of grain.\*  
Barr, R. J., grain exporter.\*  
Gibbons, J. T., gr. dealers, mixed fd. mfrs., exptrs.  
Kalman Co., Paul R., recvrs., shprs., exptrs. of grain.  
Langeberg Hay & Grain Co., recvrs., shprs. & exptrs.  
Lunham & Moore, forwarding & freight brokerage.  
Matthews & Bro., Chas. R., grain & grain pdts., brks.\*  
Matthews & Sons, Geo. B., recvrs. & shprs., feed mfrs.  
Milam-Morgan Co., Ltd., recvrs., shprs., mx. fd. mfrs.



## Directory of the Grain Trade

In Organized Markets Only Members of the Local Grain Exchange Will Be Listed

### NEW ORLEANS (Continued).

Nathan & Fettes, fwdg. agt. & expt. fght. broker.\*  
Neumond, K. & E., dlrs. & expts in feed articles.  
Richeson Co., Inc., W. L., expt. shpg. fgt. bkg. & fg.\*  
Rodd & Co., Chas. M., gr. brokers & fwdg. agents.\*  
Rogers, James M., grain brokerage & forwarding.\*  
Royal Feed & Mfg. Co., mfgs. of mixed feed.  
Thomas Co., James, exporters & forwarders.  
Waterman & Co., J. S., gr. flour & fd. bkrs., fr. jbrs.\*

### NEW YORK CITY.

Produce Exchange Members.  
Bolle-Watson Co., Inc., receivers and exporters.  
Brainard Commission Co., consignments.\*  
Jones & Co., M. B., buyers—quote us.\*  
Knight & Company, commission merchants.\*  
Morey, L. A., grain.  
Schwartz & Co., B. F., commission merchants.\*  
Therrien, A. F., broker.\*

### NORFOLK, VA.

Moon-Taylor Co., grain and hay brokers.\*

### OKLAHOMA CITY, OKLA.

Grain Exchange Members.  
Cherokee Grain Co., grain merchants.\*  
Cosart Grain Co., C. B., grain merchants.\*  
Conyers Grain Co., grain merchants.\*  
Dustin Grain Co., grain, feed, seeds.\*  
Langenberg Bros. Grain Co., grain merchants.  
Maney Export Co., grain merchants.  
Marshall-Jacobson Grain Co., grain, feed, seeds.\*  
Mid-West Grain Co., grain merchants.\*  
Oklahoma Export Co., grain commission.\*  
Okla. City M. & E. Co., grain merchants, millers.\*  
Rutledge Grain Co., commission merchants.\*  
Perkins Grain Co., W. L., commission merchants.\*  
Polson & Co., C. A., commission merchants.\*  
Stinnett Grain Co., grain merchants.\*  
Stowers Grain Co., W. B., commission merchants.\*

### OMAHA, NEBR.

Grain Exchange Members.  
Adams-Reitz Grain Co., consignments.\*  
Butler Welsh Grain Co., grain merchants.\*  
Carlisle Grain Co., S. S., consignments.\*  
Cope & Kearney, grain commission.  
Crowell Elevator Co., receivers, shippers.\*  
Dawson Grain Co., grain merchants.\*  
Holmquist Elevator Co., receivers and shippers.\*  
Kern Co., brokers and commission merchants.\*  
Maney Grain Co., The, consignments.\*  
Merriam Commission Co., consignments.\*  
Miller Wilson Grain Co., receivers and shippers.\*  
Nye Schneider Fowler Grain Co., consignments.\*  
Oswald Delaney Grain Co., consignments.\*  
Roberts Grain Co., Geo. A., grain merchants.\*  
Rothschild Grain Co., corn and oats.\*  
Stockham Grain Co., E., commission merchants.\*  
Taylor Grain Co., brokers.\*  
Trans-Mississippi Grain Co., receivers and shippers.\*  
United Grain Co., grain commission.\*  
Updike Grain Co., consignments.\*

### PEORIA, ILL.

Board of Trade Members.  
Bartlett Co., S. C., grain commission.\*  
Bowman & Co., Geo. L., grain commission.  
Buckley & Co., grain and seeds.\*  
Cole Grain Co., Geo. W., receivers and shippers.\*  
Conover Grain Co., E. B., grain commission.\*  
Dewey & Sons, W. W., grain commission.\*  
Feltman Grain Co., C. H., grain commission.\*  
Harwood-Young Co., grain commission.  
Luke Grain Co., grain commission.\*  
McFadden & Co., G. C., consignments.\*  
McCreery & Son, J. A., wheat, corn, oats.\*  
Miles, F. B. & C. O., grain commission.\*  
Mueller Grain Co., receivers and shippers.\*  
Slick, L. E., consignments solicited.  
Turner Hadrut Co., receivers and shippers.\*  
Tyng Grain Co., commission merchants.\*  
Worth-Gyles Grain Co., grain commission.\*

### PENSACOLA, FLA.

Bonacker Bros., bkrs., gr., hay, feed, fr., etn. sd. pts.  
Consolidated Grocery Co., whole gro., grain, produce.  
Gonzalez Co., The M. F., gr., hay, feed, mfrs. crn ml.  
Jones & Co., B., grain, hay, fr., mill feed brokers.  
Meador & Co., W. M., mdse. & grain brokers.  
Wagenheim Co., The I. E., bkrs. mdse., gr., shorts, bn.  
Wolf, I., mdse., grain, feed broker.

### PHILADELPHIA, PA.

Commercial Exchange Members.  
Delp Grain Co., E. E., grain and millfeeds.\*  
Dunwoody Co., Ezl., flour, grain, feed.\*  
Lemont & Son, E. K., hay, grain, millfeed.  
Miller & Sons, L. F., grain, seeds, hay.\*  
Richardson Bros., grain, flour, millfeeds.\*  
Richardson, Geo. M., grain and feeds.\*  
Rogers & Co., E. L., hay, straw, grain, feed.\*  
Taylor & Bournique Co., grain merchants.\*  
Young & Co., S. H., wheat, corn, oats.

### PITTSBURGH, PA.

Members Grain and Hay Exchange.  
Elwood & Co., R. D., hay and grain.\*  
Geldel & Leubin, grain and hay.  
Hardman & Heck, grain, hay, millfeed.\*  
Heck & Co., W. E., grain, hay, millfeed.\*  
Herb Bros. & Martin, grain and hay.  
McCague, R. S., grain, hay.  
Taylor & Bournique Co., grain merchants.\*  
Walton Co., Samuel, grain and hay.\*  
Young & Fisher, brokerage and commission.\*

### PONTIAC, ILL.

Balbach, Paul A., grain buyer, all markets.

### PORTLAND, ORE.

Albers Bros. Mfg. Co., millers and exporters.  
Globe Grain & Mfg. Co., grain, hay and feed.\*  
Kerr, Gifford & Co., Inc., grain exporters.\*  
Northern Grain & Warehouse Co., grain exporters.\*  
Pacific Coast Elevator Co., grain.\*  
Pacific Grain Co., grain exporters.\*  
Tri-State Terminal Co., general grain and bags.\*

### PUEBLO, COLO.

McClelland Mc'I I. & R. Co., grain, hay and feed.\*

### RICHMOND, VA.

Grain Exchange Members.  
Beveridge & Co., S. T., grain, hay, feeds, seeds.

### ROME, GA.

Childsey, Jas. B., grain, hay and feed broker.\*  
White Brokerage Co., grain, hay, flour.

### ROANOKE, VA.

Albergotti Bros., hay, grain, feed, flour.\*

### SAGINAW, MICH.

Smith-Connor Hay & Grain Co., hay and grain.

### SEATTLE, WASH.

Albers Bros. Mfg. Co., millers and exporters.  
Fairbanks & Co., F. M., grain and feed.  
Lilly Co., The Chas. H., seed merchants.\*  
Pacific Grain Co., grain exporters.\*  
Tri-State Terminal Co., general grain and bags.\*  
Webster & Co., E. A., grain commission, orinetal seeds.

### ST. JOSEPH, MO.

Grain Exchange Members.  
A. J. Elevator Co., buyers and shippers.\*  
Gordon Commission Co., T. P., gr. dealer and broker.\*  
Great Western Grain Co., buyers and sellers.\*  
Aunt Jemima Mills Co., A. J., hominy feed.  
Marshall Hall Grain Co., consignments solicited.\*  
McKee Lindley & Dunn Grain Co., commission.  
Sloan Simmons Grain Co., consignments.\*  
St. Joseph Hay & Grain Co., grain merchants.\*

### ST. LOUIS, MO.

Annan Burg G. & M. Co., flour, grain, millfeed.\*  
Ballard-Messmore Grain Co., recvrs. grain, hay, seeds.  
Bushfield Grain Co., receivers and shippers.\*  
Dreyer Commission Co., feeding stuffs, grain, seeds.\*  
Elnore Schultz Gr. Co., receivers and shippers grain.\*  
Goffe & Carkner Co., grain commission.\*  
Graham & Martin Grain Co., grain commission.\*  
Ichtertz & Watson, grain, seeds and hay.\*  
Marshall Hall Grain Co., grain commission.\*  
Langenberg Bros. Grain Co., grain commission.\*  
Morton & Co., grain commission.\*  
Nanson Commission Co., grain commission.\*  
Picker & Beardsley Com. Co., grain and grass seed.\*  
Powell & O'Rourke, corn a specialty.  
Thurnau Grain & Feed Co., Arnold A., grn., fd., scrngs.\*  
Turner Grain Co., grain commission.\*

### SELMA, ALA.

Baker, A. E., mdse., grain, feed, flour broker.  
Campbell & Co., McD., brokers gr., fr., mdx. fd., hay.  
Crandell, H. F., merchandise & grain broker.  
Hooper, Son & Coleman, mdse., grain, flour brokers.  
Ross, Rivers F., merchandise & grain broker.

### SIOUX CITY, IOWA.

Board of Trade Members.  
Jutton Co., L. O., grain commission.  
Bailey, Walter H., grain merchants.\*  
Godfrey Blanchard Co., grain receivers.\*  
McGaul Dinsmore Co., commission.\*  
Morris Grain Co., broker.  
Kumsey & Co., receivers of consignments.\*  
Slaughter Burke Grain Co., receivers, shippers.\*  
Taylor & Bournique Co., grain merchants.\*  
Terminal Grain Corp., receivers and shippers.  
Warwick Grain Co., consignments solicited.  
Western Terminal Elevator Co., receivers and shippers.\*

### TALLEDEGA, ALA.

Farmers Ex. & Elvtr. Co., grain, feed, mfrs, corn meal.

### TERRE HAUTE, IND.

Kuhn & Co., Paul, receivers and shippers.\*

### TOLEDO, OHIO.

Produce Exchange Members.  
De Vore & Co., H. W., grain and seeds.\*  
King & Co., C. A., grain and seeds.  
Morehouse & Co., wholesale grain and seeds.  
Rice Grain Co., cash grain.\*  
Southworth & Co., grain and seeds.\*  
Wickenhiser & Co., John, grain receivers, shippers.  
Young Grain Co., grain receivers, shippers.  
Zahn & Co., J. F., grain seeds.\*

### TOPEKA, KANS.

Derby Grain Co., wheat, corn, oats, and millfeed.\*

### TOPPENISH, WASH.

Preston Co., H. P., alfalfa meal, grain, hay, feedstuffs.

### TUSCALOOSA, ALA.

Eddins Brokerage Co., grain, feed, flour, mdse.  
Indian Milling Co., mixed feed mfrs.  
Rosenbush Brokerage Co., grain, feed, flour, mdse.  
Southern Grain Co., grain, hay, mfrs, corn meal.

### TUSCUMBIA, ALA.

Belser, Harry K., grain, feeds, mdse broker.  
Hammerly, E. T., grain, hay, feed, seed.

### UNION CITY, TENN.

Cherry-Moss Grain Co., grain, hay, feeds, seeds.  
Dahnke-Walker Milling Co., milling, grain, feed.\*  
Howell Grain & Feed Co., grain and feed.

### VICKSBURG, MISS.

Weil & Son, R., brokers; corn, oats, meal, hay, chops.  
Wilkerson, R. C., grain broker.

### WICHITA, KANS.

Board of Trade Members.  
Beyer Grain Co., consignments and mill orders.\*  
Blood-Pickerill Grain Co., consignments, mill orders.  
Clark Burd Grain Co., consignments.\*  
Clark Grain Co., C. M., all kinds grain and feed.  
Craig Grain Co., J. W., consignments and mill orders.  
Groth, Samuel C., milling wheat and feed.  
Hayes Grain Co., John, Okla.-Kan. wheat for mills.  
Kansas Flour Mills Co., receivers and shippers.\*  
Kansas Milling Co., millers and grain merchants.\*  
Kelly Grain Co., Edward, grain and mill feed.  
Koch Grain Co., Geo., milling wheat.  
Kramer Grain Co., receivers and shippers.\*  
Raymond Grain Co., consignments.  
Strong Trading Co., wholesale grain and feed.\*  
Wallingford Bros., receivers and shippers.\*  
Warwick Grain Co., "always have a bid."  
Wichita Flour Mills Co., millers and grain merchants.  
Wichita Terminal Elev. Co., general elevator business.\*

### WINCHESTER, IND.

Goodrich Bros. Hay & Grain Co., whlrs. gr. and seeds.\*

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**Ewart Grain Co.,**  
Wheat, Corn, Oats, Rye, Barley.

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Receivers and Shippers of all Grains.

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Grain.

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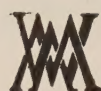
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See what we can do with your next car

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## WHEAT—CORN OATS—RYE BARLEY

### Note These Milwaukee Facts

Annual Malting Capacity	- -	17,000,000 Bu.
Mills Annual Grinding Capacity	-	5,000,000 Bu.
Corn Mills Annually Use	- -	7,500,000 Bu.
Rye Flour Mills Annually Use	-	2,000,000 Bu.
Oat Meal Mills Annually Use	-	7,800,000 Bu.

### 20,000,000 Bushel Elevator Capacity

Any of these Chamber of Commerce Members will tell you more about this market. It will be to your advantage to investigate.

*L. Bartlett & Son Co.  
Flanley Grain Co.  
Henry Rang & Co.  
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*The  
Logical  
Market*



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308-310 Chamber of Commerce, BALTIMORE

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If It's Off Grade  
We Know Values  
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MEMBERS NEW YORK STOCK EXCHANGE  
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MINNEAPOLIS and DULUTH

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20 Years Experience in assembling and  
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For Prompt Shipment in any  
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It will not be long until the new crop is ready to move. To insure proper care being given your grain shipments it behooves you to get into communication with any of the Board of Trade members listed below. They will tell you more about this fast growing market.

Exporters and millers who buy the best grains will also find it to their advantage to consult these firms.

You can't go wrong dealing with Wichita Board of Trade members.

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The Wichita Flour Mills  
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PAYS41 years in the grain business has made us a  
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FORM 88 is designed for making a certified statement of the weight of a car of grain and the condition of the car. It has spaces for Date, Car No., Initial, Date Loaded, Number of Pounds, Kind of Grain, Seal Record, Firm Name and Weigher. These forms are duplicating and are check bound, 75 originals and 75 duplicates in each book. The original forms are printed on bond paper and are machine perforated so that they will tear out readily. The duplicates remain in the book as permanent record. The certificates are numbered in duplicate. Size 8½x4½ inches. Price with four sheets of carbon paper, 75 cents.

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Nashville's Location in the center of Tennessee and its unusual rail facilities enable the market to easily draw large supplies of western grain for reshipping to the Southeast.

The Nashville Grain Exchange Members extend a cordial invitation to the grain trade of the country to help them fill the sacks.



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42-47 Board of Trade

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10 Chamber of Commerce PEORIA, ILL.

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We buy Wheat, Corn, Oats, Beans, etc.  
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We buy and sell Grain of all kinds, also Beans.

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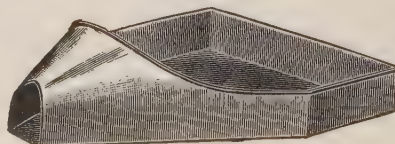
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"Everything for Mixed Feeds"

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**G. E. Patteson & Co.**  
Mfrs. Mixed Feed, Grain

**Pease & Dwyer**  
Grain, Mixed Feed

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SIX MILLS—13,500 barrels capacity—all catering to the popular demand

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GRAIN, MILL PRODUCTS, FIELD SEEDS  
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Advertising Seed**may grow anywhere, but  
you are sure to reap a  
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We Handle  
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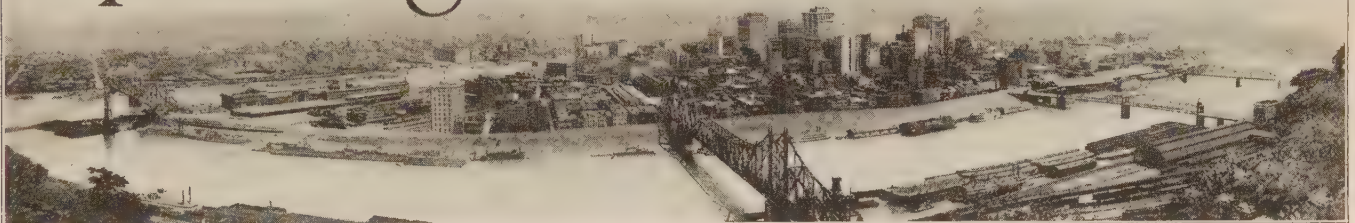
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A GOOD MARKET FOR CORN AND OATS  
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OUR ADVERTISERS OFFER THE BEST.

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*The last word in  
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## “P” Stands for

- 1—Pittsburgh
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- 4—Progressiveness
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- 7—Possibilities unparalleled

Associate your grain transactions with  
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**Receivers and Shippers**Wheat, Barley, Oats, Corn, Rye,  
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Try us—we are excellently equipped to sell your "To Arrive" or Consigned Grain.

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Solicited.  
We place  
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SHIP US THAT NEXT CAR

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Two Rowed and  
Heavy White  
Barley Wanted

*Send Samples*

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**"DOMINO FEEDS"**

We buy at all times  
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Over Forty Years of Service  
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SOLICIT YOUR CONSIGNMENTS OF GRAIN AND SEEDS  
ALSO HEDGING ORDERS

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CASH GRAIN ONLY  
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with Rogers Grain Co.

GRAIN COMMISSION  
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Figure the amount of advertising **Produce Results**  
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CAPITAL . . . . . \$ 5,000,000.00

SURPLUS AND PROFITS . . . . . 10,000,000.00

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**ACCOUNTS OF GRAIN MERCHANTS INVITED****RECEIVERS, SHIPPERS AND BROKERS****S. J. BROWN**

Grain Broker, Spokane, Wash.  
Accounts from reputable grain  
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**WILSON BROKERAGE CO.**EXPORT FREIGHT BROKERS  
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ROANOKE, VA.HAY, GRAIN, FEED AND FLOUR  
We Cover a Great Consuming Territory



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### JOHN WICKENHISER & CO. Wholesale Grain Dealers TOLEDO, OHIO

We make track bids and quote delivered prices.  
Solicit Consignments of Grain and Clover Seed.  
Members Toledo Produce Exchange and Chicago  
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### S. L. Rice Geo. D. Woodman RICE GRAIN CO. TOLEDO, OHIO

Milling Wheat Cash Grain  
Mill Feeds

TOLEDO  
**J.F. ZAHM & Co.**  
HERE SINCE 179

We have no grain of our own to  
sell so can give your shipments our  
undivided attention.

**WHEAT-CORN  
OATS-RYE-SEED**  
Why not Consign—All the Time?

The paper the Grain Dealer  
supports, because it supports  
the Grain Dealer—

**GRAIN DEALERS JOURNAL**

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International Game, played in Toledo, Ohio.  
Providence does dealing. When "Seedy" favor

**C. A. KING & CO.**  
Like Billy Sunday they deal in cash and futures

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**O**ur **A**im **T**o **S**erve  
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## RECEIVERS, SHIPPERS AND BROKERS

## PERIN BROS. CINCINNATI OHIO WANT CORN

**Cedar Rapids Grain Co.**  
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CEDAR RAPIDS, IOWA

**FEED MIXERS**  
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Telegraph collect for delivered prices.  
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Call us for track bids  
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**CUBA**  
WE CAN SELL YOUR  
**CORN, OATS**  
and OTHER GRAINS  
advantageously through-  
out the Island of Cuba.

**JAQUITH, PARKER, SMITH & CO.**  
708 Cham. of Com., BOSTON, MASS.  
We buy all kinds of Grain and Mill  
Feed, Sample Feed Wheat, Barley,  
Milo, Kaffir Corn, etc., for  
NEW ENGLAND AND EASTERN TRADE  
Send Samples—Write Us

ALL KINDS  
**SALVAGE GRAIN**  
WANTED  
Submit Samples Get Our Bids  
**Stockbridge Elevator Co.**  
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WHEAT—CORN—OATS—RYE—BEANS  
1548 Penobscot Bldg., DETROIT  
"CONSIGN TO CARSON"

Solicit inquiries for Natural and Kiln  
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Ask for Prices

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FORT WORTH, TEXAS  
GRAIN, FEEDS, FIELD SEEDS AND PUBLIC STORAGE  
KAFIR, MILO MAIZE, FETERITA  
Wire or Write Us to Sell or Buy

CONSIGN  
**WHEAT - CORN - OATS**  
—To—  
**DUMONT, ROBERTS & CO.**  
301-2 Cham. of Com., DETROIT  
"The top o' the market to you."



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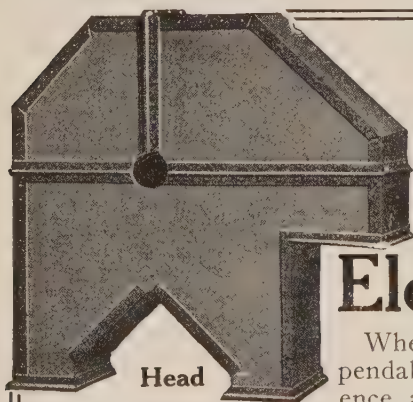
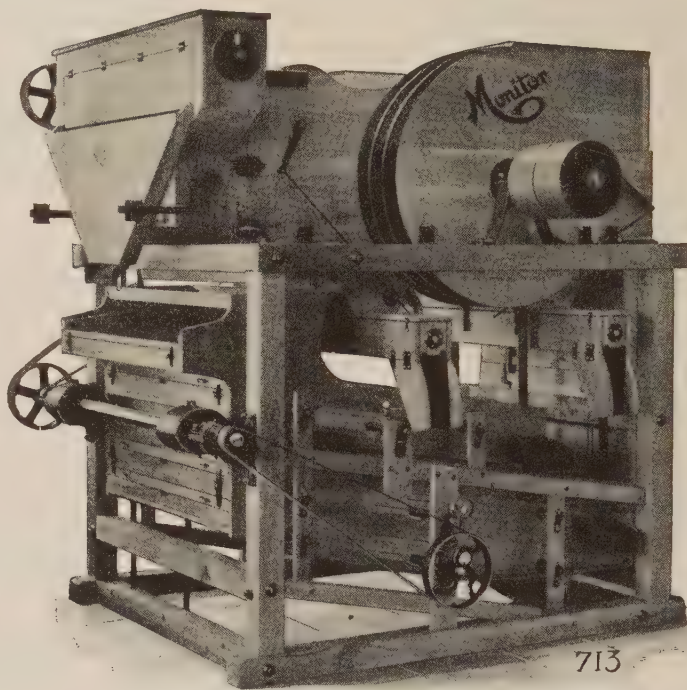
## "DON'T KNOW BEANS"

It was raised on beans and it knows them perfectly. It exercises this knowledge and takes out the poor beans, delivering good stock for shipping.

**IT POLISHES—  
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GRADES  
BEANS or PEAS**

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**Silver Creek  
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CANADIAN PLANT:  
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Where economy, dependability and permanence are demanded get our quotations on Dust Tight Steel Elevators. Built according to standard designs or your specifications.

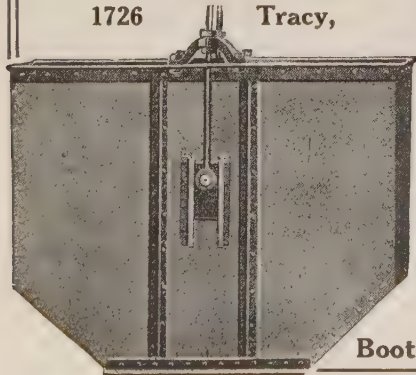
**Standard Steel Works**

Successors to The Ell-Kay Mfg. Co.

1726

Tracy,

Kansas City, Mo.



We manufacture Steel Elevator Casings, Heads, Legging boots, Buckets, Spouts, Dust Collectors, and all Steel and Galvanized Elevator Equipment.

## WHAT DO YOU NEED?

to modernize your plant so it will minimize your labor and increase your profits? Is it here?

- |                     |                             |
|---------------------|-----------------------------|
| Account Books       | Gravity Cleaner             |
| Attrition Mill      | Herringbone Reduction Gears |
| Bag Closing Machine | Lightning Rods              |
| Bags and Burlap     | Manlift                     |
| Bearings { Ball     | Moisture Testers            |
| { Roller            | Oat Bleachers and Purifiers |
| Belt                | Oat Clipper                 |
| Boots               | Painting or Repairing       |
| Buckets             | Portable Elevator           |
| Car Liners          | Power { Gas Engine          |
| Car Loader          | { Kerosene Engine           |
| Car Mover           | { Motors                    |
| Car Puller          | Power Shovel                |
| Car Seals           | Renewable Fuse              |
| Cleaner             | Rolls for Cracking Corn     |
| Clover Huller       | Sample Envelopes            |
| Coal Conveyor       | Scales                      |
| Conveying Machinery | Scarifying Machine          |
| Distributor         | Self Contained Flour Mill   |
| Dockage Tester      | Separator                   |
| Dump { Auto Truck   | Sheller                     |
| { Wagon             | Sieves                      |
| Dump Controller     | Siding-Roofing { Asbestos   |
| Dust Collector      | { Steel                     |
| Elevator Leg        | Silent Chain Drive          |
| Elevator Paint      | Spouting                    |
| Feed Mill           | Storage Tanks               |
| Fire Barrels        | Testing Apparatus           |
| Grain Driers        | Transmission Machinery      |
| Grain Tables        | Transmission Rope           |

or anything used in a grain elevator.

Draw a line through the supplies wanted, and write us regarding your contemplated improvements or changes. We will place you in communication with reputable firms specializing in what you need, to the end that you will receive information regarding the latest and best.

## INFORMATION BURO

Grain Dealers Journal, 305 So. La Salle St., Chicago





## Don't Let the Shovel Hold You Down Any Longer

The coal shovel is your worst enemy.  
It is eating up your hard earned profits.  
It is breaking up your coal—depreciating its value.  
It is limiting you to a **little** business when you should have a **big** business.  
It is **now** an unnecessary, and therefore a **sinful** waste.  
For the

## Godfrey Conveyor

will load your wagons or trucks—a ton at a time—in a few minutes, at a cost of perhaps 3c to 4c a ton—or **one tenth** the cost of shoveling.

One man unloads a 40 ton car in 80 to 90 minutes and deposits the coal in piles, in bunkers, or in trucks or wagons.

**It pays 100% a year in most instances**

Did anything else you ever bought pay you such dividends?

And while it is putting money into your pocket, it is allowing you to expand your business—to make such efficient use of motor trucks that you can control the country trade for miles in every direction.

**It makes you the master of  
the local coal business**

by permitting you to give quicker service and to deliver clean, whole coal rather than dirty **broken** coal.

Let us be your “efficiency engineers”—showing you how you can declare your independence from the shovel “holdup.”

**GODFREY  
Conveyor Co.  
Elkhart, Ind.**

Please send catalog  
of your Conveyor. We  
do a business of.....  
tons a year and would like  
to increase it.

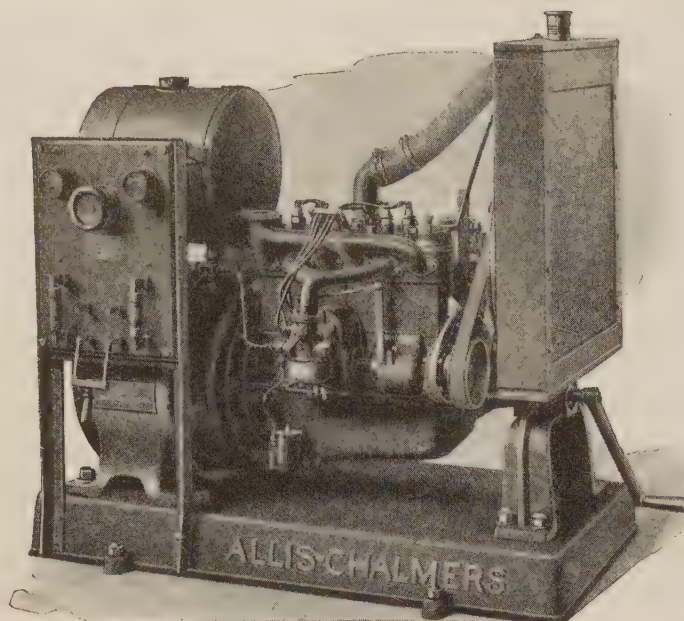
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## Godfrey Conveyor Company

Elkhart, Indiana, U. S. A.





## Electric Light and Power

### For the Community

*A Reliable, Portable,  
Independent Source  
of Electric Power.*

These sets are self contained and are furnished complete including gasoline engine, electric generator, switchboard and all auxiliaries. They are ready to operate upon the addition of fuel, lubricating oil and cooling water.

Ratings: 5 k. w. and 15 k w., 110 or 250 volts.

These sets were developed for the U. S. Government and large numbers used for various war purposes.

*Send for leaflets 2040 and 2041*



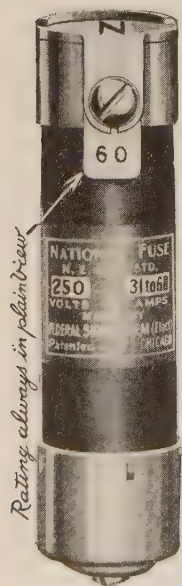
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Electrical Machinery  
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**ALLIS-CHALMERS**  
MANUFACTURING COMPANY  
Milwaukee, Wisconsin, U. S. A.  
*District Offices in All Leading Cities*

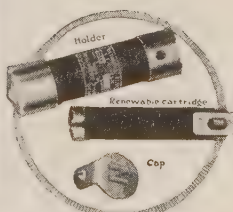


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Flour and Saw Mill Machinery  
Power Transmission Machinery  
Pumping Engines-Centrifugal Pumps  
Steam and Electric Hoists  
Air Compressors - Air Brakes  
Agricultural Machinery  
Condensers

## Can the Fuse You Use Meet These Specifications?



*Only 3 Parts*



**I**S it renewable?

Is it so constructed that only one element can be used at a time?

Is the ferrule of the fuse you use securely riveted to the fibre casing?

Is your knife blade fuse and element provided with gas vents in the ends to allow gases to escape on short circuits?

Does the fuse you use contain an inner cartridge consisting of a fuse element and powder packed filler, and an outer fibre cartridge with suitable and substantial contact, with a permanent method of indentifying capacity of renewable element without opening the outer cartridge?

Has it been approved by the Underwriters' Laboratories and rated by them as complying with the standard requirements as regards rating and short circuit performance?

Has the fuse you use been approved by the Mutual Fire Prevention Bureau of the Millers' Mutual Fire Insurance Companies?

Has it been approved by the Associated Factory Mutual Fire Insurance Companies?

The National Renewable Fuse is the **ONLY** renewable fuse approved by all of the following:

- 1—Underwriters' Laboratories, Inc. (their report E 4199—Dec. 1, 1919)
- 2—Associated Factory Mutual Fire Insurance Companies.
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We make a complete line of renewable fuses, both 250 and 600 volts, up to and including 600 amperes. Write us today for complete information and prices of the National Renewable Fuse, mentioning your dealer's (or jobber's) name and address.

### Federal Electric Company

Representing Federal Sign System (Electric)

LAKE AND DESPLAINES STREETS, CHICAGO

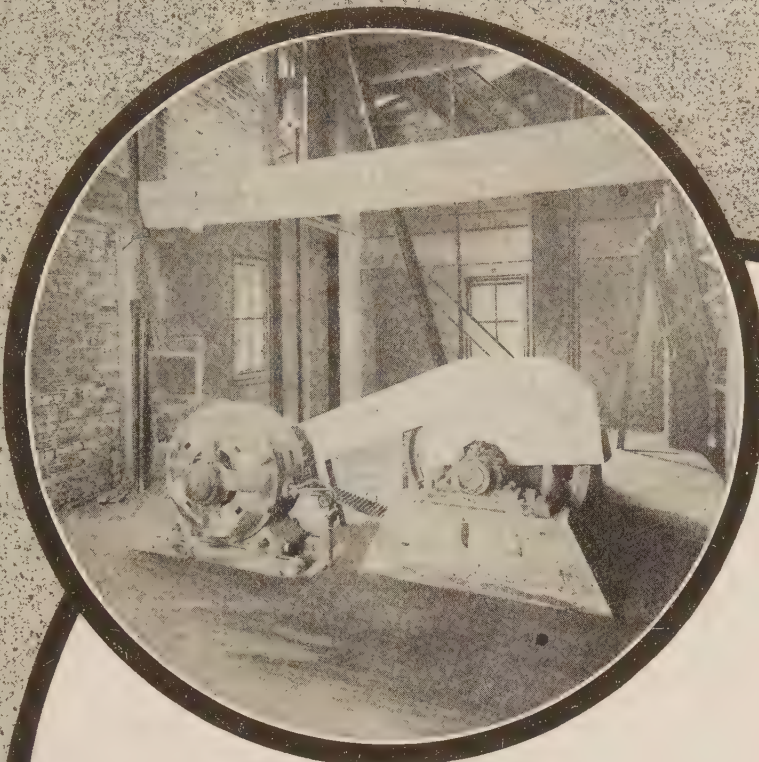
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# Endurance — — —



Westinghouse type "CS" motors when used to operate elevators, grinders, conveyors, scourers, separators, packers and other grain elevator or flour mill equipment secures you a reliable drive.

These motors are of few parts and simple in construction. The shaft and bearings are of generous proportions with form wound coils thoroughly insulated, well braced winding and practically indestructible rotor.

This means Endurance —

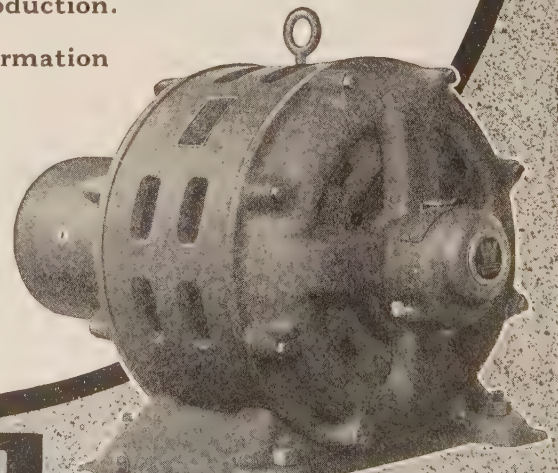
That lasting quality which only the best can produce.

With Westinghouse apparatus installed, your power troubles are few and your time can be devoted to production.

Our nearest office will furnish further information upon application.

WESTINGHOUSE ELECTRIC &  
MANUFACTURING COMPANY  
East Pittsburgh, Pa.

*Sales Offices in All Large  
American Cities.*



816

# Westinghouse



# "The Standard" Scales

Accurate—Rapid—Durable

Have you ever considered how much time you can save by using scales that are always right—always ready to go? We take pride in having—in scales—a kind for every use and a size for every need, when we talk to the grain elevator man.

"THE STANDARD" Automatic Dial Scale, shown here, is one adapted to the needs of every elevator—regardless of its size. It is made entirely of metal and is delivered ready to install. Obtainable in every size.



Automatic Dial Scale

Send for Catalog No. 73. It tells more about "THE STANDARD" Scales

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**Bauer's**  
SCIENTIFIC

"Scientific" Attrition Mill.  
THE BAUER BROS. CO.  
SPRINGFIELD OHIO U.S.A.

Motor Driven Attrition Mill;  
also made for Belt Drive.

### The Bauer Attrition Mill;

#### An Ideal Feed Grinding Mill

Uniform Product, fine or coarse. Ball-bearings, easy running.  
Requires minimum lubrication. Instant adjustments.  
Quick change of Plates. "Scientific" Grinding Plates.  
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Investigate these points and you'll find others,  
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### THE BAUER BROS. CO.

506 Bauer Bldg. Springfield, Ohio, U. S. A.

We also make Single Disc Mills, Centrifugal  
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SCIENTIFIC

## What's Wanted?

The quickest way to supply your needs is to tell your wants to a sympathetic audience.

The "Wanted and For Sale" pages of the GRAIN DEALERS JOURNAL affords the largest and best medium to make your wants known.

Read what satisfied customers say:

"It pays to advertise in the GRAIN DEALERS JOURNAL. We have had several desirable replies to our Help Wanted ad."—Bloomington Mills, Bloomington, Ill.

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The charge for such announcements is small—only 25c per type line. Try them. Supply your needs quickly.

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305 So. La Salle St.

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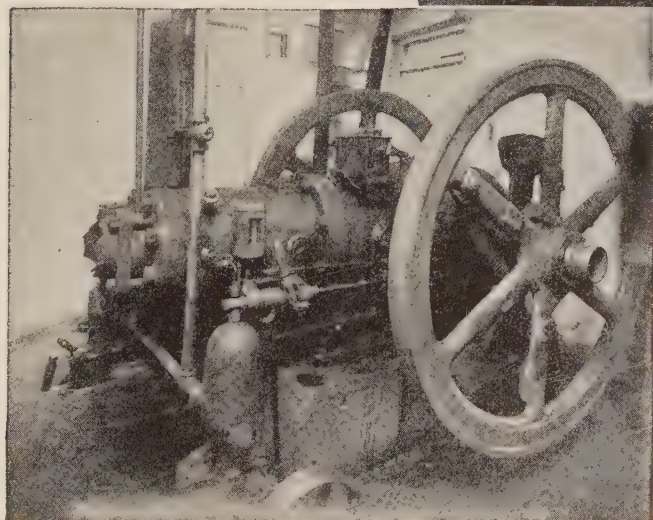


## ***The Stanford Elevator Now Gets Power From Low-Priced fuel Oils***

by using a Fairbanks-Morse  
20 H. P. "Y" Oil Engine.

"We consider it first class in all respects" says Wm. H. Springer, Manager of the Stanford Grain Company. "We only use about one-half as much coal oil as we did gasoline with other makes of engines, and have no bother with it in any way since installed."

"Y" oil engines—10 H. P. to 200 H. P. — give unfailing power from a wide variety of low priced fuel oils. Guaranteed by Fairbanks-Morse Quality.



Simple in construction — practically automatic in operation and regulation—no carburetors, valves, batteries, mixers, timers, igniters, magnetos, switches or spark plugs.

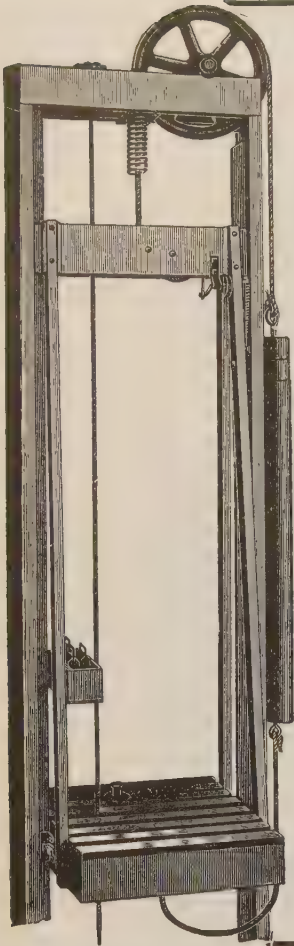
Special quick-starting device; air seal prevents oil from being blown out of bearings; positive lubrication; sensitive governor. No hot bulb—no water injection.

# **Fairbanks, Morse & Co.**

MANUFACTURERS CHICAGO

Engines - Pumps - Motors - Hoists - Air Compressors - Fairbanks Scales - Sheffield Motor Cars - Stand pipes - Coalng Stations - Etc.





## HOW MANY TRIPS

to the top of your elevator do you make in the course of a year? Make an estimate, and then figure the amount of time and energy you are wasting if your house is not fitted with an easily and cheaply operated manlift.

## The CONSTANT SAFETY MANLIFT

will eliminate this waste—and it will make the elevator and the elevator operator more efficient in every way. It is the easiest and cheapest operated manlift on the market, and the safest as well. The sheave over which the rope operates is fitted with ball bearings; the brakes are adjustable and positive in their action, and they work independently of each other. All material is carefully selected for the work it is to perform.

*A manlift reduces the insurance rate.*

Ask for our catalog of elevator machinery.

### B. S. CONSTANT MFG. CO.

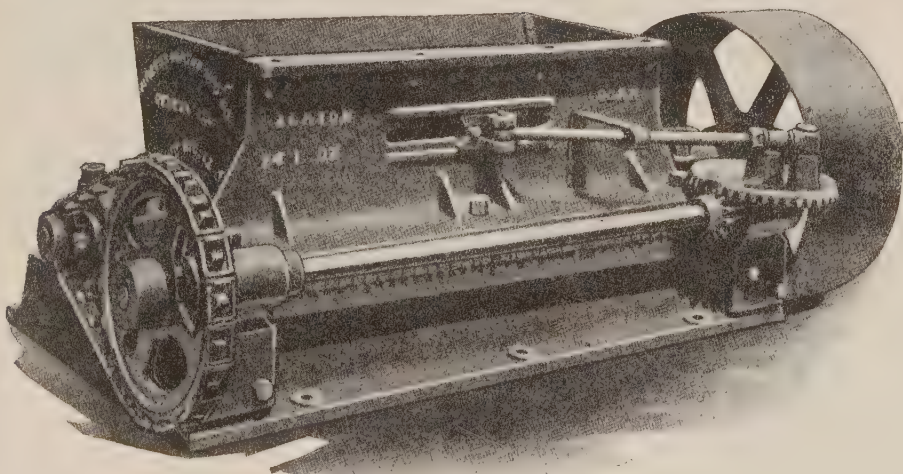
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## KENYON Ear Cob Crusher

No irons such as horseshoes, hammers, etc., can stop this machine. Positively eliminates all corn crushing troubles.

Send for catalog and all details.

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### TYDEN CAR SEALS

Bearing shipper's name and consecutive numbers.

Prevent  
**CLAIM LOSSES**  
**10,000 SHIPPERS**  
Are now using them.

Write for samples and prices.

**INTERNATIONAL SEAL & LOCK CO.**

Chas. J. Webb, Vice-President  
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### CONE-SHAPE GRINDERS

**It PAYS to GRIND ALL GRAINS**

Look to the Grinders. They do the work! Bowsher's Cone-Shape grinders are the correct principle in Feed Mill construction. They mean larger grinding surface close to center of Shaft; thus More Capacity, Lighter Draft, Longer Life.

"Desire to express my appreciation of the long-lasting, trouble-proof Bowsher. Have used a No. 4 ten years with less than One Dollar per year for repairs." *E. W. Watt, Jacobburg, O.*

10 sizes; 2 to 25 H. P. Write for free catalogue.

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by advertising directly to people who want to buy, by using a

**Grain Dealers Journal  
Want Ad.**





## With SELDEN Trucks Their Business Increased

*The Magazine*  
 "TRUCK  
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 will be sent free to all  
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 CORPORATION  
 Rochester, N. Y.



"OUR company was established in 1903, and for several years thereafter we conducted what we thought to be a successful business. We used one team of horses. Then we purchased a Selden Truck and now we have two and our business has increased wonderfully because of our ability to give better service to our customers. Now, with our new offices and warehouses, we continue to grow. Of course, the highly efficient service rendered by our Selden Trucks has played its part and will continue to play its part in the future as it has in the past. Without our Selden Trucks to-day we would be practically standing still. We think Selden Trucks are the best trucks on the market."

The opinion of this operator of Selden Trucks is typical of the many we receive daily.

Many businesses have been built with the aid of Selden Trucks. These businesses have profited by the highly efficient and economical haulage service secured from Selden "In-built Quality" construction.

Ship by Truck  
 —SELDEN Truck

1½, 2½, 3½, 5 Ton Models—All Worm Drive

# Selden Motor Trucks

SELDEN TRUCK CORPORATION, Rochester, N. Y., U. S. A.



**NOW Is The Time to  
Make Sure of Your  
Season's Supply Of**

**Ankorite**  
STEEL FENCE POSTS

The tremendous demand for Ankorite Steel Drive Posts from our established dealers makes it impossible for us to promise immediate shipment on new orders. We are doing our utmost to meet the situation, but even our tremendous facilities are being severely taxed.

But you still have time to get a supply of Ankorite Posts for the best part of the selling season.—IF YOU WILL LET US KNOW YOUR REQUIREMENTS AT ONCE.

The fencing season is almost at hand. Farmers want Ankorite Steel Drive Posts, and they want to buy them from you if you have them in stock. But

they can't afford to wait—if you can't supply them with Ankorites they'll seek them elsewhere or look for a substitute.

We have a tremendously effective sales plan that is helping Ankorite dealers sell posts as they never sold them before. Write for 24-page Prospectus and details of exclusive sales proposition.

But above all—GET THE POSTS!

**To Ankorite Dealers:** If the supply of posts which you have ordered is not sufficient to meet your entire season's demand, by all means place your order at once for your additional requirements, so you may be absolutely sure of having the posts when you need them. This is merely for your protection and to avoid the possibility of disappointment on belated orders.

**Two Sturdy Ankorite Types—Angle Post and "T" Post. Be Sure to State Which Type Is Wanted.**

**Calumet Steel Co., 208 S. La Salle St., Dept. 9, Chicago**

**OUR MOTTO** for over twenty-five years has been **SAFETY FIRST.** During these years we have equipped thousands of elevators with dust collecting systems to

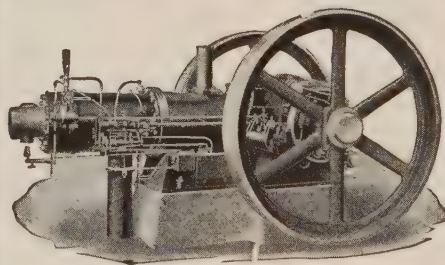


prevent explosions, and no dust explosion or loss of a single life has occurred in one of them.

Your liability attaches if you don't protect your elevator and workers.

FOR CATALOGUE, WRITE

**THE DAY COMPANY**  
Minneapolis, Minn.



**Real Power—**

the kind that you can depend upon at all times is the kind of power you want. And you want this kind of power as cheap as you can get it. The MUNCIE OIL Engine for many years has been giving satisfactory service in hundreds of country elevators. It should be in yours. Here are a few

**Reasons Why**

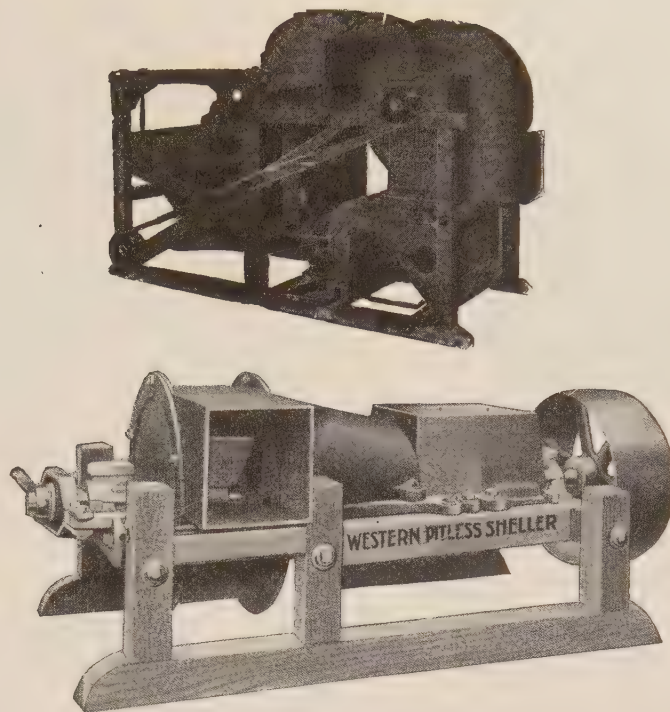
Lower Fuel Cost  
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No Shut-downs  
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Steadier Power  
Greater Reliability  
Fewer Repairs  
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Write for proof of facts.

**MUNCIE OIL ENGINE CO.**  
518 JACKSON STREET MUNCIE, IND.





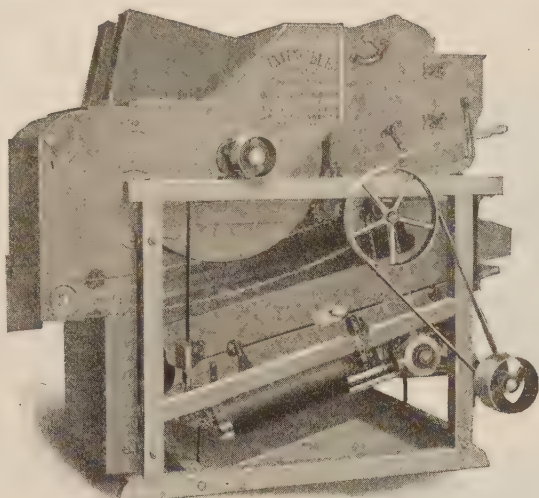
## ELEVATOR MACHINERY

The "Western Line" comprises everything needed to make an Elevator efficient and successful. We manufacture all necessary equipment, and anticipate fully the requirements of the trade. If your plant is fitted with "Western" machinery there is little fear of difficulty, but whenever things go wrong this is the house to supply you. We employ a large force of expert engineers and have spent a lifetime studying the needs of Elevator requirements. All our facilities are at your disposal.

Forty years of continuous service and satisfaction have given the "Western" line a reputation for reliability and performance that to us is as good as a copyright. Whenever equipment is wanted in a hurry we are at our best, and if you've the slightest idea of remodeling or repairing, get in touch with us at once. You should have our new catalog for quick references, and always think "Western" when in need of any elevator equipment.

### UNION IRON WORKS, Decatur, Ill.





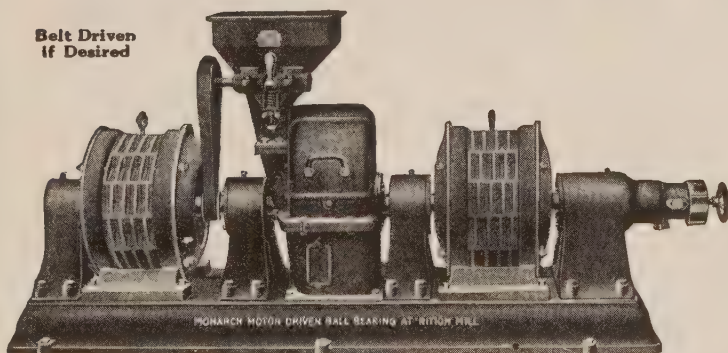
Recognized as the ONE dependable Receiving Separator — Fully described in bulletin No. 24—Write for it.

**Invincible Grain Cleaner Co.**  
SILVER CREEK, N. Y.

## MONARCH BALL-BEARING ATTRITION MILL

*Designed and Built to Perform Satisfactorily  
All Attrition Mill Requirements*

Belt Driven  
if Desired



Years of experience in attrition mill building has enabled us to produce this "Monarch" of all Attrition Mills.

It, in your elevator, will pay big interest to you by

### **Saving Labor and Time**

It is trouble-proof, delay-proof and has dust-proof bearings that require the minimum of lubrication and attention.

Write us today—now—for full particulars and descriptive catalog.

We have much interesting information about feed grinding that we will gladly furnish you.

*Write for Catalog No. D115*

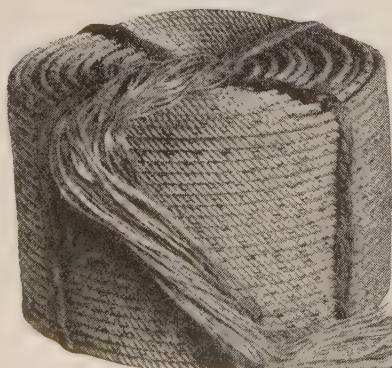
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**Mill Builders and Milling Engineers**

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## FROM MAINSHAFT TO HEAD

Specify AJAX TRANSMISSION Rope in your elevator. Designed especially for grain elevators, it is the best rope the most skillful workmanship can produce. Delays caused by broken transmission rope are expensive. Specify AJAX on your next job.

**H. CHANNON CO.**

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**AJAX**





# Save Your GRAIN

USE

## *Safeguard Your Profits*

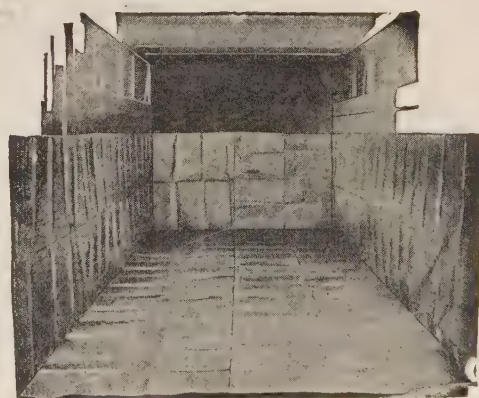
by cooping bad order cars that are sure to be given you during the coming shipping season. The above illustration is a fair example of what you can expect if you load your grain without first making sure that all holes are covered.

Kennedy Car Liners afford the most advantageous, inexpensive and yet the most efficient method of preventing leakage of grain in transit.

You will soon have a new crop to handle. Do not wait until the rush of harvest comes and an out-of-condition car is offered you for grain loading. Demand for Kennedy Car Liners is heavy this season and we advise getting orders placed now and be assured of early deliveries.

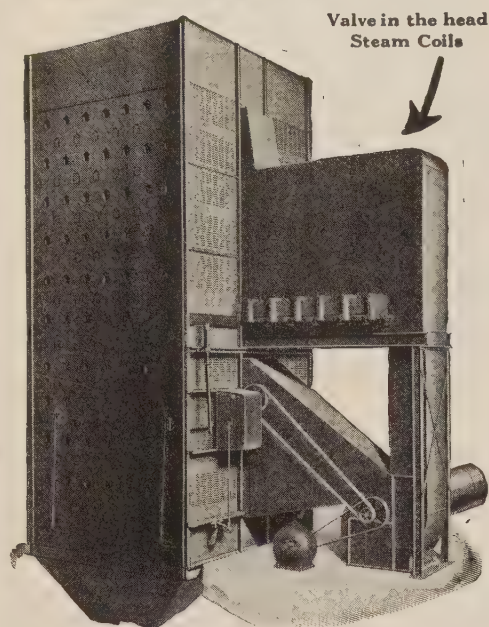
**The Kennedy Car Liner & Bag Co.**  
Shelbyville, Ind.

# KENNEDY CAR LINERS





# MORRIS GRAIN DRIERS



"MORRIS DRIED SAME AS SUN DRIED"

**BE PREPARED  
BUY AN AUTOMATIC MORRIS**  
You will use it every year.

## **AUTOMATIC and SELF CONTAINED**

Requiring no special Building Construction. The Automatic feature eliminates an attendant. This is the simplest, most positive and uniform drier made. Write for detailed information and you will BUY A MORRIS if you buy a Drier.

**We Also Offer a Complete Line of  
Direct Heat Driers**

## **THE STRONG-SCOTT MFG. COMPANY**

*"Everything for Every Mill and Elevator"*

GREAT FALLS

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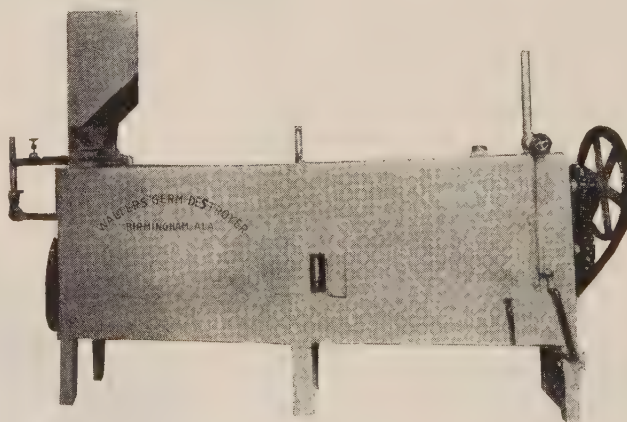
WINNIPEG

## **—for Destroying Weevil**

**A machine for which the grain trade has been waiting, one that kills the weevil and germ without increasing fire hazard**

### **CONSTRUCTION**

The construction of the machine is steel and sheet iron thruout. The grain is fed from a hopper above and passes thru three conveyor housings and out the lower right hand end. The plant is motor-driven and steam is used to secure the heat for making the process. It stands 5 ft. 2 inches from floor to top, and 11 ft. 6 inches in length.



### **OPERATION**

The operation consists of three motor-driven screw conveyors which are fed from hopper in upper left hand corner. A breath of steam is allowed to come in contact with the grain as it enters the first conveyor housing. The temperature, regulated to suit, from 155° to 160° does the work of killing the weevil and germs. Little motor power is required to run the machine and a 45-lb. steam boiler is sufficient to supply the heat required.

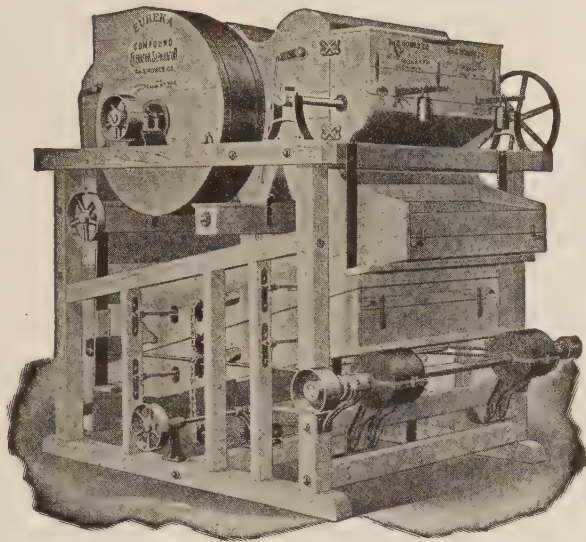
MANUFACTURED BY

## **James Walters' Germ Destroyer Co.**

2009 MORRIS AVENUE



BIRMINGHAM, ALABAMA





*Ask someone who owns one*

From a purely  
"RESULTS-DELIVERED"  
standpoint no machine  
quite compares with the  
"SERVICE-FULL"  
"EUREKA"

It's a really-truly worth-more  
 **GRAIN CLEANER** 

MAY WE SEND YOU COPY OF OUR NEW BOOK (No. 75)?

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requires little of your time for filing, and contains spaces for all the necessary information in the order which assures prompt attention on the part of the claim agent. It increases and hastens your returns by helping you to prove your claims and by helping the claim agent to justify payment.

These claim blanks are printed on bond paper, bound in book form, each book containing 100 originals and 100 duplicates, a two page index, instructions and summary showing just which claims have not been paid, and four sheets of carbon. You tear out the original to send to the claim agent, and the carbon copy remains in the book, as a record of your claim.

- A is for Loss of Weight in Transit Claims.
- B—Loss in Market Value Due to Delay in Transit.
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Forms A, B, C, D and E are well bound in three books, as follows:

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Send all orders to

**GRAIN DEALERS JOURNAL**

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Belts punched accurately for buckets if desired. Estimates for elevator equipments cheerfully furnished.

## New York Belting & Packing Co.



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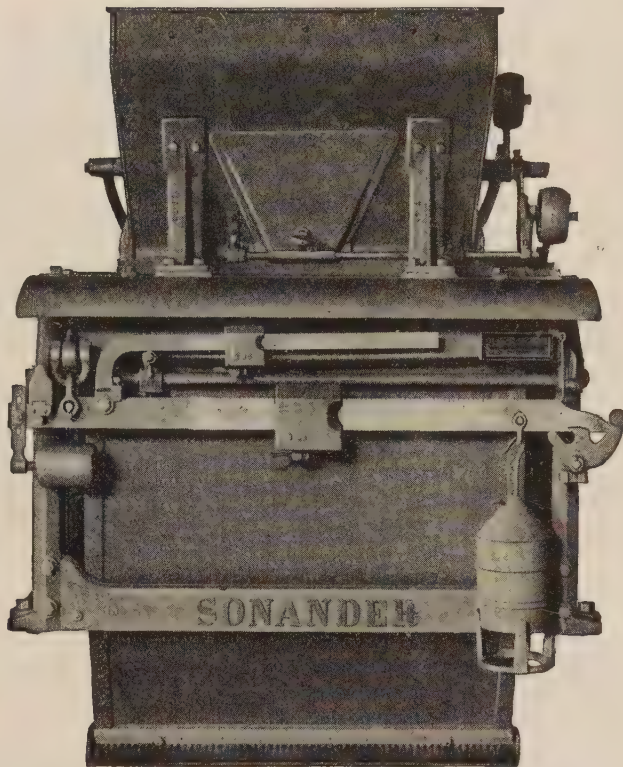
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CHICAGO, ILL.

2d Ave. N. and 3d Street  
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ST. LOUIS, MO.



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The whole object of weighing is to know accurately the quantity; and any such system is useless, unless it will determine this minutely and definitely. Science and mechanical practice has demonstrated that for this purpose, the scale lever is the best for determining the fractions of pounds, and this is the principle of the

## Howe-Sonander Automatic Scale

When buying a scale be sure to inquire and investigate this scale. Its simplicity and accuracy will suit your every purpose, and save money for you. Any of the offices below will be glad to give you complete information. Address the one nearest you.

### Howe Scale Co. of Illinois

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ST. LOUIS, MO., 512-514 St. Charles St.  
KANSAS CITY, MO., 1510 Main St. W. C. Peak, Mgr.  
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## MORE HUMPHREY SERVICE BELT ELEVATORS



than ever before are being installed all over the world.

### SAVES--TIME--MEN--MONEY

A necessity in any mill or elevator with two or more floors. Quickly conveys men and sacks of grain or flour from one floor to another.

**SPEEDS UP PRODUCTION,** enabling your men to do more work in the same time without useless waste of energy.

Write for Bulletin D-5 giving detailed information why the "Humphrey Endless Belt Route" means bigger profits to the owner.

If not made by Humphrey Elevator Co. it is not a "Humphrey."  
The original standard belt man lift for 30 years.

**HUMPHREY ELEVATOR CO.,** Sole Manufacturer  
Fairbault, Minnesota

## IMPROVED DUPLICATING GRAIN TICKETS

With the use of Form 19GT as a scale book much time and labor will be saved as one writing with the use of carbon will give you a complete record and at the same time, a ticket will be ready for the hauler. Very convenient for dealers who regularly issue scale tickets for each load of grain received. Chance of error will be minimized as both the ticket and office record will be the same.

This book contains 250 leaves. Each of the 125 original leaves bears four scale tickets, is machine perforated, printed on white bond, size of tickets 3x6 $\frac{3}{8}$  inches. The 125 duplicates are printed on manila, but not perforated. Check bound at top of tickets with hinge top cover, 500 tickets in each book arranged horizontally. Size of book 7 $\frac{1}{2}$ x12 inches, each book supplied with 5 sheets of carbon.

The printing is crosswise the ticket and has spaces for the following record: "Owner, Hauler, Grain, Grade and Dockage, Gross, Tare, Net, Total Dockage, Net Pounds, Bushels, Price and Amount, Storage Ticket No., Station Ticket No., and Date, Weigher, Name of Firm or Buyer."

Form 19GT Duplicating (200 pages) \$1.35

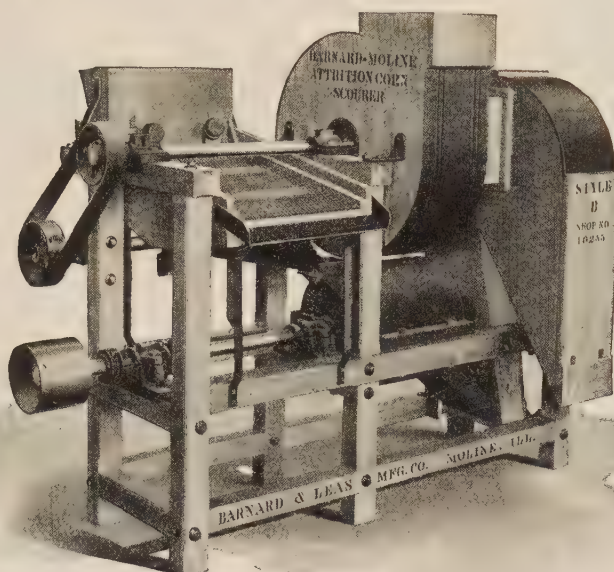
Form 19GT Triplicating (300 pages) 1.75

### GRAIN DEALERS JOURNAL

305 So. La Salle St.

Chicago, Ill.





## Scour Your Corn With A

### *Barnard-Moline Attrition Corn Scourer*

THE most valuable feature claimed by users of this wonderful, efficient corn scourer is the fact that it scours corn without waste and without fracturing or abrading the tender, starchy and most valuable end of the kernels of corn, which are wasted or damaged when scoured with machines of the revolving beater type.

The degree of scouring is fully controllable by means of an adjustable, automatic, valued outlet. The interior revolving, agitating mechanism gives each kernel of grain a rapid, rolling or revolving motion. The friction of the corn against itself and against the walls of the case removes all adhering foreign substance as well as black caps and the hard, woody tips.

*Write Today for Bulletin No. 15-A for Full Details*

## Barnard & Leas Manufacturing Co.

*"Builders of High-Grade Mill and Elevator Machinery Since 1860"*

MOLINE, ILLINOIS, U. S. A.





# "Hamilton Made"

## When Equipping New or Rebelting Your Elevator

Investigate and Buy "Hamilton Made" Rubber Belts. There are two standard brands: INVINCIBLE for Cleaners, Shellers, Clippers, Separators, Feed Mills, Car Pullers, Engine and Motor Drives. LAKEWOOD for Legs and Conveyors. *Write for particulars.*

**HAMILTON RUBBER MFG. CO., 218 No. Wells Street, CHICAGO**

FACTORIES: Trenton, N. J. BRANCHES: New York and Philadelphia

## Elevator Buckets for High Speeds

### Minneapolis "V"



This bucket, "Minneapolis V," enjoys preference by those who desire efficiency in elevating.

The "V" bucket can be set closer on the belt, and run at greater speed and greater capacity than any other bucket.

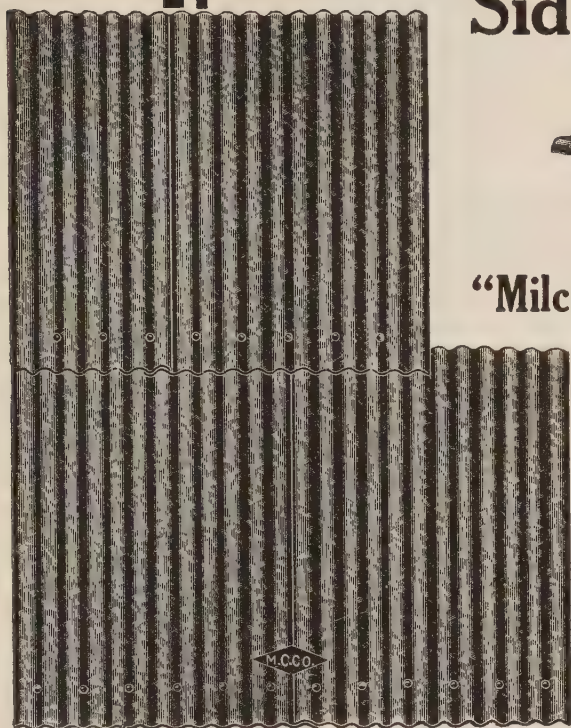
The fact that it delivers the entire load at head when loaded to full capacity should convince you, as it has others, that it is the solution of the elevating problem, all other equipment in the leg being of equal value.

Send for our catalog No. 18 which gives more information about this bucket and our other grain handling machinery.

**Skillin & Richards Mfg. Co.** 4516-60 Cortland Street **Chicago**



# Fire Proof Roofing Siding and Awnings



## "Milcor" Sheet Metal Roofing

"Milcor" sheet metal roofing adjusts itself to expansion and contraction due to heat or cold. Made particularly for Grain Elevators, Mills and high buildings of similar character. Single or double curved awnings from 24 to 48 gauge—galvanized or painted.

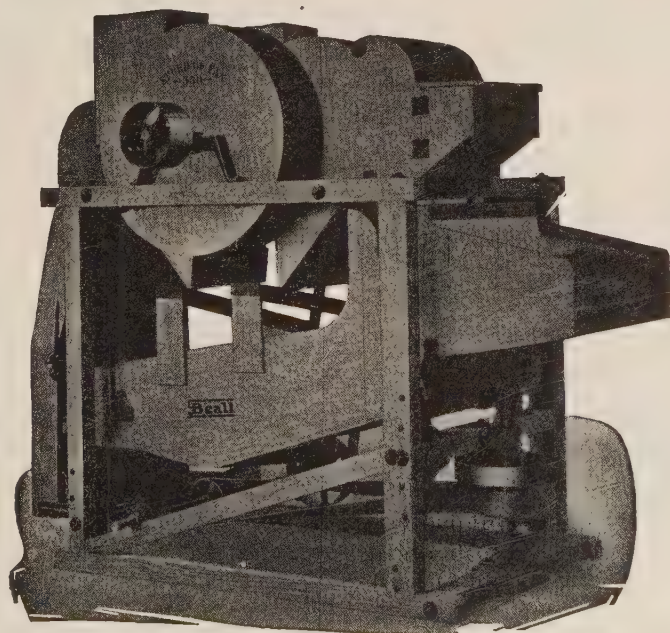
**Milwaukee Corrugating Co.**  
Milwaukee, Wis.

Branch at  
Kansas City, Mo.

Minneapolis Sales Agency  
929 Lumber Exchange



# The Beall Grain Separator



*A splendidly balanced, smooth-running machine that pays a good profit wherever used, cleaning grain evenly and thoroughly with two independent fans.*

**Beall**  
THE MARK OF QUALITY

*The air system is as perfect as modern ingenuity can make it. Never any disappointment. Send for Catalogue giving full working particulars and complete details.*

**Beall Improvements Co., Decatur, Ill.**



For Accurate Moisture Tests  
use our Grain Dealers' Air  
Tight Cans for forwarding  
your grain samples.  
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### GIVES BEST PROTECTION

Because it is a natural and not an artificial mixture of the silica and graphite pigment. This pigment is practically inert and indestructible. Not affected by air, moisture, acids or alkalis. Produced at Dixon's Ticonderoga mines only. Write for Booklet No. 15-B.

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ESTABLISHED 1827



## After 11 Years of SERVICE

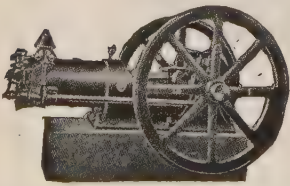
B. C. White, President and General Manager of The Buffalo Creek Sheep Co., Buffalo, Montana, bought an Otto Engine 11 years ago. This is an excerpt from a letter received from Mr. White recently:

"The engine is certainly a marvel of efficiency. I have run mine for eleven years on the ranch, and if it were possible for blundering ignorance to destroy it, mine would have been in the scrap pile long ago. The men have been sawing wood with it today, and altho the weather is cold, it humps right along."

Stock designs up to 50 H. P. are ready for early delivery.

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Rubber Protector, \$2.00

Sent postpaid on receipt price; or on trial to responsible parties. Has automatic valve and fine sponge.

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The dollars LOST by mixing grain through your "turn spout" accumulate rapidly.

The cheapness of the spout in first cost is a negligible factor unworthy serious consideration.

The profits from using a

## HALL SIGNALING DISTRIBUTOR

come in silently, invisibly, but regularly with the returns from your grain sales.

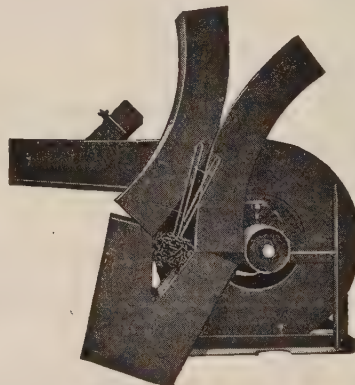
### HALL SPECIAL ELEVATOR LEG

DOUBLE THE CAPACITY  
NO MIXING GRAIN  
NO DANGER OF FIRES  
MARVELS OF SIMPLICITY

ONE-TENTH THE LABOR  
NO CHOKING BELTS  
THEY LAST FOREVER  
SENT ON TRIAL

**Hall Distributor Company, 222 Railway Exchange Bldg. Omaha, Nebr.**

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"We loaded 1,800 bu. of corn an hour the day before Thanksgiving. We never have to get in a dirty, dusty car to scoop grain."

Kennedy Elevator Co., Kenney, Ill.

"Your Boss Air Blast Loader is giving the best of service." G. N. Falknor & Son, West Milton, O.

"We would not think of going back to gravity." J. L. Baum & Son, Storms, O.

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### Load Even Ear Corn Without Scooping

We have Portable and Stationary outfits for loading even EAR corn as well as loose grain into railroad cars without scooping. Our Boss Crib Filler fills cribs with EAR corn by air. Compact, low down and shedded as easily as a binder. Quickly set up.

Write today for our new catalog "GOOD BYE SCOOP."

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Corn Grading  
Grain Moisture  
Bean and Seed Testing Scales  
Special Scales for Special Purposes



Style No. 6000  
Corn Grading Balance

### Torsion Balances

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No Knife-edges—No Friction—  
No Wear—No concealed  
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Used by U. S. Dept. of Agriculture, Testing Stations, Grain Elevators, Seedsmen, Commission Houses and Boards of Trade.

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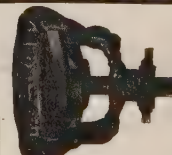
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is here. Now is the time to let the elevator man know you want his business. Advertise in the

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in your plant. These clutches will save you money power, time and trouble. Investigate today. A card brings our Free Booklet.

Decatur Foundry, Furnace & Machine Co., Dept. L, DECATUR, INDIANA

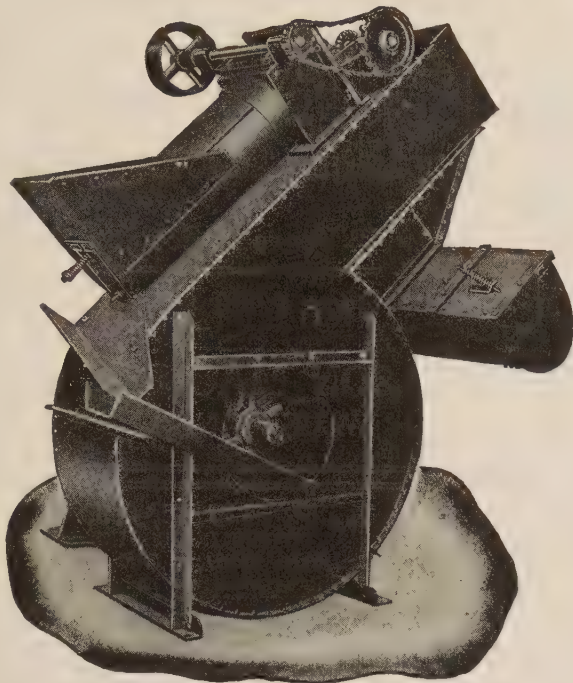


A better way  
to handle

**GRAIN** is the  
Bernert Way

**WHY?**

Because the Bernert way will save you money and make you money. With the Bernert Pneumatic Grain Handling Machines, whatever your grain handling problem today, will be no problem at all. The Bernert Pneumatic Machines handle all grain ahead of the fan; that's why there positively will be no grain damaged.



They are positively **NON-CHOKABLE**; that's why you will not have any trouble. They will at the same time they handle the grain, **MAKE BETTER GRAIN**. Let us tell you **WHY**. Simplicity of installation and efficiency in operation, handling any problem, is for what the Bernert Pneumatic Elevators, Combined Elevators and Loaders, Conveyors, Portable Track-loaders, Car-loaders, etc., stand.

For further information, write for catalog to the

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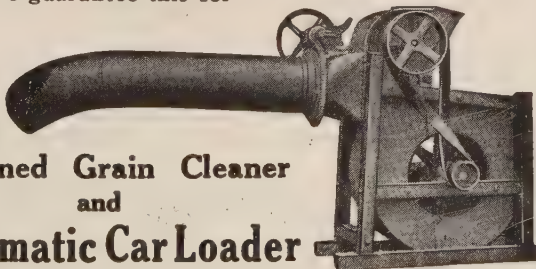
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Now that grain is commanding such a high price and there is and will be such a demand for it, it behooves you to load it properly, this means that you should use a Car Loader that cleans and loads at the same time. We guarantee this for

The  
Combined Grain Cleaner  
and  
Pneumatic Car Loader



It does not mill or crack the grain; fills cars to full capacity; strong, durable, requires no attention after starting. Cools and dries the grain as it passes through the air.

Used by hundreds of elevator owners. List of users will be sent you on request.

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**MATTOON GRAIN CONVEYOR CO., Mattoon, Ill.**

**NEW BADGER**  
**SLIP-PROOF**  
**SAFETY SPURS**  
**CAR MOVER**  
Insures Against Damage Claims



—the  
Slip-  
proof  
**Safety Spurs**

grip edges of rail, where they have not been polished hard by wheels. This double grip insures against mover slipping and allowing operator to fall and break his arms or injure his face as frequently happens with ordinary movers or crow-bars.

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Do not ever let yourself hear you say that, because there are very few modern elevators without me. **WHY?** Because they need service. Anything that is not serviceable in this day and age is soon discarded. But when you know me—I'm always at your service. I increase your business because of the easy manner in which I handle your dump. The Farmer likes me because I don't let the dump down jerky, and frighten his horses. I'm your friend. When you get tired of the old dumping device and are looking around for something 100% efficient, you'll find a friend in need is a friend indeed.



But don't wait too long.

Procrastination is the thief of time.

Now is the time.

Circulars upon request.

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## Elevator Equipment

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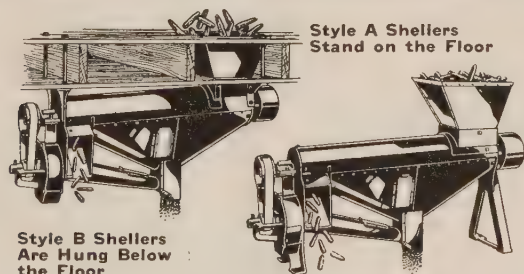
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right, my eighteen years experience  
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CORRESPONDENCE SOLICITED



Style B Shellers  
Are Hung Below  
the Floor

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**YOU CAN HANG  
TRIUMPH SHELLERS  
BELOW THE FLOOR  
OR STAND THEM  
ON THE FLOOR**

And either way they shell corn  
thoroly and cost little to run.

Bulletin giving sizes, capacities,  
and power requirements gladly  
mailed upon request.

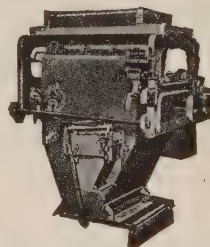
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Self Compensating, Operating and Adjusting

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be satisfied with a  
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Full Automatic?  
The Richardson is  
an Automatic that  
IS AUTOMATIC. It  
automatically does  
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being to do.



The NEW Richardson  
is SELF OPER-  
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of from twenty to  
sixty pounds to the  
bushel without  
change of adjust-  
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A book for the use of country grain buyers in keep-  
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dealers record oats receipts in front and corn receipts  
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Its column headings are: Month, Day, Name, Kind,  
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305 South La Salle St., CHICAGO, ILL.



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Any Size or Capacity  
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Contracts and Builds  
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We can furnish and install equipment in old or new elevators, guaranteeing greater capacity with less power, and positive Non-Chokable working leg.  
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We have the most complete organization in the Northwest for the construction of

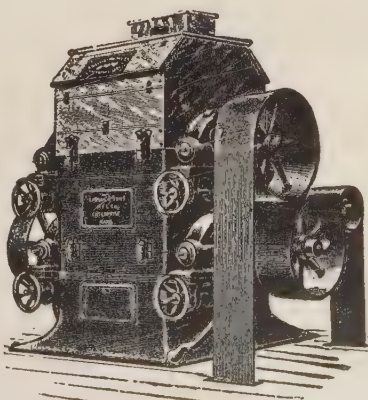
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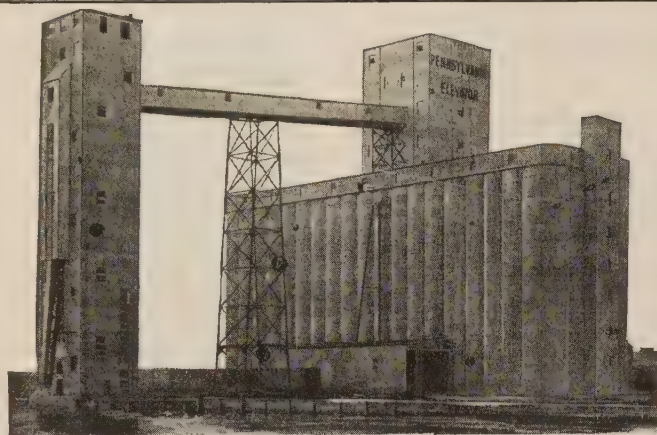


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The elevator owner who installs a feed mill in his elevator puts himself in line for more profits. No other sideline is as profitable. Your farmer patrons must have feed for stock. Are you going to let them go to your competitor? Drop us a line asking for further particulars regarding a feed mill for your elevator.

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assure you economical design, first class work, efficient operation.

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Concrete Central Buffalo, 4,500,000 Bu.

One of the modern houses which has made a record for rapid and economical handling

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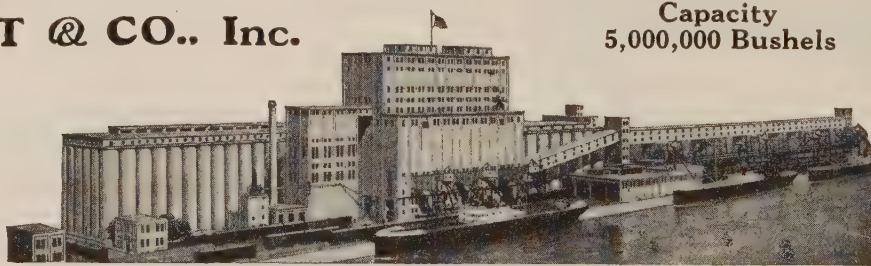
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Capacity  
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**GRAIN ELEVATORS**  
IN ALL PARTS OF THE WORLD  
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15th Floor, Westminster Bldg.  
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W. R. SINKS, Manager



"We have built for many of your friends. Eventually we will build for you. Why not now?"

## GROUP OF TERMINAL ELEVATORS BUILT RECENTLY BY US AT

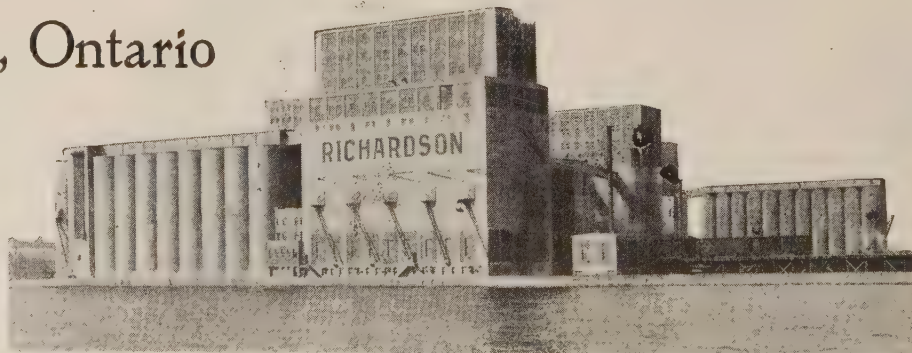
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FOR

The Grain Growers' Grain Company, Limited.  
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**THE BARNETT-McQUEEN COMPANY, LIMITED**

Designers and Builders of GRAIN ELEVATORS  
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1,500,000 Bushels

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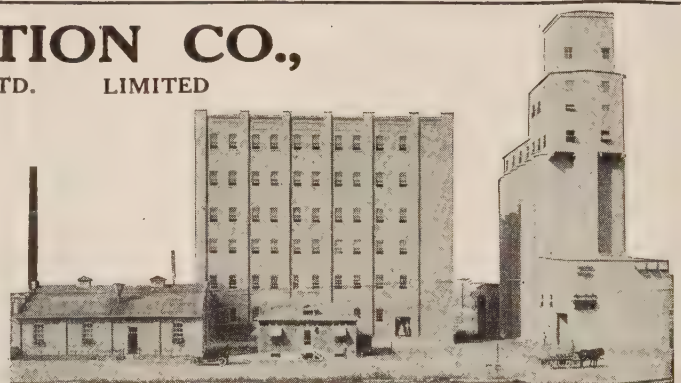
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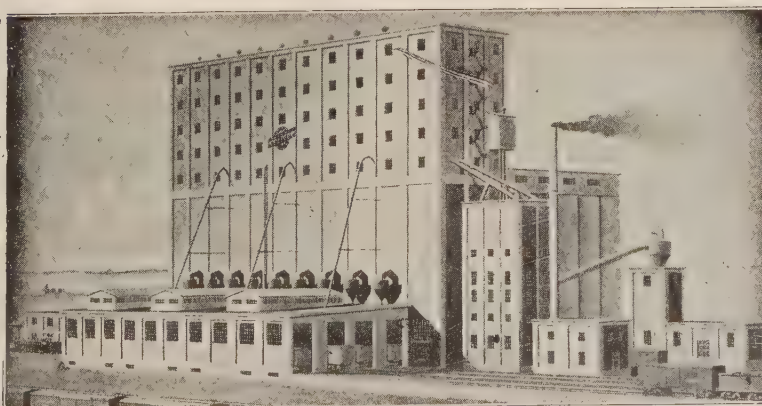
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is the latest acknowledgment of our  
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Builders of Modern, Fireproof  
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**1850** Barthelmy Thimmonier, after many unsuccessful attempts to prove to the natives of France the value of his machine, came to the United States and obtained a patent in 1850.

**1881** The first Union Special sewing machine was invented by I. C. Muther, D. W. Corey and Russel Green Woodward in 1881. This company was then organized and has since then played an important part in bringing the sewing machine to its present efficiency.

**1920** The sewing machine has become indispensable. It is a great factor in the clothing of humanity. And now Union Special Machines stand predominant.

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CHICAGO, ILLINOIS

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Adopted by  
U. S. GOVERNMENT

Present conditions demand accuracy in testing grain—your profits depend upon your tester. Don't take chances with experiments or guess.

THE  
**EMERSON**

is no experiment. It is used by all grain men and officials who must have accuracy at all times and under all

Can Be  
Furnished  
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NEW  
**KICKER**

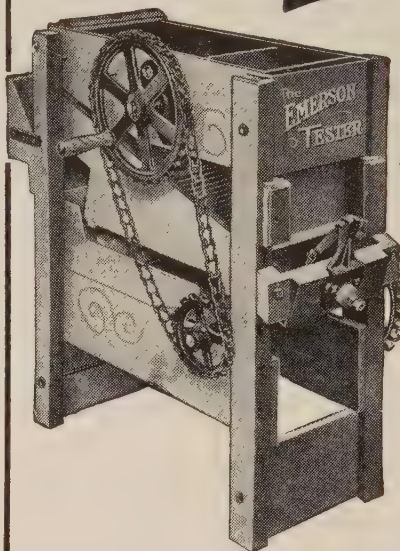
used by all grain men have accuracy at all conditions.

**10,000  
KICKERS**

are now in use—the Government uses over 100—Line, Independent and Farmers Elevators all over find the Emerson dependable and a sure enough arbiter between seller and buyer on any dockage dispute.

Write for pamphlet  
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This places the entire burden for any misunderstanding of your intentions upon the other party and protects you against the expensive misinterpretation of your trades.

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Fifty confirmations in triplicate, bound with pressboard and wire stitched, size 5½x8".

Order form No. 6 CB, Price 90 cts.

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305 S. La Salle Street  
CHICAGO

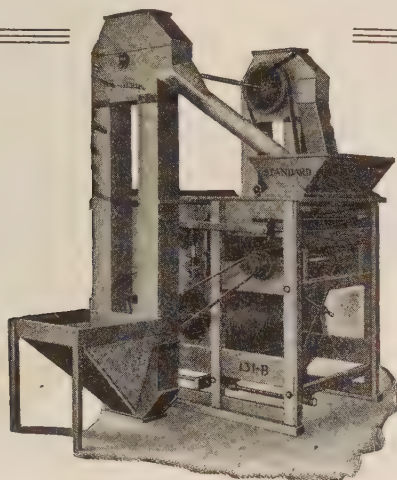


The Progressive Grain Dealer is interested in the cleaner that will effectually do his work with the least trouble and expense. In the

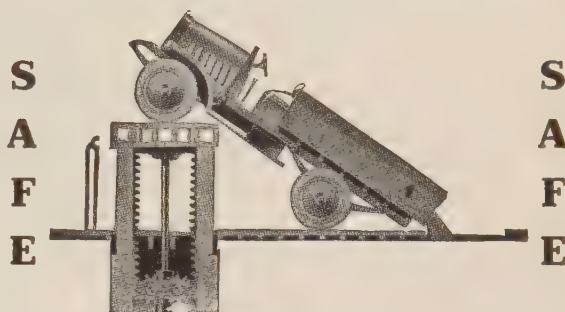
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you have embodied simplicity, economy and completeness. Write for our catalogue giving full particulars.

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## The Only Dump Made on Sound Mechanical Principles



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Yours is ready for you.  
WRITE ABOUT IT

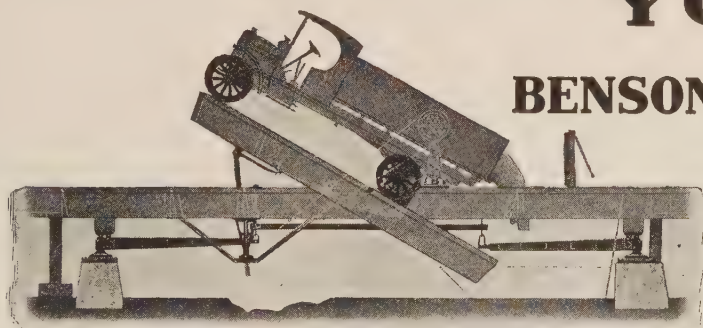
*All Kinds of Elevator Machinery*

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HAND OPERATED  
NO COMPLICATED MECHANISM  
NO POWER CONNECTIONS

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MINNEAPOLIS

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Is built for service. Made from Selected White Birch, has direct acting springs, double safety device that will not fail, runs easy, unnecessary to bolt or lock it while not in use.

Has wire cable and cotton hand line. Sold on 30 days' trial. Write for circulars and prices.

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We Build  
Modern Grain Elevators



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Advertise in the "Situation Wanted" columns of the Grain Dealers Journal.



## THE ONLY SANE, SAFE THING

to do is to install an All Metal  
Fire Proof

**Knickerbocker "1905" Cyclone  
DUST COLLECTOR**

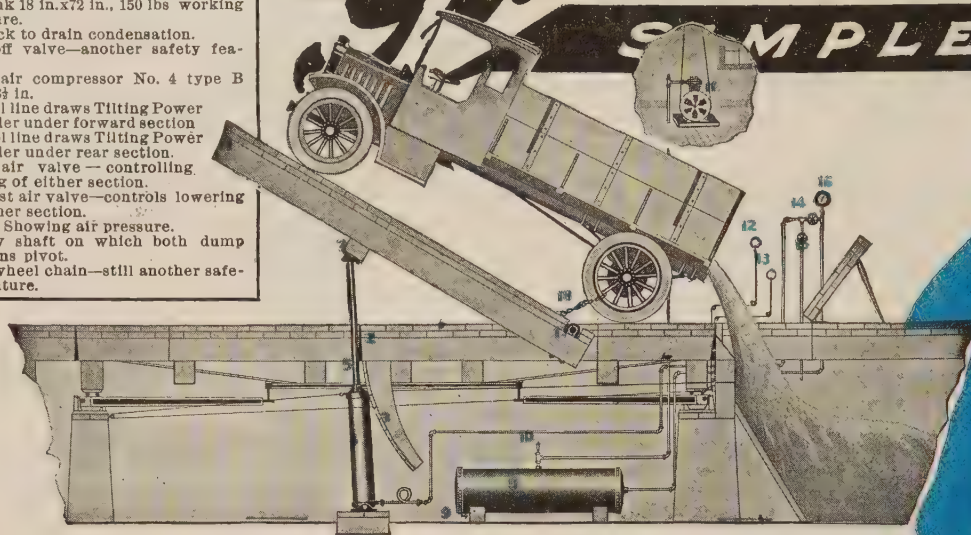
**The Knickerbocker Co.,**

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1. Truck, or outside forward section.
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4. Patented tilting power cylinder, 10 in. x 48 in.
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9. Pet cock to drain condensation.
10. Blow off valve—another safety feature.
11. Curtis air compressor No. 4 type B 3 in. x 3 1/2 in.
12. Control line draws Tilting Power Cylinder under forward section.
13. Control line draws Tilting Power Cylinder under rear section.
14. Inlet air valve—controlling raising of either section.
15. Exhaust air valve—controls lowering of either section.
16. Gauge Showing air pressure.
17. Hollow shaft on which both dump sections pivot.
18. Rear wheel chain—still another safety feature.



# Globe Combination

## SIMPLE - RELIABLE

(SPENCER)

# Dump Truck, Wagon or Sled

There is only one way—our patented Tilting Power Cylinder operating “the dump within a dump” gives you a complete outfit—no “extra platforms.”

No other dump has the Tilting Power Cylinder, our patented feature, that makes possible the operating of “the dump within a dump” or the “two-in-one” idea. This eliminates the added cost of “extra platform” and increased installation charges necessary with other dumps.

When the GLOBE COMBINATION AUTO TRUCK AND WAGON DUMP is installed independent of scale, it is only necessary to sink a narrow well and build a small base for the Tilting Power Cylinder. Others are compelled to dig pit, 2 or 3 feet in depth, as large as the square platform used, and when two such dumps are used in series, so both wagons and trucks can be dumped, just twice the excavation is necessary.

You can readily see how easily and how economically the GLOBE COMBINATION TRUCK AND WAGON DUMP can be installed in connection with a scale—simply a slight deepening in the center of the scale pit to accommodate the foundation for the Tilting Power Cylinder—a few minutes’ work, rather than digging out from one-third to half the area of the scale pit bottom, as is necessary in installing most other truck dumps. This dump operates perfectly and is installed easily in connection with any type, size or make of scale or independent of scale.

The incline or “Jack Knife” type of platform, comprises really two platforms, one within the other, both being pivoted on a hollow shaft at the entering end. This is considered the

**No Binding  
or  
Vibration**

very best engineering practice as it raises the front wheels in a manner similar to driving a wedge under them, and at the same time forms a rigid support because the timbers are firmly secured at one end—not shaky or binding, as would be the case if the platform were lifted straight upward and had corner guides.

**Absolutely  
Safe  
Always**

A feature much appreciated by elevator operators and farmers is the absolute safety of the Globe Combination Auto Truck and Wagon Dump. It is under positive control of the operator of the air valves, both in raising and lowering. The wagon section is so constructed as to permit of a horse guard; no horse can possibly back into opening as dump is raised. We have overlooked nothing in making the GLOBE COMBINATION AUTO TRUCK AND WAGON DUMP absolutely safe. Our safety chains, instantly adjustable to the rear wheel of any truck or wagon or to the rear runner of a sled, are positive assurance against any accident—you can hold the load with the brakes but if the brakes fail, you have nothing to fear if you slip on the chains.

The air tank is equipped with a blow-off valve which eliminates any chance of overcharging.

A pet cock is inserted in one end of tank to enable draining of the slight condensation which always takes place when air is compressed.

A careful observation of the illustrations above reveal that they are identical with the one exception—in the left-hand picture the truck section (No. 1) is raised and the wagon section (No. 2) is down, while in the right-hand picture the reverse is true. Notice that the horse guard (No. 3) comes into play when the wagon section is raised—this absolutely pre-

**Shifting  
Cylinder  
Simple**

vents a horse from backing into opening when the wagon dump is up.

The patented Tilting Power Cylinder (No. 4) operates either the wagon or the truck section—simply shift forward by pulling line (No. 12) to raise the truck section, or backward by pulling the other line (No. 13) to raise the wagon section. A lever shift can be used if desired.

This patented Tilting Power Cylinder, solidly seated on a rocker base bearing (No. 6) is the secret of the wonderful success of the Globe Combination Auto Truck and Wagon Dump.

This simple, yet marvelous idea, enables us, with but the one Cylinder and the divided platform or “dump within a dump feature” to accomplish the dumping of Trucks, Wagons or Sleds, while some other dumps have to use two separate sections, two cylinders, and consequently charge you considerably more for this outfit.

**Both  
Sections  
Firmly  
Pivoted**

You will notice that both platforms are pivoted on the hollow shaft (No. 17) and as they raise “Jack Knife” fashion, there is no binding or sid tilting to strain the cylinder or rack the sections as in the case with a “straight upward lift” having corner

# Globe Machine

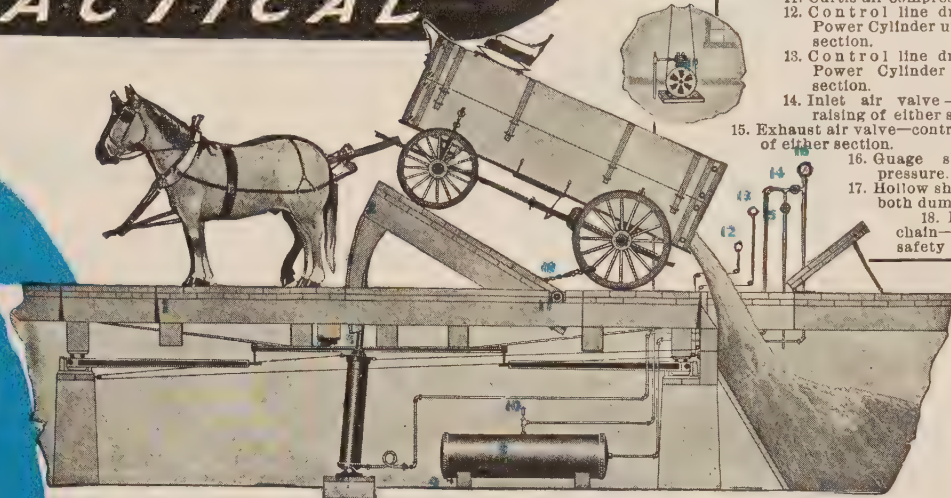
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# Truck & Wagon Dump

## LE - PRACTICAL

(FEB. 1919)



### WAGON DUMP

1. Truck, or outside forward section.
2. Wagon or sled, or inside rear section.
3. Horseguard—an added safety feature.
4. Patented tilting power cylinder.
5. Piston Rod, 2 and 15-16 in. shafting.
6. Rocker Base bearing, heavy cast iron.
7. Socket bearing.
8. Air Tank 20 in. x 72 in., 150 lbs. working pressure.
9. Pet cock to drain condensation.
10. Blow off valve—safety feature.
11. Curtis air compressor.
12. Control line draws Tilting Power Cylinder under forward section.
13. Control line draws Tilting Power Cylinder under rear section.
14. Inlet air valve—controlling raising of either section.
15. Exhaust air valve—controls lowering of either section.
16. Gauge showing air pressure.
17. Hollow shaft on which both dump sec. pivot.
18. Rear wheel chain—still another safety feature.

# on One Platform—at One Cost

operates perfectly on any type, size or make of scale or independent of scale—no “setting ahead” or moving of pit doors.

sides, and useless and expensive ears to keep it in place. The piston (No. 5) simply engages the socket bearings (No. 7) in either platform and pushes upward, with nothing to weigh down or interfere with scale.

Another feature of great advantage is the incline platform, which enables the front wheels to climb to their proper elevation as platform raises under them rather than risking the backing off of the front wheels or the jacking forward of the rear wheels, as is the case when the “flat platform, upward lift” type dump is used. The Truck or Wagon, stopped at the proper distance from the pit door, will remain in that position by simply rowing on the brakes, as the front wheels gradually ascend the incline the platform rises until the proper dumping angle is reached.

However, we furnish a pair of chains (No. 17) for use in locking the rear wheels, when desired—this a simple, easy and safe way to assure the proper operation of load, relative to pit door, and eliminate any danger of wagon or truck backing.

The heavily riveted storage tank (No. 8) is constructed of 3-16 inch

### Strong Storage Tank

steel, guaranteed for 150 pounds working pressure. This tank is 18x72 inches and has capacity sufficient to dump several loads—in fact, the largest pit full of grain with one charging.

It is fitted with blow-off valve (No. 10) and pet cock (No. 9) to prevent overcharging and drain condensation.

This tank can be located under the dump or anywhere from basement to cupola in the elevator, even outside if desired, as piping to compressor and Tilting Power Cylinder is a simple matter.

The Air Compressor (No. 11) is a Curtis Machine of Type B No. 4, 3x3½ inches. It is fitted with unloader and is the very latest and most improved machine on the market. No oil can get into the air line, because of the scientific method of oiling and the system by which air is taken into it.

The compressor can be driven from motor or line shaft; in the latter case the same power that elevates grain will recharge air tank, thus eliminating the necessity of starting machinery to operate dump.

The Air Valves (No. 14 and No. 15), together with air gauge (No. 16), and draw line (No. 13) can be conveniently located in office, near scale beam or on work floor.

Inlet valve (No. 14) lets the air into the Tilting Power Cylinder after Cylinder has been shifted backward or forward to accommodate truck or wagon as the case may be.

These easily operated valves give the operator complete control of the rising load—stop and start or stop and hold the load at any point. This is the feature that makes for absolute safety in unloading wagons—if the team is the least bit skittish, stop instantly and if necessary lower the load

### No Run-aways on This Dump

to the level again. You need never have a runaway or hurt a horse on the Globe Dump.

When you are dumping truck or wagon, open inlet valve, raise to proper angle, close inlet valve, hold truck, wagon or sled until empty, open exhaust valve and lower as speedily or slowly as you desire—the inlet valve controls the raising; the outlet valve controls the lowering of dump.

You will notice that it is not necessary to unhitch the horses when dumping wagons, neither do you have to drive forward or back up, after weighing, if dump is installed on scales. Stop wagon, truck or sled at proper distance from pit trap, weigh the load, shift the Tilting Power Cylinder to the wagon or truck section as desired, open the inlet valve and instantly your load is dumped, close the valve and open the exhaust and your truck or wagon or sled is at once returned to the level. The whole operation is accomplished in as little time as it takes to tell it.

### Dumps Any Vehicle Instantly

### POINTS OF SUPERIORITY.

1. Dumps Trucks, Wagons, Sleds.
2. Fits any type, size or make of scale.
3. Only one cylinder, but doing double duty.
4. Accidents are impossible.
5. No tilting sidewise or binding of brakes.
6. Does not “set ahead” of scale.
7. Unnecessary to “back up” after weighing.
8. Safety chains, horse guard, blow-off valve and drain cock prevent trouble.
9. Only slight excavation necessary to set Tilting Power Cylinder.
10. No gears, chains or clutches to break or obstruct driveway.

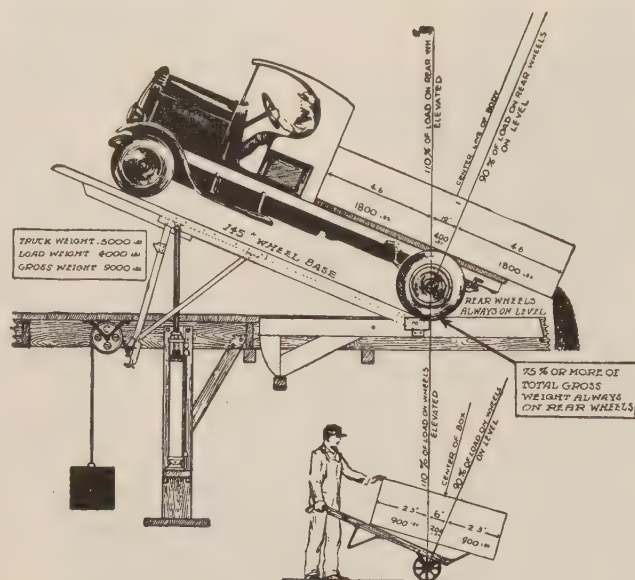
### Ten Reasons Why

Manufactured by

**Supply Co**

IOWA





# The MOFFITT Automatic America's Foremost Truck Dump

**I**N the above illustration you will note that the method of carrying the load on the auto truck in relation to the rear wheels is identical with that employed on a hand truck.

When the trucks are on the level 90% of the load is on the wheels of a hand truck or rear wheels of the auto truck. When the trucks are raised to the inclined position 110% load is on the wheels of the hand truck or rear wheels of the auto truck. The man is obliged to hold down on the handles to balance the load; however, the truck has no tendency to run forward or backward because the wheels carrying the total load are on the level floor. The weight of the auto truck, without load, is about equally divided between the front and rear wheels when the truck is on the level and, with a load, about 25% of the weight is on the front wheels and 75% on the rear wheels. When the auto truck is on the incline the weight on the front wheels is considerably lessened by reason of the fact that all of the load is transferred to the rear wheels and in addition 10% overbalance, which overbalance lessens the weight on the front wheels just that much.

Instead of using power to raise the front end of the truck, the truck dump is raised to an inclined position by use of a counter-weight. The front wheels of the truck are then driven up the incline. The rear wheels always remain on the level of the driveway. There is very little tendency of the truck backing down the incline and this is offset by a slight depression just at the rear of where the truck dump is pivoted to the floor and in which the rear wheels rest.

It is therefore readily understood that the principle involved as regards the load on the hand truck and that on the auto truck are very similar, varying, of course, depending on how the load is centered over the wheels.

After the load is dumped the auto truck is heavier on the front wheels by reason of the overbalance having been removed. The weight of the front end of the truck causes the truck dump to return to the floor level, descending smoothly, being controlled by a hydraulic oil controller.

The truck dump is locked in its horizontal position as well as when it is in an inclined position. This lock is releasable and is in control of the man in charge of the elevator. A slight pull releases the lock, and, in locking, it is automatic in its action.

## Automatic Truck Dump Company

Manufacturers

### 502 Grain Exchange Building, Omaha, Nebraska

*Information of Value to Elevator Owners will be Mailed Free on Request—Write To-day*



# Wanted and For Sale

The rate for advertisements in this department is 25 cents per type line each insertion

## ELEVATORS FOR SALE.

**30,000 BU. ELEVATOR** for sale, complete with machinery and additional warehouses. Address Coleman State Bank, Coleman, Wisc.

**25,000 BU. CRIBBED** elevator in N. D. on Great Northern R. R. with Coal and Flour and Feed business for sale. Address: Chance, Box 9, Grain Dealers Journal, Chicago.

**25000 BU. ELEVATOR** For Sale, complete with machinery and additional warehouse and coal bins. Address The Camden State Bank, Camden, Indiana.

**OHIO ELEVATOR** For Sale—Good town with good schools and churches, finest farming community, no competition. Electric Power. Good reasons for selling. Address Community, Box 6, Grain Dealers Journal, Chicago, Illinois.

**WE SPECIALIZE** in the sale of Grain Elevators in Kansas, Oklahoma, Texas and Western Missouri. Let us know what you want.

**STRONG TRADING CO.,**  
Wichita, Kansas.

**NEW 18,000 BU.** elevator for sale. Well equipped with machinery, in A-1 condition. In good wheat, oats and corn belt. Doing good business. Good reason for selling. Address: Ketchum Elevator Co., Ketchum, Okla.

**GRAIN ELEVATOR** for sale on the B. & O. R. R.; 15,000 bushels capacity, two legs, electric power, iron clad, metal roof, 14 bins, all cribbed. Everything in A No. 1 shape. Price \$20,000.00. John V. Dirk, Weston, Ohio.

**INDIANA ELEVATOR** For Sale or will trade for farm. 7500 Bushels Capacity. On Lake Erie & Western R. R. Convenient to any or all points, Chicago, Indianapolis, Cincinnati, Ohio, Buffalo, N. Y., or Toledo, Ohio. Doing a good business. Address C, Box 6, Grain Dealers Journal, Chicago.

**8,000 BU. CAPACITY ELEVATOR** with good farm implement business for sale. Advancing years and death in firm makes it advisable to sell. Good grain locality. Excellent business. For complete information apply to Alex Verdort & Co., Bonnot's Mill, Mo.

**GENERAL ELEVATOR** and feed business for sale. 1st class attrition feed grinder in connection. Also handle coal, farm machinery, fencing, building supplies, etc. No competition. Excellent business. Address: Field, Box 8, Grain Dealers Journal, Chicago.

**15,000 BU. CRIBBED ELEVATOR** for sale in southern Indiana. Handles 75,000 bu. Wheat with the following side lines, Flour and Feed. Good business. Death in firm makes it advisable to sell. For complete information apply to A. H. Richner, Broker, Crawfordsville, Ind.

**SOUTH CENTRAL MINNESOTA** Cribbed Elevator. 25,000 Bushels Capacity, coal shed, flour and feed business and five acres of land adjoining elevator, with dwelling house and other improvements for sale. Located in town of 250 population. Price \$15,000.00—no trades. Either one can be bought separately. Makes an ideal place for one or two parties contemplating going in a general grain and live stock business. Address Land, Box 6, Grain Dealers Journal, Chicago, Illinois.

## WANT ADS WORK WONDERS.

They sell elevators, find help and partners, secure machines and engines which you want, and sell those for which you have no further use, and perform a myriad of kindred services for shrewd people who use them regularly. **READ AND USE THEM.**

## ELEVATORS FOR SALE.

**ELEVATOR AND GRAIN BUSINESS** for sale. Line of Country Elevators, Central Illinois. Capacity 90,000 bus. Excellent condition. Also good commission business. Price \$30,000.00. Address Troy, Box 5, Grain Dealers Journal, Chicago, Illinois.

**OLD ESTABLISHED** Wisconsin Elevator, feed and seed business for sale. Equipped with Electric power; attrition mill, corn crusher, feed mixers, corn sheller, corn cracker and grader, seed cleaner, power loader and unloading machinery, large storage for sacked and bulk feeds. Large grain and dairy territory. A real money maker. Address D. W. B., Box 5, Grain Dealers Journal, Chicago, Illinois.

**CENTRAL WESTERN OHIO** Elevator for sale. 20,000 bu. Iron Clad. Private ground and spur. Electric power. In town of 3,000, with first class high school and located in the best grain section of Ohio. Side lines—Coal, Flour, Feed and Salt. Have done a retail business of \$30,000.00 in the last year. New grinder and crusher, new corn sheller and conveyor. Price \$15,000.00. No trades. Address Opportunity, Box 5, Grain Dealers Journal, Chicago.

**THREE NORTH DAKOTA** Elevators for sale. 40,000 bushels capacity each. In good operating condition and located on Great Northern R. R. Two of these elevators are equipped with Monitor Cleaners, other has no cleaner. All equipped with 8 h. p. Fairbanks Gasoline Engines. We are naming an exceptionally low price on these elevators, about half of what it would cost to build them today. For further particulars address Northern, Box 5, Grain Dealers Journal, Chicago, Ill.

**N. DAKOTA, 20,000 Bu. Capacity Elevator** 12,000 bu. annex for sale, motor driven. Storage for 200 tons of coal in addition. Equipped with all modern machinery. Receipts up to 60,000 bushels annually. Easily accessible to Duluth and Twin Cities, located on N. P. R. R. in good town which has good schools and churches. Young man who knows the business and is a hustler can greatly increase the business. Present owner too old to give business proper attention. Address Annex, Box 3, Grain Dealers Journal, Chicago, Illinois.

## ELEVATOR

For sale—5 years old, located in suburb of Omaha. Capacity, 20,000 bus. 500 bus. capacity Hopper Scales, also Fairbanks-Morse Wagon Scales. Electric power worm-gear drive. Hopper bins and cribbed house. Best foundations and well constructed. Feed mill, electric drive. Extra storage. Milling in Transit Grain Rates. Present price to construct would be over \$20,000. Price, \$17,500. On account of the big market for ground feeds in and around Omaha this equipment could form the basis of a highly profitable feed business. Address the

## Insurance Building Co.

432 World Herald Building Omaha, Nebr.

## ELEVATORS FOR SALE.

**SOUTHERN MINNESOTA ELEVATOR** for sale. Capacity about 20,000 bushels, with warehouse attached. Exceptionally well built house in fine condition, very good territory and well established business. Handle coal, flour and feed and have good jobbing trade. Address Y, Box 5, Grain Dealers Journal, Chicago.

**TWO GRAIN ELEVATORS** for sale, located in North Central Iowa at adjoining stations. Good grain territory and both handle flour, feed and coal. Side line will take care of operating expense. Both do good business. A mighty good opening for the right man. Write quick. Address Adjoining, Box 5, Grain Dealers Journal, Chicago, Illinois.

**7,500 BU.** frame iron clad elevator for sale, 60 miles north of St. Joseph, Mo., on Wabash R. R. New Electric motor power, new combined sheller and cleaner, 14 in. x 7 in. Buckets for ear corn, Howe Dump Scale. 1½ acres of private ground with warehouse adjoining elevator goes with it. New stove in office, new adding machine, fire proof safe, portable platform scale. Only elevator in town. For quick sale at \$6,000.00. W. C. Bailey, 433 Railway Exchange Bldg. Omaha, Nebraska.

## ELEVATORS WANTED.

**WILL BUY** Good 8 to 15,000 bu. elevator in North Central Okla. or Southern Kansas. Address Box 524, Pond Creek, Okla.

**WILL TRADE** equity improved section Hettiger Co., N. Dakota, for first class grain or lumber business. C. A. Pfund, Roland, Iowa.

**WILL BUY ILLINOIS ELEVATOR** at good grain point. No objections coal and lumber with it. Give full details. Address Buyer, Box 6, Grain Dealers Journal, Chicago, Ill.

**WISH TO BUY GRAIN ELEVATOR** located at any good grain point. Address with full particulars Wm. F. Allbrecht, 2727 Du Pont South, Minneapolis, Minn.

**WE HAVE** several live wire clients in market to buy elevators. Send particulars of what you have.

**STRONG TRADING CO.,**  
Wichita, Kansas.

## ELEVATOR BROKERS.

**JOHN A. RICE**, exclusive elevator broker, Frankfort, Indiana.

**IF YOU WANT** to sell or exchange your property, write to me. John J. Black, 57th Street, Chippewa Falls, Wis.

**ELEVATOR BUYERS SAVE TIME AND MONEY BY WRITING ME WHAT YOU WANT. I HAVE IT OR WILL GET IT.** NAT CLAYBAUGH, elevator broker, Frankfort, Ind.

**WE SPECIALIZE** in Elevators in Kansas, Oklahoma, Texas and Missouri.

**STRONG TRADING CO.,**  
Wichita, Kansas.

## BIDS WANTED.

**BIDS WANTED** on the erection complete of two 40 ft. x 14 ft. concrete grain tanks. Tanks to be plain with no machinery in connection with same.

The Rolla Co-Operative Equity Exchange, Rolla, Kansas.

**SOMEBODY'S** always hunting an elevator, and there's no wrong time of the year to put an ad in the "Elevators For Sale" columns of the Journal.



**SITUATIONS WANTED.**

**POSITION WANTED**, with a company doing track buying business, by a young man who has had six years office experience, two years experience in grain and four years manufacturing. Can furnish best of references. Address Man., Box 6, Grain Dealers Journal, Chicago.

**SITUATION WANTED** as manager of Farmers Elevator in town of not less than 400. Seven years experience as grain buyer. Married, 50 years of age. At present employed but can change at any time. Can furnish references. Address Employed, Box 6, Grain Dealers Journal, Chicago.

**POSITION WANTED** in Sales or Traffic Department of some reliable concern. Am married—32 years old. Have had seven years railroad experience and five years mill office experience. Would consider any good position with a future to it. At present employed but seeking change. Address Railroad, Box 6, Grain Dealers Journal, Chicago.

**AS SOLICITOR** for good grain firm. Have had seven years' experience as country grain buyer and manager of elevators. Am at present employed, but want to change. Married, twenty-seven years of age. Can give A-1 references. Address M, Box 5, Grain Dealers Journal, Chicago, Illinois.

**POSITION WANTED** as manager of Farmers Elevator or line of Elevators. Thoroughly familiar with grain and bean grades, government reports and income tax returns. A-1 accountant. If you need a capable executive, write or wire Account, Box 5, Grain Dealers Journal, Chicago, Illinois.

**AS MANAGER** of country elevator in Iowa or Ill. wanted. Farmers' or individual, no line company. Can handle all side lines and any kind of grain machinery. Can furnish the best of references. Good book-keeper, 34 yrs. old and married. Must be a man's size job, with a good future. Address Capable, Box 5, Grain Dealers Journal, Chicago.

**POSITION WANTED** by young man in Grain Business; 11 years' experience, 4 years as manager of elevator. Have had buying and selling experience in grain, flour, feeds and seeds, both retailing and jobbing. Have also had experience in milling. Married, 28 years of age, and employed at present as foreman and head miller for Grain and Feed Co., but desire change. Can furnish best of references. Address Married, Box 4, Grain Dealers Journal, Chicago.

A Missouri advertiser in our "Situation Wanted" columns says: "I find that advertising in your journal pays, for I had letters from all parts of the country offering positions."

## Clark's Wagon Load Grain Tables

(ON CARDS)

show the reduction of any weight of grain from 100 to 4090 pounds by ten pound breaks, to bushels of 32, 48, 56, 60 70 and 72 pounds.

Six tables printed in two colors, on both sides of three cards, size  $5\frac{1}{2} \times 10\frac{1}{2}$  inches. Price 55 cents, postage 4 cents.

**GRAIN DEALERS JOURNAL**

305 So. La Salle St.

CHICAGO, ILL.

**MALE HELP WANTED.**

**MANAGER WANTED** for small country station handling lumber, coal and grain. Must be married. Address Holcomb-Dutton Lumber Co., Sycamore, Ill.

**WANTED**—Hay and Grain Salesman. Can make from \$50.00 to \$75.00 a week. Address Hay, Box 4, Grain Dealers Journal, Chicago.

**EXPERIENCED ELEVATOR MAN** Wanted. Must know grain, how to handle men and take care of machinery.

The Ansted & Burk Co.,  
Springfield, Ohio.

**COMPETENT MANAGER WANTED** for Farmers Elevator in Nebraska. Give reference, state experience and salary expected. Address Leoma, Box 6, Grain Dealers Journal, Chicago, Illinois.

**COMPETENT FOREMAN** wanted for Mill Elevator good sized Oklahoma City. Must understand the grain business. Good salary for right man. Give experience and salary expected first letter. Address Foreman, Box 3, Grain Dealers Journal, Chicago.

**MILLS FOR SALE.**

**FIRST TIME ON SALE**—60 barrel Minnesota Flour Mill. Steam power. Town of 500. No competition. Is running steady and has good trade. Address Minnesota, Box 5, Grain Dealers Journal, Chicago, Ill.

**GREAT WESTERN** 2 pair High Roller, 9x24, Feed Mill, rebuilt, for sale. Belt Driven. Never been used since rebuilt. One pair extra rolls, all newly corrugated; never been uncrated. Guaranteed A-1 in every respect. Will ship F. O. B. cars here. Address Feed Mill, Box 6, Grain Dealers Journal, Chicago.

**DYNAMOS—MOTORS.**

**WANTED**—1 15 h. p. Motor, Type RI—220 Volt, 60 cycle, single phase, with starter. Nebraska Farmers' Union Assn., Hastings, Neb.

1—10 H. P., 220 VOLT, 3 phase, 60 cycle, 1800 R.P.M. Western Electric Type K. T. motor with auto starter and overload relay, new, for sale—\$185.00.

1—5 H. P., 220 volt, 3 phase, 60 cycle, 1200 R.P.M. new Western Electric motor with starting switch, for sale—\$95.00.

Instant shipment.

Studabaker Grain & Seed Co.,  
Bluffton, Indiana.

**EXPERIENCED HELP**, such as managers for country stations, foremen, auditors and employes needed in the grain business are readily secured thru an ad in the "Help Wanted" column of the Grain Dealers Journal, Chicago.

## IF You Knew

How easy it is to sell anything thru a want ad in this paper, you would send in your ad at once.

Try it. At least ask us about the success of others.

**Grain Dealers Journal**

305 So. LaSalle St., Chicago, Ill.

**ENGINES FOR SALE.**

Oshkosh, Wisconsin.

**25 H. P. MUGUL KEROSENE ENGINE**, good running order; 2 Barnard & Leas Warehouse Shelter, new; pulleys, shafting, belting, for sale. Sergeant Bluff Farmers Elevator Co., Sergeant Bluff, Iowa.

**FOR SALE:** Three 18-h.p. Lauson Oil Engines, in good running order, only been used about three months. Also one 14 H.P. Lauson Oil Engine, brand new, never been set up. Anyone needing such power can get a bargain by corresponding with The Farmers Elevator Company, Metcalf, Illinois.

1-40 H.P. Foos Engine, \$800.00.

1-26 H.P. New Era Engine, \$300.

1-9 H.P. Foos Engine, \$150.00.

1-6 H.P. Gray Gasoline Engine, \$100.00.

1-40 H.P. high pressure boiler, \$500.00.

2-75 H.P. Heine Watertube Boilers.

CASEY BOILER WORKS Springfield, Ohio.

**ENGINES WANTED.**

**USED GASOLINE OR OIL ENGINES** Wanted, Otto and Fairbanks preferred. In answering describe condition and name lowest price. Address Fairbanks, Box 5, Grain Dealers Journal, Chicago, Illinois.

**GASOLINE OR KEROSENE Engine** Wanted, 20 to 25 h. p., prefer standard make, must be good machine, no old or worn out machine wanted.

BLISH MILLING COMPANY,  
Seymour, Indiana.

**DYNAMOS AND MOTOR BUYERS** are reached in largest numbers and less expense by offering them for sale in the grain trade's accepted medium for power bargains—the "Dynamo—Motors" columns of the Grain Dealers Journal, Chicago.

**SCALES FOR SALE.**

**ONE 100 BUSHEL FAIRBANKS HOPPER SCALE** For Sale, in A-1 condition. Price \$50.00. Address Andrew Ringlein & Company, Havana, Ohio.

**NEW AND REBUILT** scales of all kinds. The Howe Scale Co. of Ill., 512-514 St. Charles St., St. Louis, Mo.

**ONE 4-TON Fairbanks dump scale** with type-registering beam, for sale, used less than year. Perfect condition. \$160.00. Drawer M, Marcus, Iowa.

**FAIRBANKS AUTOMATIC SCALES** for sale. Crated ready to ship. Grain augers, number of cast iron pulleys and shafting. All new, never been used. Farmers Grain, Fuel & Supply Co., Macomb, Illinois.

**LATEST TYPE 12 BUSHEL Avery Automatic Scale** for sale. Out of use account of enlarging elevator.

GOOCH MILLING & ELEVATOR CO.,  
Lincoln, Nebraska.

**4 TON 14X8' FAIRBANKS WAGON SCALE**, Compound Beam, Shelf and Hangers, including complete steel frame, for sale. Scale used less than one year. Good as new. Price \$175.00.

HOWE SCALE CO.,  
Kansas City, Mo.

**500 BUSHEL FAIRBANKS Hopper Scale** \$250.00; 500 bushel Monarch Hopper Scale, \$150.00; 500 bushel U. S. Hopper Scale, \$125.00.

Richardson Scale Co., Wichita, Kans.

**RICHARDSON AUTOMATIC BAG SCALE.** New. Suitable for overhead track, complete, used 10 days.

Very cheap.  
NATHAN KLEIN & CO., 210 Center Street,  
New York City.

**SCALES REPAIRED AND SOLD**

50 wagon scales, capacity from 4 to 15 ton. Any size platform in following makes: Fairbanks, Howe, Buffalo, Standard and Columbia. Each scale that leaves our factory is thoroughly overhauled and tested and guaranteed to be correct. We furnish competent men for outside work.

COLUMBIA SCALE CO.,  
2439 N. Crawford Ave. - - - Chicago, Ill.



## MACHINES FOR SALE.

**WILL YOU BUILD CONCRETE BINS?** We have for sale, cheap, 125 second hand jacks for this purpose. Address Concrete, Box 2, Grain Dealers Journal, Chicago.

**FOR SALE: BIG LOT** of elevator belting and cups, No. 9 Clipper, No. 1 Monitor Receiving Separator, Elevators, Shafting, Pulleys and Hangers. 20 carloads of everything in the elevator and milling line. A. D. Hughes Co., Wayland, Mich.

### FOR SALE:

Two 200 h. p. Westinghouse Motors.  
One 125 h. p. Westinghouse Motor.  
One 50 h. p. Westinghouse Motor.  
One Three high Great Western Corn Mill.  
Full description to any one interested.  
**SUNNY SOUTH GRAIN CO.,**  
Birmingham, Ala.

**BOWSHER MILL** for sale. Also—  
9x24 Barnard and Leas. 3 pairs high, feed roller mill.  
The above in good condition.  
Reason for selling, have discontinued feed business.

**DROGE ELEVATOR COMPANY.**  
Council Bluffs, Iowa.

1 No. 7 Dust Collector.  
1 only Auto sacking scale on truck for bagging meals, oats & chops & corn 5 to 200 lbs.  
1 only corn cleaner, 1000 bu. per hour.  
1 only 18 to 20 in Attrition Mill.  
1 only Magnetic Separator for 9x24" corn rolls.  
1 only Auto. scale for sacking molasses feeds.  
1 only mix. conveyor with jacket for molasses feeds.  
40 ft. 9" conveyor; 40 ft. 12" conveyor complete.  
Address X, Box 3, Grain Dealers Journal, Chicago.

**FLOUR MILL MACHINERY FOR SALE—**  
Two double stand roller mills newly overhauled and ready to do first class work. One Hughes flour sifter, four sections five sieves deep. The above cost \$1,400.00 when new. All practically as good as new as far as service is concerned. Also one 16 to 19 h. p. new oil engine corn and buckwheat buhrs wheat scourer bolts, line shaft, belts and pulleys and feed mill nearly new. Reason for selling, bad health and am going out of the milling business.

**STOVER MILLING COMPANY,**  
Stover, W. Virginia.

### SECOND HAND Machinery For Sale:

One Erie City Ninety horsepower boiler in good condition.  
One Ellis Grain Drier, capacity 350 bushels per hour, in first class condition. This is a good drier and it will pay you to investigate it.  
One Richardson Automatic Scale, capacity 1500 bushels per hour. This scale is in good condition.  
One No. 9 Oat Clipper, made by Huntley Manufacturing Co., Silver Creek, N. Y.  
One No. 6 Cleaner made by the same people.  
These are good machines and have been used but very little.  
Address Miscellaneous, Box 6, Grain Dealers Journal, Chicago, Ill.

### SECOND HAND Machinery For Sale:

1—6x12 Eight Roller Mill, being 2—6x12 Mills connected together; good for short system mill.  
1—7x14 Nurdyke 4 Roller Mill.  
1—6x18 Graham Single Roller Mill.  
1—6x20 Todds & Stanley 4 Roller Mill.  
1—Nurdyke Swing Sifter for 100 Barrel Mill.  
1—California Separator and Smutter 75 barrel Mill.  
1 Richmond Bran Duster for 75 barrel Mill.  
2 No. 3 Smith Centrifugal Reels.  
1 Nu-Way Separator.  
1 Lee No. 1 Perfection Wheat Peeler, 12 to 20 bushels per hour.  
1000 feet Elevator Belting, 3-inch Cups.  
100 feet 9-inch L. H. Spiral Conveyor.  
30 feet 12-inch L. H. Spiral Conveyor.  
Lot of other Mill Equipment, Pulleys, Belting, etc.

**TEUSCHER & SON MCHY. SUPPLY CO.,**  
527 N. Second Street. St. Louis, Mo.

## MACHINES FOR SALE.

**MIDGET MILLS:** If you wish to buy or sell a Midget Mill from 25 to 60 bbl. capacity write me. All kinds of new and used mill machinery. H. C. Davis, Bonner Springs, Kansas.

1 NO. 2 JOLIET Corn Sheller for sale, perfect mechanical condition, practically new, has shelled only 10,000 bu. corn this season. Reason for selling have quit shelling corn. A bargain at \$500.00 f. o. b. Fayetteville, Tenn. Quick sale, need the room. Thornton Grain Company, Fayetteville, Tenn.

### SECOND HAND Machinery For Sale:

One nearly new 22-inch Monarch B. B. Attrition Feed Grinder with drive and leather belt.  
One No. 2 Richmond Grain Cleaner.  
24 feet 2 7/16 shafting.  
36 feet 10-inch Gandy Belt.  
200 feet No. 76 detachable chain belting.  
Quantity buckets, belts, pulleys, etc.  
**WM. M. MONROE & SON,**  
Bronson, Michigan.

**DO YOU** want a machine or machinery? Have you a machine or machinery which you do not use or do not want? Do you enjoy having discarded machines and machinery laying around in your way, to rust out, or would you prefer to exchange it for elevator supplies you need? Use this department. Get what you want. Put your idle capital to work.

### REAL BARGAINS.

Prompt Attention. Quick Shipments.  
When in need of elevator or mill machinery, notify us. We are headquarters for power and transmission equipment, and have on hand several well-known makes of motors, boilers, engines, etc.

Send us list of all your wants. We can supply you with full line of machinery for elevators, flour, corn and cereal mills. Complete equipments for modern mills of all kinds, molasses stock, and poultry feed plants, plans, specifications, flow sheets, etc., our specialty.  
Write us without delay.

Geo. J. Noth, Mgr.,  
9 S. Clinton St., Chicago, Ill.

### SECOND HAND Machinery For Sale:

One No. 4 Invincible Special Close Scouring and Separating Machine Single.  
Two No. 6 Invincible Double Special Close Scouring and Separating Machines.  
Two No. 38 Perfection Dust Collectors.  
One No. 35 Perfection Dust Collector.  
One No. 25 Perfection Dust Collector.  
One No. 23 Perfection Dust Collector.  
Three No. 15 Perfection Dust Collectors.  
Five No. 6 1/2 seventy-two inch twelve section Universal Bolter, 50 extra sieves each.  
Three seven-foot by thirty-two inch round reel Flour Dressors.  
Four Reliance Flour Packers, Tubes and Augers for barrels 98s—48c and 24 pound sacks.  
One No. 6 Invincible Milling Separator.  
On Barnard & Leas Packer for 98s & 48s.  
One Case 9 by 30 Roller Mill with Girard Roller feeder.  
One 9 by 18 three high Nurdyke Marmon Corn roll.  
One Columbia First Break Feed Governor 45 bushels per hour.  
One First Break Feed Governor, 90 bushels per hour.  
One Hercules Bran Packer.  
Two 30 Inch ABC Steel Plate Exhaust Fan.  
**THE ARKANSAS CITY MILLING CO.,**  
Arkansas City, Kansas.

"YOUR ADS have produced satisfactory results." The Starnwill Mfg. Co., Alpha, Minn.

### YOU MAY BE MISSING SOMETHING

If you do not read the Wanted and For Sale ads each issue. If you have something to sell or exchange, insert small advertisement in the Wanted and For Sale Department. When in need of an employee or employment write Grain Dealers Journal, 305 S. La Salle St., Chicago, Ill.

## STEAM ENGINES, BOILERS.

**ENGINE AND BOILER** For Sale.

1—Frost 35 H. P. Engine.  
1—Frost 40x10 Boiler.

Been used six years, in fair condition. Will sell cheap for cash. Reason for selling, are rebuilding and will use electricity.

**OXFORD GRAIN CO.,**  
Oxford, Ind.

## PARTNERS WANTED

**CAPABLE, RELIABLE MAN** with some capital, a considerable acquaintance among millers and grain dealers in Michigan and Ohio, and first class knowledge of traffic, wants partner of ability and character to enter the Cash Grain Business. Anyone interested address P. W., Box 6, Grain Dealers Journal, Chicago.

## BAGS FOR SALE.

2000 SECOND HAND Cotton Grain Bags for sale at twenty-five cents each, f. o. b. St. Louis; 16 oz. holds 2 1/2 bushels wheat. Foell & Co., 123 Market St., St. Louis, Mo.

**BURLAP BAGS OF EVERY KIND FOR SALE;** new or second-hand, plain or printed with your brand; seamless Cotton Grain Bags; Sample Bags; Burlap, Cotton Sheeting, or Paper for Car Lining, etc. Wanted: Second-hand bags, best prices paid.

**WM. ROSS & CO.,** 409 N. Peoria St., Chicago.

## OFFICE SUPPLIES.

**SAFES—**Large stock of new and used safes on hand. Protect your valuable papers. Prices reasonable. The Howe Scale Co. of Ill., 512-514 St. Charles St., St. Louis, Mo.

**FOR SALE—**Burroughs, Wales, American, Dalton, Standard, etc., adding machines, half retail prices, fully guaranteed. Typewriter bargains, all makes. Get illustrated catalog and bargain list. Minnesota Typewriter Exchange, Department G, 236 Fourth St. South, Minneapolis, Minnesota.

## BUSINESS OPPORTUNITIES.

**PROSPEROUS, LONG ESTABLISHED** grain, hay and seed business in Colorado, for sale. Good reason for selling. Splendid location with room for expansion. Requires about \$80,000 cash. Address Prosperous, Box 3, Grain Dealers Journal, Chicago, Illinois.

**IF YOU** are desirous of obtaining more capital for the extension of your business; if you have land or realty to exchange for an elevator; if you have money to invest in the grain business, make known your desires in the "Business Opportunities" columns of the Grain Dealers' Journal.

## MACHINERY

For Grinding, Elevating and Conveying all kinds of grain. of standard makes, from smallest to largest capacities.

**NEW AND 2ND HAND REBUILT FLOUR MILL MACHINERY**

# Big Stock

**GOOD AS NEW**

We have the Largest Stock of 2nd Hand Flour Mill and Elevator Machinery in the World.

Belting, Pulleys, Shafting, Elevator Buckets and Conveyor Bolting Cloth and Roll Grinding.

Write for Net Price Book No. 78-B

B. F. GUMP CO.

THE MILL SUPPLY HOUSE  
431-437 South Clinton Street, CHICAGO, ILL.



# SEEDS FOR SALE—WANTED

## Field and Grass Seed Trade Directory

**ARNHEM, HOLLAND.**  
Wm. E. Busgers & Co., European fancy natural gr. sds.

**ATCHISON, KANS.**  
Manglesdorf Seed Co., The, wholesale seeds.

**BALTIMORE, MD.**  
Belt Seed Co., The, importers & exporters seeds.

**BELFAST, IRELAND.**  
McCausland, Sam'l, Ryegrass & Dogstail.

**BUFFALO, N. Y.**  
Stanford Seed Co., Inc., The, wholesale field seeds.  
Whitney-Eckstein Seed Co., wholesale seeds.

**CHICAGO, ILL.**  
Dickinson Co., The, Albert, seeds.  
Illinois Seed Co., The, grass and field seeds.  
Johnson, J. Oliver, seed merchant.  
Continental Seed Co., seed merchants.

**CINCINNATI, OHIO.**  
McCullough's Sons, The J. M., field and garden seeds.

**CRAWFORDSVILLE, IND.**  
Crabbs Reynolds Taylor Co., grass and field seeds.  
Crawfordsville Seed Co., seed merchants.

**DES MOINES, IA.**  
Iowa Seed Co., buyers and sellers.

**EVANSVILLE, IND.**  
Small & Co., W. H., seeds, grain and hay.

**FARIBAULT, MINN.**  
Farmer Seed & Nursery Co., seed corn & grass seeds.

**INDIANAPOLIS, IND.**  
Southern Seed Co., field and garden seeds.

**KANSAS CITY, MO.**  
Harnden Seed Co., field & grass seeds.  
Missouri Seed Co., wholesale exports and imports.  
Peppard Seed Co., J. G., wholesale seeds.  
Rudy-Patrick Seed Co., wholesale seeds.

**LAWRENCE, KANS.**  
J. Underwood & Son, grass and field seeds.

**LINCOLN, NEB.**  
Griswold Seed & Nursery Co., seed merchants.

**LOUISVILLE, KY.**  
Hardin, Hamilton & Lewman, grain & field seeds.  
Louisville Seed Co., clover & grasses.  
Wood, Stubbs & Co., grass & garden seeds.

**MERIDIAN, MISS.**  
Kimbrough Mitchell Seed Co., Southern seeds.

**MILWAUKEE, WIS.**  
Courteen Seed Co., field seeds.  
Kellogg Seed Co., grass and field seeds.  
L. Teweles Seed Co., field seeds.

**MINNEAPOLIS, MINN.**  
Dickinson Co., The, Albert, seeds.  
Minneapolis Seed Co., seed merchants.  
Northrup, King Co., wholesale seeds.

**NEWBERN, TENN.**  
Cole Seed Saver Co., Japan clover wholesale.

**NEW YORK, N. Y.**  
Loewith Larson & Co., grass & field seeds.  
Doughten, Inc., H. W., grass & field seeds.  
Nungesser-Dickinson Seed Co., wholesale seed merchants.  
Radwaner Seed Co., I. T., fld. & gr. seeds, ex. imptra.

**OKLAHOMA CITY, OKLA.**  
State Seed Co., The, garden & field seeds.

**PHILADELPHIA, PA.**  
Philadelphia Seed Co., Inc., The, wholesale field seeds.

**ST. JOSEPH, MO.**  
Chesmore Seed Co., field seeds.

**ST. LOUIS, MO.**  
Schisler, F. & G. S. Co., A. W., seed merchants.  
Mangelsdorf, Ed. F. & Bro., wholesale field seeds.  
Newman & Malkemus, grass and field seeds.

**ST. PAUL, MINN.**  
Jameson Hevener Co., shippers of field seeds.

**SELMA, ALA.**  
Geo. M. Callen, seed grasses & hay.

**TOLEDO, OHIO.**  
Flower Co., The S. W., seed merchants.  
Hirsch, Henry, whole. flour, seed.  
Toledo Field Seed Co., The, clover, timothy.

**WICHITA, KANS.**  
Floss Bros. Seed Co., fld. seeds, alf., kaffir, sweet corn

### SCREENINGS WANTED.

CORN, WHEAT, Barley, Seed Screenings, Oat Clips and Elevator Offal. Send average sample Geo. B. Matthews & Sons, 420 South Front Street, New Orleans, Louisiana.

OFF-GRADE WHEAT, BARLEY, Corn or Oats, Wheat Screenings, Corn Screenings, Corn Bran, Oat Screenings, Oat Clippings, Barley Screenings, Barley Chaff or elevator screenings of any kind wanted. Please send samples. B. J. Burns, 324 Chamber of Commerce, Buffalo, N. Y.

### WANTED.

BALED CORN HUSKS, Timothy and Clover Hay, Oats Straw and Grain WANTED. THE HAMILTON CO., New Castle, Pa.

### The Toledo Field Seed Co.

#### Clover and Timothy Seed

Consignments solicited Send us your samples.  
TOLEDO, OHIO

### GENUINE

### GRIMM ALFALFA

I have a limited amount of Genuine Minnesota Grown Grimm Alfalfa seed. Highest purity and germination. Write for sample.

H. I. DAVEY

Pine River Minnesota

### Marshall Grain Co.

#### DISTRIBUTORS OF

#### KAFFIR MILO

Weights and Grades  
Guaranteed at destination.

Responsible brokers wanted in all markets

517-18 Grain Exchange Building  
Oklahoma City, Okla.



### SEEDS FOR SALE.

GOLDEN MILLET for sale; 1919 crop.  
D. H. CLARK & SON,  
Galt, Mo.

FOR SALE—MELILOTUS INDICA (Yellow Blossom Sweet Clover), carloads or less.

PHOENIX FLOUR MILLS  
Phoenix, Arizona.

BUCKWHEAT—Car lots and less car lots.  
For Milling, Seed and Scratch Feed.  
H. J. KLINGER & CO., Butler, Pa.

ONE car Alfalfa Seed, one car Timothy, one car Cane Seed for sale.  
Johnston Land Co., Seward, Nebraska

MEDIUM, Mammoth and Alsike Clover Seed of highest quality, for sale. We buy direct from grower. Ask for prices.

WM. M. MONROE & SON,  
Bronson, Michigan.

WHEN IN THE MARKET let us hear from you. We are wholesale dealers in Sweet Clover, Sudan, Millet, Cane, Kaffir, Milo, Feterita, Alfalfa Seed, Seed Corn, Barley, Oats, Rye and Wheat, Poultry Feed.

BOWMAN BROS. SEED CO.,  
Concordia, Kansas.

TIMOTHY, RED CLOVER, SWEET CLOVER, CERTIFIED GRIMMS ALFALFA SEED, KENTUCKY BLUE GRASS, SOLID SEED, RED TOP.

If you want to buy better seeds for less money, ask us for samples and prices. We are long on these items.

WISCONSIN PEDIGREE SEED CO.,

### FLOUR FOR SALE.

MIXED CARS of flour and mill feeds in 100 pound sacks are our specialties. We are now manufacturing a full line of corn goods, cracked corn, feed meal, corn and oats chop, Ohio Farm feed, shelled corn and standard oats, in connection with our flouring mill. Would like to send you a trial to convince you of the superiority of our products. Ansted & Burk Co., Springfield, Ohio.

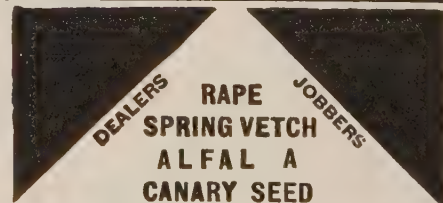
### WANTED

To buy Car Lots of

### Corn Cobs

### Indiana Milling Co.

TERRE HAUTE, INDIANA



### I. L. RADWANER SEED CO.





# THE ILLINOIS SEED CO.

CHICAGO

ILLINOIS

## Buyers and Sellers

Timothy  
Red Clover  
Alsike  
Alfalfa  
Sweet Clover  
White Clover  
Red Top

Blue Grass  
Orchard Grass  
Rye Grasses  
Hungarian  
Millets  
Sowing Rape  
Pop Corn  
Grain Bags, etc.

Field Peas  
Buckwheat  
Seed Corn  
Fodder Corn  
Hemp Seed  
Canary  
Sunflower

IMPORTERS

EXPORTERS

*Ask for Samples if Buyers*

*Mail Samples if Sellers*

# GENUINE TURKESTAN ALFALFA

OUR OWN IMPORTATION

RECLEANED AND POLISHED

CARLOTS OR LESS

PURITY OVER 99%

GERMINATION 85% to 95%

F. O. B. Milwaukee.

Cotton Bags Extra at Value

Over a long period of years this variety has shown its adaptability to the Central and Northwestern States. Its superiority over South European and South American Seeds cannot be questioned. Our recommendation is based on farm results and field tests made with all varieties of alfalfa.

# COURTEEN SEED CO.

Wholesale Field and Grass Seeds

MILWAUKEE

WISCONSIN



## SEEDS FOR SALE—WANTED

**WOOD, STUBBS & CO.**(Inc.)  
LOUISVILLE, KY.Ky. Blue — Orchard — Red Top  
BUY AND SELL  
Also full line Garden Seeds**The Stanford Seed Company, Inc.**

Wholesale Field Seeds .. BUFFALO, N. Y.

**WHITNEY-ECKSTEIN SEED CO.**

Wholesale Seed Merchants

BUFFALO, N. Y.

CORRESPONDENCE INVITED

**The Mangelsdorf Seed Co.**Sweet Clover, Alfalfa,  
Soudan Grass, Millet, Rape.

ATCHISON

KANSAS

H. W. DOUGHTEN, 59 PEARL STREET  
NEW YORK CITYImporters, Exporters and Jobbers  
**Grass and Field Seeds**  
We Are Buyers of NEW CROP ALSIKE  
and Sellers of D. E. RAPE**FarmerSeed & NurseryCo.**

Growers of Northern Grown

SEED CORN, CLOVERS, TIMOTHY  
AND ALFALFA

FARIBAULT - - MINN

**The J. M. McCullough's Sons Co.**

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FLOWER, FIELD and LAWN SEED

**J. OLIVER JOHNSON**

Wholesale

SEED MERCHANT

1805-9 Milwaukee Ave., CHICAGO, ILLINOIS

**LOUISVILLE SEED COMPANY**

INCORPORATED

LOUISVILLE, KY.

Headquarters for

RED TOP AND ORCHARD GRASS

BUYERS AND SELLERS  
OF ALL VARIETIES FIELD SEEDS**HENRY HIRSCH**

WHOLESALE FIELD SEEDS

CLOVER — ALSIKE — TIMOTHY — ALFALFA

Our Specialty

All Other Field Seeds

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**CONTINENTAL SEED COMPANY**

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**GRASS and  
CLOVER SEED**Buyers and Sellers of Timothy, Red Clover,  
Alsike, Alfalfa, White Clover, etc.NUNGESSER-DICKINSON SEED CO.  
New York, N. Y., U. S. A.**TIMOTHY SUDAN RUDY PATRICK  
ALFALFA  
FEED MILLET SEED CO. KANSAS  
CITY, MO.**We Buy **SEEDS** We Sell**J. G. PEPPARD SEED Co.**  
Kansas City, Mo.**SEEDS ANY and EVERY  
KIND**

CAR LOTS or LESS

**The Nebraska Seed Co.**

Omaha, Neb.

**WANTED**Wild Mustard Seed and Wild Mustard Seed  
Screenings. Send samples and prices.  
Sunflower Seed and Buckwheat for sale.**P. L. ZIMMERMANN CO.**  
ST. LOUIS, MO.**Crabbs Reynolds Taylor Company**

CRAWFORDSVILLE, INDIANA

Buyers and Sellers

CLOVER AND TIMOTHY SEED—GRAIN

**Wm. G. Scarlett & Co.**

Baltimore, Md.

Buyers and Sellers

SEED GRAINS, RAPE,  
VETCHES, SOYBEANS, RED,  
CRIMSON, WHITE AND AL-  
SIKE CLOVERS, ALFALFA,  
SWEET CLOVER, RYE-GRASS,  
TIMOTHY, REDTOP, MILLET,  
ETC.

REGISTERED BRANDS

**MINNEAPOLIS SEED COMPANY**

WHOLESALE FIELD SEEDS

**HARDY NORTHERN GROWN SEEDS OUR SPECIALTY**

ASK OUR BIDS BEFORE SELLING. BUYERS, RECLEANERS, SELLERS. WRITE OR WIRE FOR SAMPLES AND PRICES.

**TIMOTHY, CLOVERS, MILLETS**

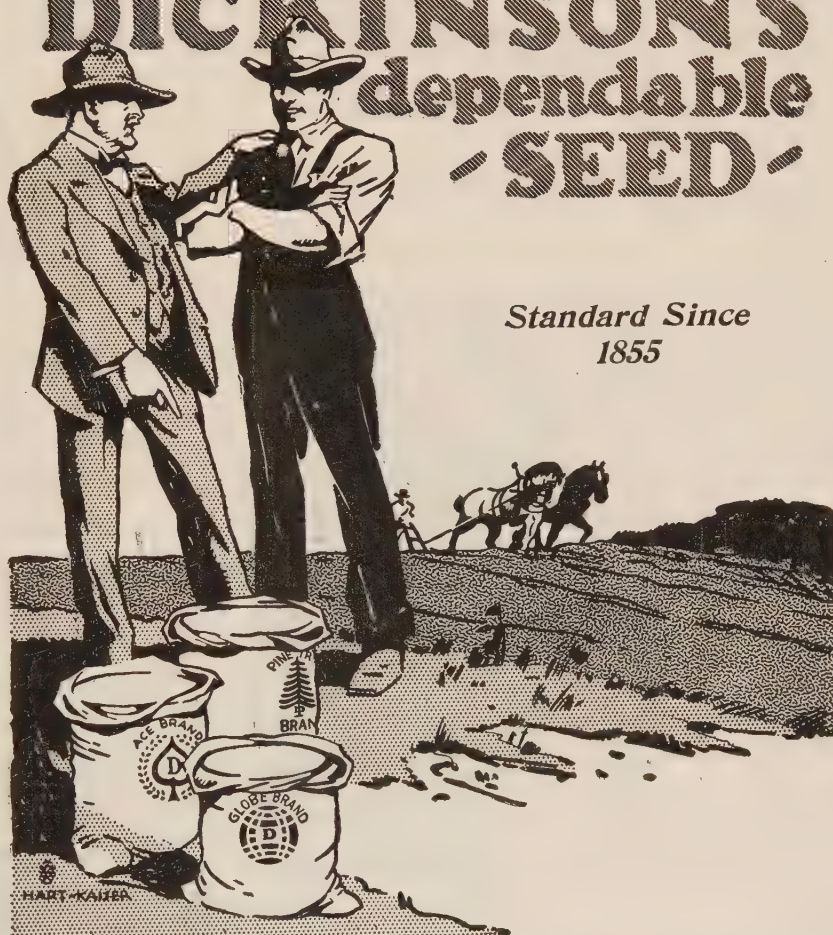
GRASSES, FORAGE SEEDS, SEED GRAINS, PEAS, BEANS AND SCREENINGS

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34TH TO 35TH STS. AND RAILROAD  
AVE. SO., ON O. M. & ST. P. RY.P. O. ADDRESS: LOCK DRAWER 1546  
OFFICES: 3444 RAILROAD AVE. SO.  
MINNEAPOLIS, MINN.GRAIN ELEVATORS AND WAREHOUSES:  
35TH TO 37TH STS. AND RAILROAD  
AVE. SO., ON O. M. & ST. P. RY.



# Better Crops!

**Lighten the Burden of  
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DICKINSON'S  
dependable  
- SEED -**



*Standard Since  
1855*

## Globe, Pine Tree and Ace Brands

*Clover, Timothy, Alsike, Alfalfa and  
Field Seeds of All Kinds*

ORDER NOW WHILE OUR STOCKS ARE COMPLETE

# THE ALBERT DICKINSON CO.

WHOLESALE SEED MERCHANTS

Minneapolis

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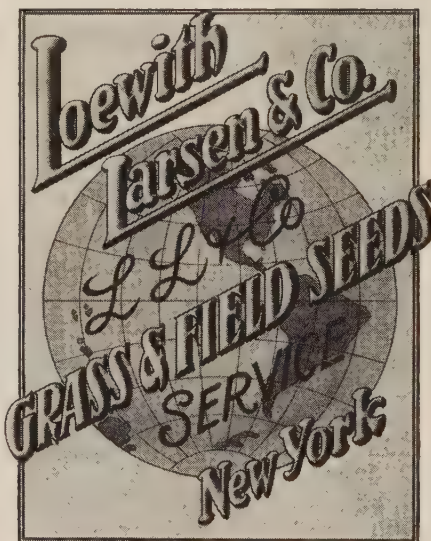
## SEEDS FOR SALE—WANTED

**Crawfordsville Seed Co.**

Crawfordsville, Ind.

**FIELD SEEDS****WE BUY SEEDS**Fancy Grains, Clovers, Grass  
Seeds, Timothy, Field Peas*Send Samples for Our Bids  
Sample Envelopes on Request***NORTHRUP, KING & CO.**  
SEEDSMENMinneapolis Salt Lake City  
Minn. UtahSend Samples  
Millets and Seed Grains  
to**THE BELT SEED CO.**Importers and Exporters  
BaltimoreWe Offer Alfalfa, Red Clover and  
Crimson Clover at Very  
Attractive Prices**The S. W. Flower Co.**WHOLESALE  
**FIELD SEED**  
MERCHANTSSPECIALTIES  
RED CLOVER, TIMOTHY  
ALSIKE  
**TOLEDO**  
OHIO**SEED OATS**Have in stock several thousand  
bushels fancy No. 2 White Oats,  
weighing 35 lbs. or over. Will  
sell them at \$1.25 per bushel  
F. O. B. cars Peoria, Ill.*Send in your order early.***G. C. McFadden & Co.**  
PEORIA, ILL.**MISSOURI BRAND SEEDS**

Specialists

KANSAS GROWN ALFALFA  
MISSOURI GROWN BLUE GRASS  
**MISSOURI SEED CO.**  
KANSAS CITY, MISSOURI**SOY BEANS****O. M. Scott & Sons Co.**  
Marysville, OhioWe are buyers of  
Clovers, Timothy and Alfalfa  
**SEEDS***Send Samples***L. TEWELES SEED COMPANY**  
MILWAUKEE, WIS.**A Trial Order****GRAIN DEALERS JOURNAL**

305 So. La Salle St., Chicago, Ill.

Gentlemen:—I wish to try the *Grain Dealers Journal* on the 10th and 25th of each month for one year just to learn if I can get any helpful suggestions from the opinions and experiences of other grain dealers. Enclosed please find One Dollar and Seventy-Five Cents.

Name of Firm.....

Capacity of Elevator..... Post Office.....

..... bus. State.....

Use Universal Grain Code and Reduce Your Tolls.

*Write to Kellogg*

We are paying top-o'-the-market prices for

ALSIKE  
RED CLOVER  
WHITE CLOVER  
ALFALFA  
SWEET CLOVER

Send us Samples

Whenever you're in the market—to buy or to sell—in carload lots or less—it will always pay you to write to Kellogg.

**Kellogg Seed Co.**  
FIELD and GRASS  
184-5 FLORIDA ST. MILWAUKEE, WIS.





# Greater Yield



Can Your Farmer Patron Be Assured  
By Buying Hardy Wisconsin Grown

## CLOVERS

Medium Mammoth Alsike

Sold Under the

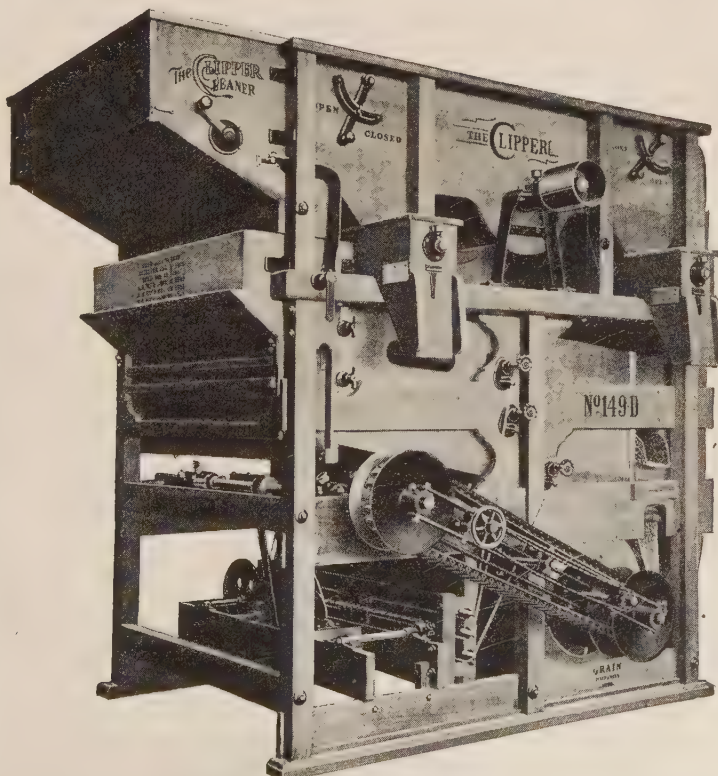
Famous Krop-King Brand

By the

# MILWAUKEE SEED CO.

MILWAUKEE, WISCONSIN

## The "CLIPPER" DOUBLE SUCTION GRAIN CLEANER



No other machine has as many wonderful TIME SAVING and LABOR SAVING devices as the Clipper Cleaners. Skilled labor is daily becoming more difficult to secure. Every move, every minute saved will enable you the more easily to take care of your business.

The Variable Air Regulator enables you to secure any desired degree of air blast without any annoying changes of pulleys or shifting of belts. A simple turn of the wheel does the trick.

Our Double Screen System permits the oats screen and the wheat screen to be left constantly in the shoe. The grain is shifted by means of a gate onto the screen it is desired to use.

Our Traveling Brush System keeps the screens from clogging and the machine working at its highest efficiency ALL THE TIME.

Ball Bearings on Exhaust Fans reduce friction, eliminate hot bearings and save power.

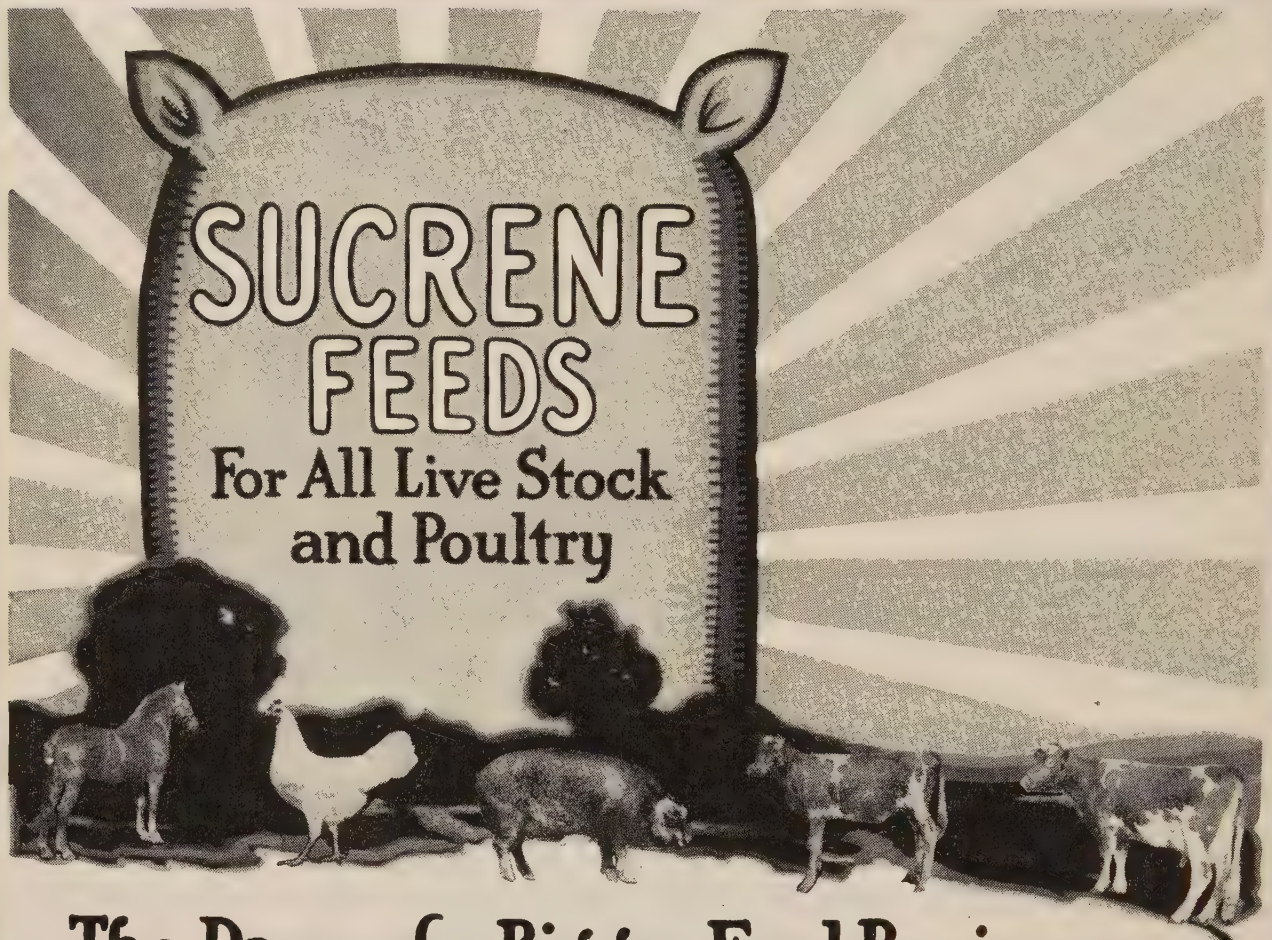
The yearly balance sheet of the user of Clipper Cleaners shows that it pays to have the best that can be procured in the line of grain or seed cleaning machinery.

*Write for catalog*

**A. T. FERRELL & CO.**

**Saginaw, W. S., Mich.**





## The Dawn of a Bigger Feed Business

Yes, we are ready for business in a bigger way than ever before. On the ruins of our main plant at Peoria, Ill., destroyed by fire some months ago, there now stands one of the greatest and finest feed mills in the world. It is turning out standard quality Sucrene Feeds at the rate of 30 cars per day.

### The Business Is Ready for You — Are You Ready for the Business?

Conditions in the feed business are more favorable to feed dealers than in many years past. The shortage of mill products and protein feeds generally, low visible supply of grain, reasonable certainty of firm prices of live stock products, etc.—these factors justify optimism on the part of feed dealers. They create an opening for profitable trade which Sucrene Feeds fill permanently because they satisfy.

#### The Sucrene Line Includes:

Sucrene Dairy Feed.  
Sucrene Calf Meal.  
Sucrene Hog Meal.  
Sucrene Horse Feed  
with Alfalfa.  
Sucrene Poultry Mash  
with Buttermilk.  
Sucrene Scratch Feeds.  
Amco Fat Maker for  
Steers.

Sucrene Feeds are firmly established in popular favor; they are strongly backed; the supply is steady; always uniform in quality; prices to dealers are right.

*Let us place your order on our books at once for a supply that will enable you to take care of the trade in your locality.*

## American Milling Company

Main Office and Mills: PEORIA, ILLINOIS

Branch Offices: Philadelphia, Pa., 205 The Bourse  
Boston, Mass., 85 Broad Street, Boston 9  
Cleveland, Ohio, 419 Sloan Bldg.



## GRAIN DEALERS JOURNAL

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**THE ADVERTISING** value of the Grain Dealers Journal as a medium for reaching progressive grain dealers and elevator men is unquestioned. The character and number of advertisements in its columns tell of its worth. If you would be classed with the leading firms catering to the grain trade place your announcements in the Journal.

Advertisements of meritorious grain elevator machinery and supplies and of responsible firms who seek to serve grain dealers are solicited. We will not knowingly permit our pages to be used by irresponsible firms for advertising a fake or a swindle.

**LETTERS** on subjects of interest to those engaged in the grain trade, news items, reports on crops, grain movement, new grain firms, new grain elevators, contemplated improvements, grain receipts, shipments, and cars leaking grain in transit, are always welcome. Let us hear from you.

**QUERIES** for grain trade information not found in the Journal are invited. Address "Asked-Answered" department. The service is free.

CHICAGO, MARCH 25, 1920

**KEEP ON GOOD TERMS** with the local freight agent. Make his work easy and he will see fit to pay for grain door lumber.

**WATER** did much more damage than fire recently in a Chicago concrete elevator. Underwriters would profit by inducing firemen to be more sparing of water.

**HESSIAN FLY**, long business agent of the Crok Killers' Union, is persistently prophesying higher prices for wheat. Evidently the long, hard winter seriously interfered with the work of this old time agitator.

**OATS** climbed to the highest price on record, \$1 per bushel, at Chicago Mar. 18, but no one has been able to prove it is due to the efforts of the Horse Publicity Ass'n or to the high price of gasoline.

**THE LEVER** food control law has been getting hard knocks in the courts, as it violates the sixth amendment to the Constitution; but the grain trade practically to a man is living up to its agreements with the Wheat Director.

**COAL** will be given 30% lower freight rates in summer than in winter if Senator Frelinghuysen's bill becomes a law, and the retail trade will be confined to dealers having modern facilities for storing fuel until needed.

**FARMERS** who reduce their acreage sown to grain on account of the scarcity of labor must be overlooking the cost of keeping high priced land in idleness. The new valuations are so high all farmers must use fertilizers liberally and cultivate intensively even tho all the work be done by machinery.

**CANADIAN MILLERS** will buy no floral offerings for the Wheat Control Board until the funeral date is announced.

**SHIPPERS** who really want cars will follow up their first order with persistent demands upon all the officials interested.

A **LIBERAL REWARD** is offered to anyone who will present a diagram of the declines in food prices brought about by the efforts of the Department of Justice. The press has been persistently flooded with copies of silly suggestions, but old H. C. L. persists in soaring.

**ONE SHIPPER**, writing in "Letters" this number, does not propose to turn over to the carrier all his original evidence of loss and damage and then wait until doomsday for payment. If more shippers would take a similar aggressive stand more claims would be settled without delay.

**SCARCITY** of cars seems not to have been alleviated by the arrival of Spring, but the railroads are gaining more confidence in the future, and are hustling not only to get their facilities in condition but to move the shipments obtained. The great pity of it is that few of the cars offered grain shippers are in fit condition to transport grain.

**ELEVATOR** operators who suffer mysterious losses of grain will be enlightened by the finding of fifteen bushels of hard wheat under one of the bins in the old elevator now being remodeled at Prairie du Chien, Wis., illustrated on page 265 of the Journal for Feb. 10, 1920. As no wheat had been handled through this elevator for fifteen years, it will be impossible to trace the shortage to any specific lot, but the wheat has been found in good condition.

**WAR'S** unsettlement of business brot many controversies that remain to be threshed out in the courts; and one of the first precedents that may serve as a guide in many cases is published elsewhere in this number, upholding a seller of oats in canceling a contract because the buyer did not get the contract as soon as represented and could not give number of contract so that seller could procure the necessary railroad control permits for his country shippers.

**CORN CORNERS** that exist only in the imagination of the space writer do not appeal to a public weary of propaganda, especially when that newspaper's readers have been repeatedly misled. This same newspaper has been advocating government operation of railroads, since proved a fizzle; has advocated government operation of ships, proved unprofitable under American standards of wages; when government bonds first sold under 95 persistently assured its readers they would soon go above par, while its financial writer wasted columns of ink early last fall condemning the federal reserve board for raising the interest, a move that since has proved timely, salutary and unavoidable. The war has so educated the citizenry that these appeals to prejudice no longer profit mercenary agitators.

**CLAUSES** in side track leases soon will be subjected to determined attack by competent attorneys, the National Industrial Traffic League being prepared to file suit to have unfair imposition of liability stricken out.

**DIFFICULTY** with a shipper over a side-track lease does not warrant a railroad company in refusing to set in cars for loading or in denying service generally, is the salutary decision of the Supreme Court of Ohio published on page 566. If the demands of a railroad corporation are so unreasonable that it has no standing in law it can not resort to duress to force collection.

**SHIPPERS** wishing to provide in their last will and testament some valuable bequest that is certain to be saved for the benefit of their great grandchildren should file a claim for loss or damage or overcharge accruing during the federal control of railroads. The control has ended, but the Washington authorities have not yet decided how these claims will be handled. Are they saving them to be mulled over in the departments to give employment to the 100,000 extra clerks now wasting time at the capital?

**FERTILIZER DEALERS** will be pleased to know that the Secretary of War still has faith in the Government's ability to manufacture commercial fertilizers, and only this week told the Senate Agricultural Committee that the Government could operate the Muscle Shoals nitrate plant at an annual profit of \$2,900,000. Inasmuch as Congress has already sunk over \$100,000,000 in this project, it does not seem likely that it will authorize any further investment in the operation of the so-called War Enterprise.

**SIDE TRACK** maintenance should not be assessed against the owners of any property located along the track as track is invariably used extensively by shippers other than the one having a building located upon it. The railroads could with as much reason ask those having buildings near their track to pay the entire cost of maintaining right-of-way. As long as shippers will submit to their demands the carriers are simply encouraged to think up new tricks for raising revenue from untried sources.

**ALL CRITICISM** of the federal grades would be disarmed if the government would abandon classification, on the certificate, which is not necessary, and content itself with issuing a grade certificate stating the exact percentage of test weight, moisture, damaged kernels, foreign matter, mixtures, etc. Any one then could determine the classification from the certificate. In other words, the government licensed inspectors could omit the grade No. 1, No. 2, etc., from the certificate, while the Department of Agriculture retained regulations that would prevent anyone from offering in interstate commerce grain as No. 1, No. 2, etc., that did not come up to the percentage limits of the present well studied grading system. By such a change it would be brot home to the Northwestern wheat growers, that buyers and not the government set the value on the grain.



THE MORE GENERAL use of confirmation blanks would give both parties to every contract more opportunity to detect misunderstandings before it is too late to correct without loss to either party. Where correction of error is made immediately at least one party to the contract shows that he is not willing to profit by the misunderstandings of the other.

ELEVATOR OWNERS who have attempted to rebuild their plants with their insurance money have been sorely disappointed, because the cost of construction and equipment has so far exceeded their expectations that they now find that their plants were not insured for one-half of their real value. An appraisal of many plants would justify a material increase in the amount of fire insurance carried on the building.

A COMFORTABLE RECEPTION room for your farmer customers, many of whom come long distances, when supplied with papers and information regarding markets, is always appreciated by the grain growers and in most cases it promotes cordial relations and helps business. A kindly consideration for customers, as well as an evident desire to serve, not only overcomes petty prejudices, but builds up good will, a most valuable asset.

CONCRETE MIXTURES should be comparatively dry, as stated by an expert at the meeting of scalemen reported elsewhere in this number, a result that can hardly be obtained under the present loose specifications for concrete work. It were more practical to call on the builders for results than to limit them in the method of accomplishment, to require a certain strength rather than a certain mix, just as the mixed feed manufacturers can choose their own component ingredients as long as the protein and carbohydrates come up to the labeled percentage.

QUESTIONNAIRES sent out by the Federal Trade Commission need not be answered, under the opinion of the attorney of the Illinois Manufacturers' Ass'n, when they go into such detail as the filing of monthly cost sheets and monthly production records, as Congress has no power to authorize the Commission to gather such detailed information unless at the will of the informant. Another flaw in the Commission's procedure is raised in the case of the U. S. v. Basic Products Co., that the collection of this information attempts to authorize search and seizure without charge of wrong doing.

WHEAT LOSS claims the direct result of the Government's fixing the price far below the real market value will be pushed vigorously by the grain dealers ass'ns of Illinois, Missouri, Kansas and Oklahoma. Many grain dealers in the course of their regular business accumulated much wheat of the 1917 crop which they could not get cars to ship out before the Government fixed the price at \$2. They held the wheat against their own wishes and should not be made to suffer loss on account of the price being fixed by the Government. However, no dealer will be reimbursed for his loss unless his claim is filed and payment persistently demanded. All who lost shud join together and demand reimbursement.

AN ELEVATOR MAN at Tripp, South Dakota received a large carload of coal recently that he had not ordered, and in fact, which was not even consigned to him, but it jumped the switch near his house and ran right through the corner of the elevator with the result that part of his cribbed elevator had to be rebuilt. Under the new lease with which the Railroads are now favoring elevator owners, he will, of course, be required to replace the coal and repair the car, as well as the track. This new spirit of fairness which the Railroads are evidencing in their leases and track maintenance contracts is very broad, and always brings joy to the elevator owners called upon to sign one.

WOULD-BE grain shippers of the West have been much annoyed by their inability to obtain box cars for shipping out grain while the markets were favorable, but the grain and feed dealers of the Eastern States, which were snowbound for a long period without getting in any fresh supplies, were sorely distressed by the pleadings of the owners of live stock who were confronted with the dilemma of either shooting their animals or condemning them to starve to death. Of course, the trouble can be traced back to the Max Thelen brand of Government efficiency in operating the railroads. Left with worn out power and poor equipment of box cars, the railroads were so crippled that they were unable to meet the extra demands made upon them by reason of the storms in the East.

### Freight Rate Increases Certain.

The Interstate Commerce Commission is now conducting an investigation to determine how much it must permit the railroads to advance freight rates, in order to pay the increase already granted the railway employees, and still show a return of 5½ per cent to the railroad companies, guaranteed by the latest law.

The government during its twenty-six weeks of railroad operation advanced the wages of the employees \$1,030,000,000, and this week the representatives of the employees presented demands to the Joint Conference for further increase amounting to \$1,100,000,000. If the railroads meet both the new and old increases it will necessitate an increase in the freight charges of approximately 50 per cent.

Grain shippers are interested in being able to pay grain producers as near the terminal market price as possible, as they have less to explain and more satisfied customers. Any further increase in freight rates must quickly be reflected in the selling prices and a 50 per cent increase is sure to effect a marked advance in the selling prices of all commodities. Even the railroad employees who will profit most by the change will find it necessary to contribute their share to the increased freight rates in the higher prices they will have to pay for their necessities.

Shippers now holding grain bought on the basis of present freight rates owe it to themselves to move out all their grain, at the present tariff rates, even though it be sold for distant delivery. The early advance in freight rates may wipe out any present margin of profit they may have in their holdings.

## The Proposed Change in Wheat Grades.

Last week a delegation from the Spring Wheat States called upon the new Secretary of Agriculture and tried to convince him that five cars of No. 3 Northern would have been graded No. 1 Northern except for some non-essential technicalities, such as are indicated in the table given elsewhere in this number. The agitators made sufficient impression upon the new secretary to induce him to call a hearing on the proposed changes at Chicago, April 2nd.

Judging from the statement in the call and the proposed changes published in full elsewhere in this number, these agitators, none of whom seem to be either wheat growers or wheat handlers, have not yet arrived at a definite understanding of exactly what is wanted, but in keeping with the general unrest of the day, they feel that a change should be made.

The Federal wheat grades were put into effect August 1st, 1917, and were given virtually no test until the Government fixed the price of the different grades. Since then the wheat trade has been arbitrarily driven along by the dictators of the Grain Corporation; it has not been given an opportunity to function as it might deem proper, or best. Not much consideration has been given to the merits or demerits of the new wheat grades, so that virtually before the trade has had an opportunity to put the new grades to a real test, along comes a band of northwestern agitators, of unknown interest in the problem, demanding a change.

If Mr. Jacobson thinks No Grade wheat will look all right to Mr. Wheat Buyer if it be called No. 1 Northern, he has much to learn. Wheat buyers everywhere depend upon their own judgment for 90% of their guidance in buying and seldom over 10% upon the judgment of the inspectors. Real No. 1 Northern wheat by any other name would look just as valuable to the experienced wheat buyer, although he might try to buy it for less money.

Northwestern growers of trash would not be convinced that their crop possessed superior merit because the misguided inspection department graded it No. 1 Northern. Value cannot be injected into poor wheat by tricky grading. If the work of the grain inspector is ever to become a positive force in the marketing of grain, then the inspectors must always call a spade a spade. The wheat growers and the wheat dealers know values fully as well as any of the inspectors, and many of them know it far better, so that the inspectors cannot hope long to deceive any experienced dealer through tricky rules or false grading. Surely the work of the inspectors can not long be of value to anyone, unless those served have confidence in the sincerity and honesty of those controlling the inspection work.

The grain trade needs and wants uniform inspection rules and uniform grading, but if the whole system is to be rent asunder at frequent intervals to satisfy the whim of some loud mouthed agitators, the much longed for uniformity will never be attained. It takes a long time for all the great army of license inspectors of wheat handlers to become thor-



oughly familiar with the limitations of each grade, and each change in a rule or the grade postpones the attainment of uniformity.

The movement of the new crop is too near at hand to consider calmly any changes in the grades. If the members of the trade and the growers who are more interested than anyone else, decide after giving the present grades a fair trial that some changes would facilitate the fair grading of spring wheat, then, no doubt, a change can be readily obtained without any interference on the part of vote-chasing politicians.

## The Future Course of Prices.

Cash handlers of and dealers in grain, while carrying large stocks on hand, nearly always manage to keep their position on one side of the market offset by commitments in the other direction, so that price movements are not of such importance to them as commonly supposed.

Having to deal in commodities that are fluctuating the grain dealers are better informed than the average merchant on the principles that govern price movements, and usually need only to know the conditions existing in order to forecast for themselves the future course of prices. Most dealers who happen to be in an unhedged position are well enough satisfied to be on the right side of the market without attempting to pick the exact quotation that will be reached at the extreme culmination of a market movement.

One of the conditions that is soon to make itself felt is the diminished production of gold the world over, and especially in the United States. The gold production in the United States declined from \$101,035,700 in 1915 to \$68,646,700 in 1918, and is estimated by competent authority to have been only \$58,488,800 in 1919. At the same time the use of gold in the arts and for jewelry has increased. Sales of bullion to the trade indicate the consumption for 1919 to be \$80,337,600, exceeding by \$21,848,800 the output of the mines.

These figures are small compared with our accumulated stocks; but our stocks are affected by exports. It is true our stock of gold January 1, 1920, was \$2,788,000,000, against \$1,816,000,000 on January 1, 1915, as reported by the Treasury.

For the seven months ending with January our exports of gold were reported by the Department of Commerce to have been \$318,934,774; and for the one month of January alone \$57,758,000. Exports of silver for the seven months were \$121,543,290, more than offsetting the combined imports of gold and silver, \$38,000,000 and \$55,000,000 respectively. Continued at the January rate it is only a question of time until our stock of gold will fall below the pre-war standard.

From January 1 to March 10 United States gold exports totaled \$119,182,202; and for the ten days ended March 10, \$28,316,953.

Our present inflated prices are based on paper money, and credits, and the withdrawal of \$1 in gold cuts down the bank credits \$8 to \$10. The deduction is that bank credits must be curtailed, paper currency called in and our volume of business carried on with a

smaller volume of cash and credits, with consequent lower prices all round. All commodities will not drop at the same time. First one and then another will find a lower level. This may not apply to old crop futures such as May oats and May corn, but will apply to the new crop.

## When Will Wheat Control End?

Rumor persistently credits the Wheat Director with asking Congress for a 30 or 60 day extension of the wheat guaranty. Inasmuch as the Wheat Director has refused to pay more than \$2.26 basis Chicago for stored wheat on the last day of the wheat control, it is not likely that the owners of wheat, who have accumulated fat storage charges, will be anxious to recommend an extension of the guaranty. Neither will they forget about the accrued charges and turn over their holdings to the Wheat Director at \$2.26. They will prefer to take the chances of the market.

It has been suggested that the next crop of winter wheat will be slow to move because of the scarcity of cars and locomotives, and if the markets are at all wild, many wheat shippers will hesitate to buy because their recent experience with the railroads convinces them that it is next to impossible to market grain as one desires.

The law has fixed the last day of May for the end of the wheat price guaranty. Dealers generally will strive to reduce their stocks to a minimum before that date, and few will be in a hurry to accumulate any stock after the termination of the wheat control, until the market becomes somewhat settled. So long as European exchange is at such a heavy discount and many of the nations cannot obtain credit, the European purchasing power will not be large. With a surplus of wheat and wheat products on hand, the United States markets in June should not vacillate greatly, unless the condition of our winter wheat crop declines below normal.

The average wheat handler will, no doubt, be glad to have a month or two of quiet markets and light movement after the termination of wheat control, in order that he may get his bearings and become adjusted to the new conditions of doing business.

Wheat dealers everywhere will be pleased to have their former freedom of action again. Most of them have tried to comply with the spirit and the letter of the wheat directors' regulations, notwithstanding the war is over and the constitution would protect dealers in freedom of action persistently denied them by the Grain Corporation.

The long period of Government regulation of the grain trade has been most trying and most expensive to the wheat handlers of the land. Merchants generally have not been long restricted in any of their dealings and the labor unions have been gorged with the granting of their greedy demands, yet the wheat handler has been limited to a gross profit of eight cents a bushel which is many cases did not pay the handling cost.

The termination of the wheat control cannot come any too soon to satisfy the wheat dealers; the war is over.

## Would Require Railroads to Build Seaport Elevators.

Trunk line railroads having terminals at Atlantic Coast ports have long recognized the need of better grain storage and handling facilities, and most of them have seriously considered the building of additional elevators, but the press of their necessities, such as \$10,000,000 hotels has crowded the plans for the needed elevators into the background, obliging the railroads to continue to store grain in their box cars. They cannot overlook the fact that such storage is the most expensive obtainable, but nevertheless, the delay in building the elevators has forced the railroads to adopt this expedient.

Congressman Anderson, who seems somewhat wearied by the railroad's procrastination, has introduced a bill authorizing the Interstate Commerce Commission to require the railroads to establish storehouse facilities at all seaports, in the hope of expediting the efficient handling of grain.

A Minnesota delegation, no doubt, friends of the Congressman, appeared before the House Committee this week in support of his bill. While such legislation may help to impress the railroads with the crying need for better grain handling facilities, at our seaports, it is not likely to attain the desired results until the railroads can borrow money at a reasonable rate for the needed improvements. No grain shipping route has capacity in excess of the handling capacity of the weakest link in the system.

The great State of New York has spent \$150,000,000 in building a canal, yet its carrying capacity is reduced to the minimum by lack of boat loading facilities at the canal's Western terminal, and even a greater lack of unloading facilities in New York harbor.

No system of transportation which is largely dependent upon grain for tonnage is complete without modern, up-to-date grain elevators, and no system can be operated efficiently or without great waste and expensive delays until the needed elevators have been provided. Shippers and exporters will both be much pleased when better terminal facilities are handling grain at every port, because the stream of shipments will then go forward without delay, and box cars will always be more readily obtained.

SO MANY of the fires reported in our news columns during recent months have been credited direct to preventable causes, that it would seem the members of the grain trade would soon be so deeply impressed by this useless fire waste, they would take steps to prevent any more fires being so caused. Shingle roofs, defective chimneys and overheated stoves have contributed more than their share. According to statistics compiled by the National Board of Fire Underwriters, about one-fourth of the fire losses paid during 1918 were traceable direct to preventable causes. Defective chimneys and flues were credited with losses aggregating \$11,985,782, matches and cigar stubs caused losses amounting to \$16,453,652, sparks on roof earned a credit of \$6,703,037, while overheated stoves, furnaces, boilers and their pipes caused a loss of only \$12,234,455. All of these causes are easily preventable, and they are known to elevator operators. If every elevator owner would exercise greater vigilance and resolve repeatedly to protect his property from the known fire hazards, these preventable losses would be greatly reduced, and the cost of insurance in the mutual fire companies also would be reduced.



## Asked— Answered

[Readers who fail to find information desired on any subject of interest to grain dealers should send us their query for free publication here. The experience of your brother dealers is worth consulting. Replies to queries are solicited.]

### To Generate or Buy Electric Current?

*Grain Dealers Journal:* In reply to G. Lee Barnes of the Three Rivers Co-operative Ass'n, asking in the Journal whether it would be more practical to produce his own current or to buy power, we would say that it does not pay to generate one's own current for less than 40 to 50 K. W. when commercial current can be purchased.

It would pay to run an oil engine to generate 50 K. W. which would take care of 125 h. p. in individual electric motor drives.

When crude oil costs \$1.50 a barrel as it did a few years ago electricity can be generated at a cost of 1 cent per kilowatt hour, allowing nothing for superintendence, etc.—W. R. Crawford, engineering dept., Allis-Chalmers Mfg. Co., Chicago, Ill.

### Higher Rate to Intermediate Point?

*Grain Dealers Journal:* The C. M. & St. P. Railway Co. is endeavoring to collect additional freight charges from us, and other shippers, on hay shipped from Kittitas, Washington, to St. Maries, Idaho. They publish a commodity tariff on hay from Seattle to St. Maries of 24c per hundred weight. Kittitas being directly intermediate, we hold that we should have the benefit of the Seattle rate. Their auditors office is endeavoring to collect from us on basis of rate Kittitas to Spokane, and Spokane to St. Maries. In other words they are endeavoring to collect a greater amount for a short haul than they are for the long haul. We have refused payment of the additional charge.—Geo. O'Dwyer, Inc., St. Maries, Ida.

**Ans.:** This may be one of the many cases where special permission was granted by the Interstate Commerce Commission to charge the higher rate.

The tariff department of the C. M. & St. P. at Chicago states their belief that the shipper should have been protected in the thru rate and has written the western office for the tariffs governing.

### Which Grade Governs Sale?

*Grain Dealers Journal:* Last December we sold a car of No. 3 yellow corn our track to company A and billed it to Aberdeen for beyond. Company A sold it to Company B and discounted us 7½ cents per bushel, saying car graded No. 4 yellow, basing grade on sample we had sent them.

We have found that the car was billed from Aberdeen to Montana to Company C; and that the last named had accepted the car as No. 3 yellow corn.

Have they any right to grade a car at Minneapolis that has never seen Minneapolis? Is acceptance of car by Company C proof that we sent No. 3 corn? The inspection certificate sent me does not specify that it was taken from the car.—J. P. Olson, mgr. Gayville Grain Co., Gayville, S. D.

**Ans.:** The inspection department has a right to grade any sample irrespective of the whereabouts of the car, the certificate stating that it certifies to grade of one sample only. As this sample was sent in by the shipper himself he cannot question the sampling. He could appeal from the grading and have the sample re-inspected if in his opinion the first inspection was erroneous. Further than that the shipper cannot go, as he is governed by the terms of the contract and the rules of the market where the buyer does business. Acceptance of the car on a contract for No. 3 by any subsequent buyer

is not proof that the grade actually was No. 3, and in fact the buyer, if he learned the grain had once been graded No. 4 might refuse it.

### Payment for Grain Doors?

*Grain Dealers Journal:* I had grain doors ordered over 60 days ahead, and on Feb. 5 a car was spotted for me to load, but no doors. So I purchased lumber for it and paid for it myself.

On Feb. 7 another car was spotted and I again purchased lumber. After I had the second car loaded and billed the freight brot in 50 doors. I knew these doors were due here, but I had to rush these cars to fill contracts and the agent here does not keep us supplied with doors.

Can I collect the amount I paid for the lumber. I sent the bill to the superintendent and he said it was against the Interstate Commerce Act to allow a bill for lumber which was purchased by me without the agent's consent.

If I had waited for all their red tape it would have taken three days. I am going to collect even if it is only \$8.45. I am getting tired of this "wait till you get it" stuff.—W. B. Lau, Klemme, Ia.

**Ans.:** The superintendent is correct in his statement that a bill for lumber purchased by shipper without agent's consent cannot be allowed. Local agents have ample authority by tariff to purchase lumber when none is on hand, and there is nothing to prevent the local agent from purchasing the lumber from the shipper.

There is still an opportunity to settle this with the local agent who can pay for the lumber at any time before or after shipment. The local agent should be prevailed upon to pay for the lumber. It is all the same to the agent if he buys the lumber from a lumber yard or from a shipper. The rules do not specify from whom he shall purchase the lumber.

### Delay in Giving Shipping Instructions?

*Grain Dealers Journal:* On Nov. 11, we sold 2500 barrels of flour to the Grain Corporation, in 140-lb. jute sacks at \$10.45 per barrel, delivered Baltimore. Shipment within 60 days. In the confirmation of these orders, the Grain Corporation inserted an addition to the word 140-lb. jute "Or Bulk" and we accepted that proviso. You will note that prevented our proceeding with the making of any part of this flour, until shipping directions were furnished to us by the Grain Corporation.

After persistently writing and asking for shipping direction and on the 49th day we received same by wire. We proceeded at once and got order out as quickly as possible, in accordance with the full spirit of the contract, but we are now demanding an allowance of 40c per barrel to compensate us for waiting on these shipping directions 49 days. We make the claim that our quotation on this flour at \$10.45 was figured down to a minimum cost of operation, due to the fact that we work the full capacity of our mills. We kept in our employ two live organizations under full pay, waiting daily for these directions. This expense advances the price of the flour that we produced nearly 53c a barrel over and above the regular cost of operation. And we believe their failure to follow the customs that they themselves established since the beginning of the Grain Corp. namely, that of giving shipping directions three or four days after the acceptance of the contract, makes that custom practically a law. They ignored that custom without acquainting us with the fact.

We have appealed the case to them and they have offered us 13c a barrel for 900 barrels. This we have so far rejected.—The Hanley Milling Co., Coshocton, O.

**Ans.:** In order to hold the buyer seller must show that buyer was advised that seller's capacity was so limited that it was imperative that he be informed immediately whether the flour was to go out in jute or bulk, and that loss was bound to result from the delay. Such showing, taken in connection with the trade custom, makes the Grain Corporation liable. A

moral reason supporting the claim is that the miller quoted a lower price on the supposition that he would get shipping directions in time to keep his mill running steadily at low cost.

### Do Federal Administration Rates Supersede State Statutes?

*Grain Dealers Journal:* Previous to the Federal Railway Administration on Ohio intrastate traffic, application of grain rate from points to a destination to which no grain rate is published was governed by the rate to the nearest point beyond the destination, to which a grain rate is published when the short haul is included in the long haul, as per Ohio Senate Bill No. 197.

During the Federal Administration, we have had a couple of cases arise. In one instance: a car of wheat was shipped to a mill, no grain rate published to destination but a 10½ ct. rate published to point beyond, we were assessed a 16½ ct. rate. In another case, we were assessed the difference between 9 cts. to station beyond and 11½ cts. intermediate. In the former case we filed claim against the Baltimore & Ohio R. R., who declined the claim with the statement that Senate Bill 197 in Ohio is of no effect during the Federal operation of the roads, as the Public Utilities Commission of Ohio is without power.

After taking up with the Public Utilities Commission it seemed of the same opinion. Since the return of the railroads, we have asked their opinion as to whether this law is now in effect. The matter was submitted to their attorney and he advises that Section 208 of the law providing for such return, Congress has enacted that:

"All rates, fares and charges which on Feb. 29, 1920, are in effect on the lines of carriers subject to the Interstate Commerce Act, shall continue in force and effect until thereafter changed by state or federal authority, respectively, or pursuant to authority of law; but prior to Sept. 1, 1920, no such rate, fare or charge shall be reduced, unless such reduction or change is approved by the Commission." (Meaning the Interstate Commerce Commission.)

Also that by Order No. 18 dated March 1, 1920, the Interstate Commerce Commission, assuming that the above provision is in effect and is controlling, provided:

"That the said carriers be and they are hereby authorized to continue until Sept. 1, 1920, all rates, fares and charges established by the Railroad Administration during the period of Federal Control for the transportation of property for a shorter than for a longer distance over the same line or route in the same direction," etc.

Attorney E. E. Corn of Public Utilities Commission of Ohio then goes on to state: "This condition presents then a purely legal question, whether Congress has such power, and whether in so enacting, Congress has invaded any of the rights vouchsafed to the States by the Federal Constitution, and particularly the power of the States to fix and regulate rates for purely intrastate shipments."

"The question thus presented can only be settled in some court of competent jurisdiction, and will never be finally settled, until passed upon by the United States Supreme Court."

You can readily see that there is only one thing for us to do and that is "pass up" business to which we are unable to quote a price based on the grain rate, or else figure on the sixth class rate, thus helping the "high cost of living."

We have suggested to the Public Utilities Commission of Ohio that it could assume that Ohio Senate Bill 197 is in effect and in doing so be put on the defensive and resist any effort of the railroad companies to evade it.

We would be very glad to have the Journal's opinion in the matter.—E. A. Grubbs Grain Co., Greenville, O.

**Ans.:** No decision of the courts have yet been rendered bearing on the power of the federal



government and federal railroad administration to override the state statutes, during the time of war.

Considering that in time of war the federal government has power to seize and operate for exclusively military purposes any railroad, state or interstate, it is safe to assume that private traffic could be carried only on such terms and rates as the federal authority chose to prescribe, barring unfair discrimination.

If the higher 16½ cent rate objected to had been duly filed by the railroad company and approved by the Interstate Commerce Commission, the shipper must pay the scheduled rate.

Being an overcharge claim, the Interstate Commerce Commission has jurisdiction and will entertain a claim by shipper for reparation, supported by an allegation that the Ohio Statute controlled the intrastate rate. In this way a ruling could be had from the Commission on this point. There is sufficient doubt on the allowance of the higher rate to make it worth while to file claim, especially as the shipper will be allowed interest from the date he paid the overcharge.

## Why Charge Local Freight?

*Grain Dealers Journal:* We loaded G N 123540 with 16,290 lbs. hay, 16,000 lbs. oats and 1,000 lbs. flour for Plymouth, Wis., 36 miles.

The railroad company charged \$21.60 at 9c on 24,000 minimum hay; \$27.20 on 16,000 oats at 17c, and \$1.70 on 1,000 flour at 17c, total, \$50.50.

We filed claim with the C. & N. W. R. R. Co. for \$32.76 overcharge, which is the freight local on the oats and flour.

We cannot get it clear that when a car has been loaded with only 16,290 lbs. hay how the railroad company, charging us for the 24,000 minimum hay, can also charge local freight on the oats and the flour.—Huebner Bros. Co., Forest Junction, Wis.

**Ans.:** The privilege of mixing different commodities in a car has attached to it a penalty, which is, according to the official tariffs, that freight can be collected on the highest minimum at the highest rate on the several commodities.

Referring to the Railway Equipment Register the car used is shown to be an 80,000 capacity, so that the company was within its rights in charging 24,000 lbs. minimum on hay, and the local rates on oats and flour. Had the company elected to charge on the grain minimum at the hay rate the charge would have been more than \$50.50.

## Liability of Telegraf Co. for Error?

*Grain Dealers Journal:* On Oct. 7th one of our Western shippers wired us asking if we could handle another car of wheat, we replied, stating that we could. They in turn confirmed the sale of one car of wheat to us at \$2.63. The message however, arrived at Lisbon reading, \$2.53. The Western Union transmitted this telegram incorrectly, our customer contend that they sold us a car of wheat at \$2.63 and we contend that we bought a car at \$2.53. Of course, we would have lost no money had we even agreed to pay the \$2.63 but when we received the message quoting \$2.53, we promptly considered one car booked, and used the \$2.53 together with other wheat we had bought that day, in averaging up our average cost of wheat for the day and the average cost of wheat, of course was the basis we used in figuring the average cost of flour.

The Western Union contend that they are not liable because we have not proven to them that we sustained any loss. Furthermore they say they are protected by all of their conditions on the reversed side of their telegrams, which says that they are not liable beyond, etc.—Stewart & Hastings.

**Ans.:** Shipper can recover the profit he would have made had the message been correctly transmitted. The profit belongs to him and not to the telegraf company. It is simply a question of proving the profit, or loss of profit, exactly.

In case of ordinary negligence the telegraf company can take refuge behind the fine print limitation of liability on the back of the blank; but in case of gross negligence this is no defense.

A double error is necessary to transmit 63 as 53 in Morse, if the price was written out sixty-three, as the s and x would be changed to f and

f. If numerals were used the case against the telegraf company is about as bad, since 6 is indicated by six dots and 5 by three dashes. The most recent decision on gross and ordinary negligence is published on page 266 of the Journal for Feb. 10.

## Letters

[Here is the grain dealers' forum for the discussion of grain trade problems, practices and needed reforms. When you have anything to say of interest to members of the grain trade, send it to the Journal for publication. It may draw out the views of others.]

### Calculating Load from Test Weight.

*Grain Dealers Journal:* I was very much interested in an article in the Journal of Feb. 25 entitled "Calculating Load from Test Weight," and in my opinion the shipper was correct in his loading weight, in support of which opinion I offer the following:

On Dec. 15, 1919, I loaded C., M. & St. P. No. 507694 with white oats and shipped it to J. E. Bennett & Co., Chicago. The official test weight on these oats was 29½ lbs. The cubic capacity of this car was 3,267 feet and the out turn weight was 88,890 lbs., or 2,777.26 bus.

In the article referred to the car capacity was 2,811 cu. ft. My car was approximately 1/7 larger than his, while he overcame this by having oats that were 5½ lbs., or about 1/6 heavier than mine. If we decrease my net weight by 1/7 we have 77,780 lbs. as the amount his car would hold of the same weight oats, that is, 29½ lbs. By increasing the 77,780 lbs. by the difference in the test weight (5½ lbs.) we find the amount to be 92,200 lbs., or the number of pounds of 35-lb. oats that could be loaded into a car containing 2,811 cu. ft.—A. C. Parks, Good Hope, Ill.

### Claim Agents Deserve Rough Treatment.

*Grain Dealers Journal:* I note with interest the reference in the Mar. 10 number of the Journal to the unjust demands on grain shippers in claim preparation.

While the railroad company knows it does not pay a shipper to fight out every little issue yet I believe it is a good thing for the shipper to do so occasionally and let the public know about it.

Following are 2 letters I wrote with reference to one claim:

Dear Sir: You have presented claim for \$13.25 for loss account of damage on a shipment of corn delivered to you under our F/B 401 of Feb. 16, 1920.

You are including an item of nine sacks at 55c per sack, or \$4.95. It will be necessary that you turn the damaged sacks over to our agent for salvage or amend your claim allowing us proper credit therefor. Further the invoice in support of your claim is not the original nor is not certified to. I would thank you to furnish me original or certified copy thereof with your amended claim or have the damaged sacks turned over to our agent for salvage and we will endeavor to bring this matter to a prompt conclusion.—J. F. Horrigan, F. C. A.

J. F. Horrigan,  
N. P. Railway Co.,  
St. Paul, Minn.

Dear Sir: I have yours of 13th claim 0613594. In the future please refer to my claim number. Your agent at Moorhead has already three times refused to consider this claim, alleging that my preparation was not complete.

My preparation suits me and you will please return all papers at once unless you can see your way clear to send me the money, without further quibble.

The original invoice will remain in my possession and your agent can see it if he wishes. If it is necessary to do any more certifying I will do so in court.

The bags which your company delivered to me in damaged condition can be obtained whenever you are ready to pay for them and not before.

D. D. Simmons.

This little claim had excellent preparation and the agent's notation proves the damage. If my answer to the claim agent is not good logic in the matter of the bags and the invoice I would like to have my brother dealers correct me.

I am a member of the Western Railway Weighing Ass'n, whose representatives have access to all my invoice records and who always after checking me up refer to the ease with which it is accomplished.

Discrimination in large matters has largely been done away in conduct of our railways but in small matters involving personal equations it is much worse because the old incentive to treat shippers fairly in small matters and get their business, is now done away with. The attempt to make individuals who stand on their rights uncomfortable is pronounced.—D. D. Simmons, Moorhead, Minn.

### Iron Roofing Can be Made Rain-tight.

*Grain Dealers Journal:* Relative to the complaint of country grain dealers of the failure of corrugated roofing to protect their elevator from rain we would say that it is a recognized fact, that both corrugated roofing and siding are the best coverings for both the roofs and sides of elevators, from the standpoints of security of contents from the elements, security of both elevator and contents from fire from any cause, and particularly from fires and damage caused by lightning, it being recognized as a fact that sheet metal coverings for buildings of any kind are the best possible protection against lightning, even tho there be no connection between the roof and the ground, but in cases where the roofing is grounded by means of a lightning conductor, the protection is very much better and we always recommend this additional precaution.

Speaking specifically of the complaints of country elevator owners, relative to leakage of corrugated roofs, this is not due to the corrugated roofing, but entirely due to faulty application and the use of material of too light weight. This has been true since corrugated roofing was introduced and has, in our opinion, been responsible for all complaints of unsatisfactory service, seriously curtailing the use of what is well known to be the best roofing and siding, from all standpoints, where sheets of sufficient weight are used and roofing properly applied.

We avoid the dissatisfaction from the use of flimsy light weight roofing by refusing to furnish it.—Gedge Bros. Iron Roofing Co., Anderson, Ind.

AN IMPORT duty of 0.42 cents per lb. has been placed on all wheat brot into Costa Rica.

THE MEXICAN Northwestern Railroad has made an increase of 20% in all rates according to a report from Juarez Mar. 3.

THE HESSIAN fly will do considerable damage to the growing wheat this spring between Apr. 15 and May 15 if we do not have long continuous rains reports the Ohio Agricultural Station. All wheat sown in Ohio after Sept. 29 is in good condition. Wheat sown before then is in poor shape and many farmers are planning to drill oats in with wheat and harvest the two crops together.

DR. H. H. BROWN of the Bureau of Chemistry, Department of Agriculture, talked on grain dust explosion prevention before the North Carolina Millers Ass'n on March 25th, at the O'Henry Hotel, Greensboro, N. C. This is one of the series of Southern meetings included in the fire prevention campaign which the United States Grain Corporation has been conducting in the interests of the grain men of the country. A similar address will be delivered before the Eastern Tennessee Millers at a special meeting arranged by the Knoxville City Mills for Mar. 27.



# Hearing on Proposed Changes in Wheat Grades

Sec'y of Agri. E. T. Meredith has given notice of a public hearing at Chicago, April 2, on the Federal Wheat Grades.

The demand for change so near to the movement of a new crop surely did not come from handlers. Sec'y Meredith in his announcement says,

Washington, D. C., March 24, 1920.

It has been represented to me that there is dissatisfaction in the Central Northwest with the present Federal Grades for Hard Red Spring and Durum wheats. I received a request for a hearing upon the subject and on March 19, 1920, a delegation visited me at Washington and presented recommendations on behalf of the State Boards of Grain Appeals at Minneapolis and Duluth certain State officials of Minnesota and North Dakota and others that the Federal Grades for Hard Red Spring and Durum wheats be changed in the following respects to take effect at the beginning of the coming crop year.

1. That the sub-class "Red Spring" be eliminated.
2. That the word "Bright" be eliminated in the Spring and Durum grades except in the No. 1 Dark Northern Spring and the No. 1 Amber Durum.
3. That the percentage of moisture in the Spring and Durum grades be increased to 15 per cent, that this maximum percentage be the same in all grades from No. 1 to No. 5 inclusive, that Wheat having more than 15 per cent be graded the same as that having less except that in addition it be classed as excessive moisture and stating the percentage of moisture.
4. That the maximum percentage of Durum to be carried in the grade of No. 1 Spring wheat be made 5 per cent, and that the maximum percentage of Spring Wheat to be carried in the Durum grades be made 10 per cent, the said maximum percentage to be made uniform in all the Durum grades from No. 1 to No. 5 inclusive.
5. That one per cent of rye be carried in each of the grades of Spring and Durum wheat in addition to the present foreign material.
6. That the test weight of the hard red spring class be reduced one pound for each grade.

After the hearing the delegation which appeared before me selected a special committee which discussed the details of these recommendations with representatives of the Department of Agriculture. This special committee restated the recommendations in substance as follows:

1. Substitute "Humpback" as a subclass for "Red Spring" and make the definition of the subclass "Humpback" in the Federal Standards read as follows: This subclass shall include wheat of the class Hard Red Spring consisting of more than ten percentum of the variety of Humpback. This would necessitate changing the definition of the subclass "Northern Spring" so as to read as follows. "This subclass shall include wheat of the class Hard Red Spring consisting of less than 75 per centum of Dark Hard and vitreous kernels. This subclass shall not include more than 10 percentum of the variety Humpback."
2. In place of the requirement in all of the No. 1 grades in the Hard Red Spring and Durum classes that the wheat in Grade No. 1 shall be "bright" provide that the wheat in all of these No. 1 grades shall be of "good color."
3. Make the maximum percentage of moisture allowed in all grades from No. 1 to No. 5 inclusive in the Hard Red Spring and Durum classes 15 per centum and provide that all wheat in these classes having more than 15 percentum moisture shall grade sample grade, making a regulation that all licensed grain inspectors in grading the wheat in these two classes shall state the exact percentage of moisture on the grain inspection certificates when it exceeds 15 per centum. As

an alternative for the foregoing make the 15 percentum limit apply only to the first three grades, No. 1, No. 2 and No. 3, and let the limit remain at 16 percentum for No. 4 and No. 5 as it is now in the federal grades. Under this alternative also make the regulation requiring the inspectors to show the exact moisture content on their certificates when over 15 percentum.

4. Change the requirement with respect to wheat of other classes in grade No. 1 in all subclasses of Hard Red Spring wheat so that the present limit of 5 per centum on the total wheats of other classes will include durum wheat and so that the special limitation of 2 per centum shall apply only to Common White and White Club wheats leaving the lower grades as they are now. No change in the allowance for wheat of other classes in Grade No. 1 in the three subclasses of Durum Wheat.

5. Increase the allowance for total foreign material other than Dockage in each of the grades from No. 1 to No. 5 inclusive in the Hard Red Spring and Durum classes so as to permit the addition of 1 percentum of rye in each of these grades, but not to complicate the grading requirements.

6. Reduce the present limit for test weight per bushel for each grade in Hard Red Spring wheat by one pound. The question whether there shall be changes in the present federal standards for wheat is so important and effects so many interests in addition to those represented at the hearing on March 19 that I have decided to hold a public hearing at Chicago, Illinois, on Friday, April 2, 1920, at the Hotel La Salle beginning at 10 a. m. At that hearing it is of the greatest importance that farmers, country buyers, line and terminal elevator men, commission men, brokers, exporters, miller, grain inspectors and others interested in the production, storage, transportation, marketing and utilization of wheat be represented and that they present to me their views as to.

1. Whether either of the above proposals submitted by the Delegation should be adopted.
2. Whether corresponding changes should be made in other classes of wheat.
3. Whether in view of the requirement of the three months advance notice it is practicable to make changes applicable to the 1920 crop.
4. Whether changes should be made until after the present standards have been tested without fixed price conditions.

## J. A. Tiedeman for Mayor.

Sioux City is fortunate in having as a candidate for mayor at the present spring election a man of broad caliber who is certain to get the cordial support of decent citizens against the I. W. W.

Just at a time when Sioux City is forging to the front as a great terminal grain market it is fitting that its civic affairs should be administered by a grain dealer, J. A. Tiedeman, now pres. of the Sioux City Board of Trade, and at one time pres. of the Western Grain Dealers Ass'n.

In announcing his candidacy for the office of mayor, Mr. Tiedeman said, "I will stand for strict enforcement of existing laws. Economy and efficiency will form the basis on which I will conduct the affairs of Sioux City if I am elected to office. Backed by no clique, clan or combination I desire to be in a position to serve all of the people of the city in a fair and impartial manner."

## Coming Conventions.

- Apr. 1 and 2. Western Grain Dealers Ass'n at Des Moines, Ia.  
 May 11, 12.—Illinois Grain Dealers Ass'n at Decatur, Ill.  
 May 13, 14. The Missouri Grain Dealers Ass'n at Kansas City, Mo.  
 May 18, 19. Oklahoma Grain Dealers Ass'n at Oklahoma City, Okla.  
 May 21, 22. Texas Grain Dealers Ass'n at Galveston, Tex.  
 May 26, 27, 28. Kansas Grain Dealers Ass'n at Wichita, Kan.  
 June 7, 8 and 9.—The Southern Seedsmen's Ass'n at Jacksonville, Fla.  
 June 22, 23, 24. American Seed Trade Ass'n at Milwaukee.  
 June 21.—Wholesale Grass Seed Dealers Ass'n at Milwaukee, Wis.  
 July 13, 14, 15. National Hay Ass'n at Cincinnati, O.  
 Oct. 11, 12 and 13. Grain Dealers National Ass'n at Minneapolis, Minn.

## Leaking in Transit

Grain dealers can help brother sufferers in the collection of claims for loss by reporting to Grain Dealers Journal, for free publication, car initials, number, place, date and condition of car seen leaking grain in transit.

Recently we have received reports of the following leaking or bad order cars:

P. M. 51658 was leaking oats at the end of car and was set off at Sheldon, Ill., March 11, for repair.—Ben B. Bishop.

G. N. 20226 was side tracked March 22 at Bluffs, Ill., on the Wabash. It was leaking at the draw bar.—Bluffs Farmers Grain Co.

Santa Fe 881 passed thru Kismet, Neb., March 13, leaking white kafir at side. The crew was notified but they made no repairs.—F. A. Prater, Reno Grain Co.

P. & L. E. 23740 passed thru Lake View, Ia., March 13, leaking oats in a stream. Siding at the door post was broken loose. We did not have time to fix it as the train only stopped a few minutes.—W. Hynes, Farmers Union Elvtr. Co.

Erie 103.204 passed thru Emerson, Neb. Feb. 27 leaking badly from a bursted side.—John Moseman, Moseman-Heyne Co., Pender, Neb. (See other report on this car below.)

M. K. & T. 87715 was set off at Nevada, Ia., some time Feb. 28 and was picked up by way freight train going east No. 42, about 1 p. m. It was leaking white corn thru sheathing. I did not have time to repair same.—Frazier & Sons, Nevada, Ia.

Erie 103,204 loaded with white corn sprung a leak in the yards at Emerson, Neb., about Feb. 22, and leaked several bushels. It had been plugged by the car tappers and had been moved several times, every time losing more corn. It is to be transferred to another car but is still leaking every time a train goes by.—Crowell Lbr. & Grain Co. (other report on this car above).

M. P. 80072 side tracked at Rolfe, Ia., Feb. 23, was leaking on one side and at one end.—P. M. Lothian, helper Charlton & Larson Grain Co.

## Our Callers

- H. Work, Ellsworth, Kan.  
 N. W. Williams, Newark, Ill.  
 Wm. Day, mgr. the Day Co., Minneapolis, Minn.  
 H. F. Shepherdson, Quinn-Shepherdson Co., Minneapolis, Minn.  
 James Walters, pres. James Walters Germ Destroyer Co., Inc., Birmingham, Ala.



## How to Beat the Federal Grades.

A delegation of Northwestern officials interested in grain grading called on the new Sec'y of Agriculture, Meredith, at Washington Mar. 19, to express their dissatisfaction with the present federal system of grading.

O. P. B. Jacobson of the Minnesota State Railroad and Warehouse Commission headed the delegation. With him were L. J. Aandahl, North Dakota; H. N. Owen, Minneapolis; Magnus Johnson, representing Equity Farmers' exchange; State Senators Cashman and Wold of Minnesota; D. P. O'Neill of Minneapolis Board of Appeals; H. F. Tunell, chief grain inspector at Minneapolis; N. J. Holmberg, commissioner of agriculture, Minnesota; A. L. Flynn, Minnesota Warehouse Commission; Representative Sorlein, Minnesota; S. L. Allen, pres. Minnesota Federation of Farm Buros.

The hearing was well attended by Department of Agriculture officials.

Congressman Geo. M. Young of North Dakota stated that under the federal government rules for grading five cars of No. 3 northern wheat could be mixed to make five cars of No. 1 northern, the grain having been graded down to No. 3 by the government technicalities. He proved this by the table below:

	Test wt. per bu.	Moisture content.	Damaged kernels. Per Cent	Total foreign material other than dockage.	Wheats of other classes. No. 3 Northern	Grade.
Car A .....	60	13.	.5	.0	*6.	"
Car B .....	59	*14.6	1.0	.5	.0	"
Car C .....	58	13.5	1.5	*2.5	.0	"
Car D .....	58	14.	*4.5	1.	1.	"
Car E .....	56.5	13.8	2.0	.5	1.	"
Average .....	58.3	13.8	1.9	.9	1.6	No. 1 Northern
Limits for No. 1, N. 1.	58	14.2	2.	1.0	2.	"

\*The foregoing would grade No. 1, except for factors indicated by stars, which makes them No. 3. An expert mixer in an elevator might even take an additional car of No. 3 (account of 9.4 per cent heat damaged) and get 6 cars No. 1.

## Grain Reconsigned Without "Out" Inspection.

Inspection certificates issued in accordance with the United States Grain Standards Act on arrival of grain can not be used on "out" shipments if the grain has been transferred through an elevator to another car.

The amendment to the rules and regulations under the act, which permits certificates covering arrival inspection to be used for further shipment of such grain not later than the close of the second business day after the date of such inspection, specifically states that the grain shall not be "removed or transferred from its container or otherwise changed in identity."

This amendment was intended to save the time required for an out-inspection of cars of grain which are reconsigned immediately after arrival, when the conditions under which the grain was originally inspected have not been changed.

THE GERMAN PRICE FIXING plan is disapproved by the farmers and as they can not sell for more than the fixed price they have called a "delivery strike" and have refused to market their products.

## Borer Causes Quarantine.

The known prevalence of the corn borer in certain eastern states has led the department of agriculture to declare a quarantine effective March 29, against the states of Massachusetts, New Hampshire, New York, and Pennsylvania on those articles likely to spread the pest thru the agency of commerce.

The list of quarantined articles includes corn and broom corn including all parts of the stalk, celery, green beans in the pod, beets with tops, spinach, rhubarb, oat and rye straw as such or when used as packing, cut flowers or entire plants of chrysanthemum, aster, cosmos, zinnia, and hollyhock, and cut flowers or entire plants of gladiolus and dahlia except the bulbs without stems.

The restrictions do not apply to shelled corn and clean seed of broom corn, nor to other articles after they have been manufactured or processed in such way as to eliminate risk of carrying the corn borer.

The Federal Horticulture Board says that the eradication of the borer is impossible as it occurs in all common weeds and plants of a succulent nature. The quarantine is placed to prevent the further spread and to aid in control of the borer.

The department of agriculture will send representatives to inspect a district or premise that has none of these borers. If it is found that the district has no weeds that will spread the pest and that borers are not present a permit will be issued that will permit that person or locality to ship their products.

Carriers are to assist in preventing the spread of this pest by cleaning thoroughly all cars and boats at the point of unloading or destination within the infested area.

Carriers are responsible for the enforcement of this order and will carry none of the articles under quarantine without a permit from the Department of Agriculture.

Minnesota, fearing the borer, has declared that no corn on cob, corn cobs nor any portion of the corn plant except shelled corn and shelled corn products from Massachusetts, New Hampshire and Vermont will be admitted into the state.

SOUTHERN RUSSIA's large crops last year reveal the possibilities of that country, could the idea be driven from the illiterate farmer's mind that "modern methods of cultivation are new fangled crazes."

## Sudan Grass Ahead of Millet.

That Sudan grass is superior to millet for the needs of western Kansas has been demonstrated at the Fort Hays Experiment station, Hays, Kan. On June 5, millets were sown on land, summer fallowed the preceding year, and in excellent condition. They were hand-seeded in four two-inch rows, with hills eighteen inches apart. On June 24, each hill was thinned to one plant. Owing to scarcity of seed, only ten hills of each of the fifty-one kinds of millet were sown.

The result of the experiments with millet were not promising. At least three-fourths of the sowing were either of ordinary value or of decidedly inferior value. Late maturity, coarseness of stem, scantiness of leaf, were common faults.

Even the millets shown in the picture can not be said to be promising. The Kursk millet excelled in earliness, and the Goldmine millet was exceptional in leafiness. Sudan grass is so far superior to millet in western Kansas that the latter is not likely to be highly cultivated. Millett excels Sudan grass only in quickness of maturity and as a grass-hopper resistant catch crop. The Kursk variety is particularly useful in the latter capacity.

The greatest merits of Sudan grass are set forth in the Grain Dealers' Journal, Jan. 25, page 188.



Sheaf Samples of Sudan Grass, Goldmine Millet, Kursk Millet, Common Millet. July 16, 49 Days after Planting.



The achievement of today but points to the pathway of tomorrow. Through service comes growth, and through growth comes greater service.—Armour & Co.

## Crop Reports

Reports on the acreage, condition and yield of grain and field seeds, as well as on the movement to country markets, are always welcome. Let us hear from you.

### GEORGIA.

Dublin, Ga.—Corn seeding is now well under way in this section, the acreage showing a marked increase over 1919.—S.

### ILLINOIS.

Pekin, Ill., March 22.—Weather fine. No farming yet. Early sowing hurt by fly.—James Heck, Pekin Farmers Grain Co.

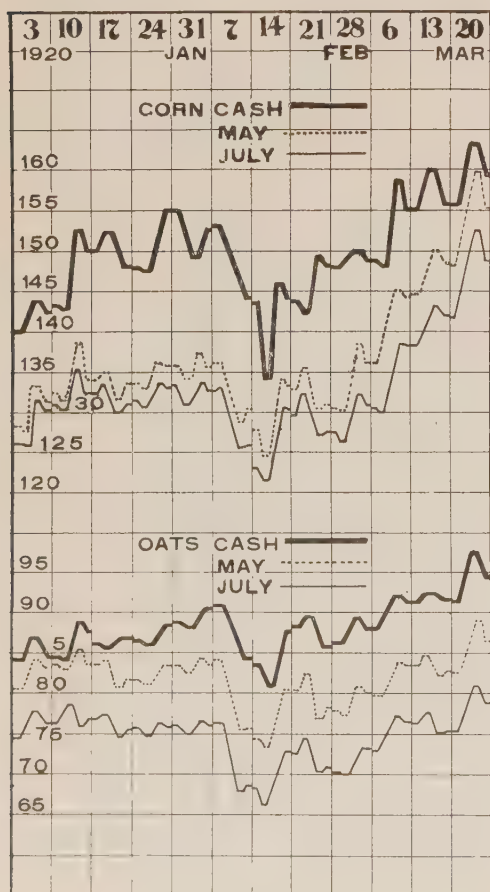
Springfield, Ill., March 24.—Winter wheat is somewhat improved. Its condition is poor in the southern part of the state and only fair elsewhere. Fly infestation is bad, especially in the southern and central counties. The warm days have caused grass to start, but on account of the wet fields no plowing or seeding has been done.—Clarence J. Root, meteorologist, U. S. Dept. of Agri.

### INDIANA.

Uniondale, Ind., March 20.—More oats sown this year than last as a part of the wheat land will be sown to oats.—Miller & Brickley Grain Co.

## Cash Corn and Oats Fluctuations from Dec. 29 to Mar. 20.

Opening, high, low and closing average prices of No. 2 mixed corn and No. 2 white oats at Chicago each week are given on the chart herewith. The daily average is used in charting; actual prices were made each day a few cents above or below the extreme charted. The May and July futures are shown by dotted and light lines for comparison.



Roachdale, Ind., March 22.—Wheat is looking better but a very short crop expected. We expect a large oats acreage. Acreage in corn will be small because of the farm help situation.—Albert C. Lockridge.

### KANSAS.

Lyndon, Kan., March 22.—The wheat in this section of Kansas has not suffered much as yet but we need rain very much.—W. A. Schroeder, Schroeder Bros. Grain Co.

Larned, Kan., March 10.—A bad dust storm accompanied by a 50 mile wind did some damage in this section to the growing wheat. We cannot tell how much.—T. H. Urton, agt. Rea Patterson Mlg. Co.

Larned, Kan., March 22.—I notice in the papers about the millions of damage to the growing wheat crop. There is some small damage but no more than usual. A good rain and this will all be forgotten.—T. H. Urton, Rea Patterson Grain Co.

Hutchinson, Kan., March 20.—The wheat crop has been damaged to some extent by the winds and we will not have more than an average crop, but there will be a large acreage of corn and sorghums if the spring planting conditions are favorable.—C. A. Lovell, sec'y, Jay Hausam & Co.

Ellsworth, Kan., Mar. 19.—We have had very fierce wind storms practically all this week. Probably considerable wheat on thin or light soil has been damaged and more will be if the winds continue for any length of time. Yesterday was one of the worst days I have seen in my 56 years residence in this state.—H. Work.

Sedgwick, Kan., Mar. 18.—A high wind is blowing today doing much damage to growing wheat. Has been blowing since Sunday 14th. Much of the wheat is holding on but the soil is blowing away from the roots which are drying up from lack of moisture. A good rain would save lots of this wheat in this territory.—H. E. Hickerson, Associated Mill & Elevtr. Co.

### NEBRASKA.

Touhy, Neb., March 19.—Farmers in this locality are complaining that the damage caused to wheat by high winds is beginning to show results.—R. J. Jelinek, mgr. Farmers Co.-op. Co.

Lincoln, Neb., Mar. 22.—Reports are reaching here of the damage of the wheat crop by the high winds which have prevailed the last several days. The plants are badly uprooted.—H.

### OHIO.

McGuffey, O., March 13.—The quality of the corn in this section has been good.—Wm. McGuffey.

Springfield, O., March 22.—Early sown wheat hurt considerably by the fly and very likely will be planted in oats. Wheat sown from Oct. 1 to 10 looking fair.—W. E. Tuttle & Co.

### OKLAHOMA.

Kingfisher, Okla., Mar. 12.—Crop conditions good for this time of the year but we need rain.—A. R. Smith, mgr. Farmers Elevtr. Co.

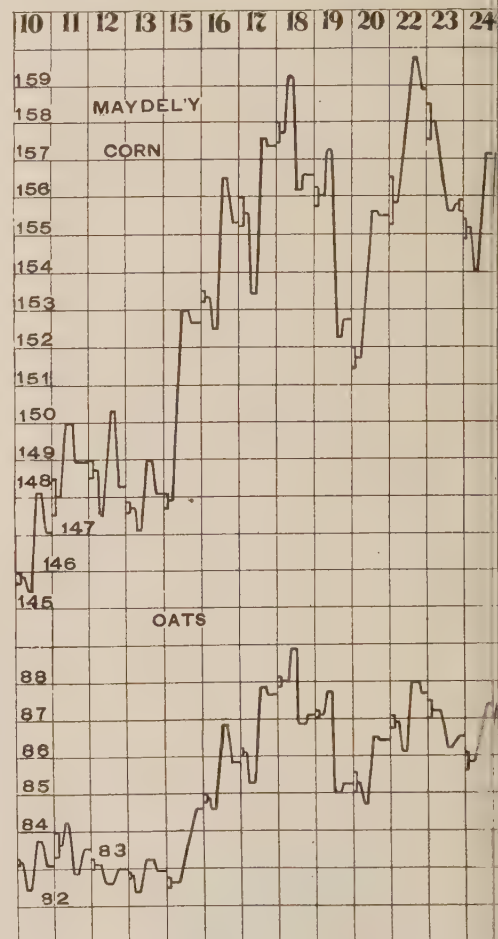
### WYOMING.

Graybull, Wyo.—We will have plenty of water for irrigation in this locality this year. Crops in this district this year amounted to about 150,000 bus.—John Larson.

CROPS in South Africa are very poor this year. According to Consul General Murphy the wheat and barley crop are 30% below normal and the oats crop is 28% below normal.

## Chicago Futures

Opening, high, low and close on corn and oats for the May delivery at Chicago for two weeks past are given on the chart herewith.



TURKEY'S yield of wheat per acre for the 1919 crop was one of the best on record. Still it is doubtful whether there will be enough to meet local requirements. Had the pre-war percentage of seed been sown there would have been a big surplus. The barley crop was large but of poor quality. Considerable barley flour is being mixed with wheat flour for use in baking. It is not expected that there will be a surplus of barley for export. Canary seed, flaxseed and oats were all small crops. There will be about 18,000 tons of maize for export after the local needs have been supplied. With war operations at an end and many more hands available together with favorable weather last fall, it is expected that the acreage under cultivation this year will be greatly increased.

## Daily Closing Prices.

The daily closing prices for oats and corn for May delivery at the following markets for the past two weeks have been as follows:

	MAY OATS.											
	Mar. 10.	Mar. 11.	Mar. 12.	Mar. 13.	Mar. 15.	Mar. 16.	Mar. 17.	Mar. 18.	Mar. 19.	Mar. 20.	Mar. 22.	Mar. 23.
Chicago	83	83 1/2	83	82 3/4	84 1/2	85 1/2	87 1/2	87	85 1/4	86 1/2	87 1/2	86 1/2
Minneapolis	81 1/2	81 1/4	81 1/2	80 3/4	82 1/2	83 1/2	84 1/2	83 3/4	82 1/2	83 1/2	83 3/4	83 3/4
St. Louis	85 1/2	84 1/2	85 1/4	85 1/2	87	88	90	89 1/4	88 3/4	88 1/2	90	88 3/4
Kansas City	83 3/4	83 3/4	82 3/4	82 3/4	84 1/2	85 1/2	87	86 1/2	85 1/2	86	87 1/2	86 3/4
Milwaukee	83 3/4	83 3/4	83	83	84 1/2	85 1/2	87 1/2	87 1/2	85 1/2	86 1/2	87 1/2	86 1/2
Winnipeg	96 1/2	96 1/2	97	96 3/4	97 1/2	98 3/4	99 1/2	100 1/2	98 3/4	99 1/2	100 1/2	99 1/2
	MAY CORN.											
	Mar. 10.	Mar. 11.	Mar. 12.	Mar. 13.	Mar. 15.	Mar. 16.	Mar. 17.	Mar. 18.	Mar. 19.	Mar. 20.	Mar. 22.	Mar. 23.
Chicago	147	148 1/2	148 1/4	148	152 3/4	155 1/4	157 1/4	156 1/2	152 1/2	155 1/4	158 1/2	155 1/2
St. Louis	148 1/2	150 1/4	150 1/4	149 1/2	153 1/2	156 1/2	158	157 1/2	154 1/2	156 1/2	159 1/2	156 1/2
Kansas City	144 1/2	146 1/2	145 1/2	145 1/2	149 1/2	151 1/2	152 1/2	152 1/2	148 1/2	150 1/2	154	152
Milwaukee	148 1/2	148 1/4	148 1/2	153	155 1/2	157 1/2	156 1/2	152 1/2	155 1/4	159	155 1/2	156 1/2



## Grain Movement

Reports on the movement of grain from farm to country elevator and movement from interior points are always welcome.

### ILLINOIS.

Chicago, Ill., Mar. 23.—For the week ending March 12 wheat receipts from farms were 4,129,000 bus., compared with 3,014,000 bus. the corresponding week in 1919. The preceding week receipts amounted to 4,031,000 bus., compared with the receipts of 3,178,000 bus. the corresponding week in 1919. The total stock of wheat in all mills and elevators for the week ending March 12 was 174,066,000 bus., compared with 203,038,000 bus. in 1919. The wheat held in elevators and mills the week of March 12 is 4,105,000 bus. less than the amount on hand a week ago. Last year for the corresponding week wheat on hand decreased 9,024,000 bus.—H. B. Jackson, 2nd V. pres., U. S. Grain Corporation.

### KANSAS.

Larned, Kan., March 10.—There is considerable wheat in the farmers hands in this territory.—T. H. Urton, agt. Rea Patterson Mlg. Co.

Lyndon, Kan., March 22.—Very little wheat left in this locality.—W. A. Schroeder, Schroeder Bros. Grain Co.

### MICHIGAN.

Pinconning, Mich.—Grain and beans at this point are nearly all marketed.—Walter Hess, Kimball-Martindale Co.

### MINNESOTA.

Rochester, Minn., March 15.—Grain movement is very light.—Leonard-Hutton Grain Co.

### MISSOURI.

Cowgill, Mo., March 21.—No grain shipped in or out of here this year.—O. McNew.

St. Louis, Mo., Mar. 21.—Elevators here are doing very little, and they are all crying for empties. Records at one elevator here show that for the month of February out of all cars received only 3 out of every 10 were fit for re-loading.—P.

### NEBRASKA.

Touhy, Neb., March 19.—Bad roads in this locality are cutting grain movement.—R. J. Jelinek, mgr., Farmers Co-op. Co.

## Exports of Grain Weekly.

[From Atlantic and Gulf Ports, in Bus., 000 Omitted.]

	Wheat.	Corn.	Oats.
	'19-20.	'18-19.	'18-19.
July 5.....	1,504	539	20
July 12.....	3,351	382	23
July 19.....	3,393	144	93
July 26.....	1,112	413	45
Aug. 2.....	3,067	543	9
Aug. 9.....	3,651	1,419	74
Aug. 16.....	2,620	1,809	44
Aug. 23.....	4,445	3,504	67
Aug. 30.....	6,072	2,938	264
Sept. 6.....	3,848	3,452	118
Sept. 13.....	7,475	3,739	42
Sept. 20.....	6,343	4,409	148
Sept. 27.....	4,690	5,088	29
Oct. 4.....	5,917	5,532	33
Oct. 11.....	2,485	4,078	28
Oct. 18.....	2,830	4,200	18
Oct. 25.....	2,920	4,143	9
Nov. 1.....	2,458	2,513	10
Nov. 8.....	3,856	4,117	23
Nov. 15.....	5,994	3,471	69
Nov. 22.....	4,629	4,005	21
Nov. 29.....	3,781	7,560	39
Dec. 6.....	6,226	10,900	16
Dec. 13.....	3,674	7,740	16
Dec. 20.....	4,639	3,674	10
Dec. 27.....	2,522	4,765	25
Jan. 3.....	1,161	3,600	46
Jan. 10.....	4,854	4,404	62
Jan. 17.....	3,950	5,013	106
Jan. 24.....	1,980	3,661	188
Jan. 31.....	1,992	1,884	84
Feb. 7.....	1,374	2,947	234
Feb. 14.....	2,018	5,684	207
Feb. 21.....	1,932	3,209	138
Feb. 28.....	867	3,914	145
Mar. 6.....	2,324	1,543	368
Mar. 13.....	2,107	2,152	50
Mar. 20.....	1,644	3,846	43
Total since			
July 1.....	133,945	137,930	2,964
			7,349
			31,765
			87,960

North Bend, Neb., March 15.—Cars are hard to get hold of for loading grain. The roads at present are too bad for the farmers to bring in their crops.—V. McIntosh, mgr. Farmers Union Co-op. Co.

### OHIO.

Coshocton, O.—Wheat receipts at many points are very light. Ohio wheat is pretty well cleaned out.—Hanley Mlg. Co.

McGuffey, O., March 13.—The movement so far this year has been slow. The outlook for grain shipments especially corn are good.—Wm. McGuffey.

### OKLAHOMA

Kremlin, Okla., March 16.—Old wheat in this locality about all in.—G. C. Rhodes, mgr., Farmers Elvtr. Co.

## Our Colossal Task.

American business enters a new era. Government and war have held the field; progress and peace now take the arena.

In the rebuilding of the world this nation is assigned the task of leadership. Our work will be colossal, and will call for every ounce of the powerful constructive forces that have been waiting the signal to start.

The backbone of all enterprise must be increased production. American business will meet its responsibilities with overwhelming success if no obstacles are put in the way of the expansion necessary to achievement.

With production increased and the full confidence of the country behind our industrial leadership. America will be a busy and prosperous nation.—M. Field & Co.

## Blumson Caught Nearer Home

After having slipped thru the fingers of the detectives in South Africa Herman J. Blumson, who defrauded Chicago and Milwaukee banks of \$400,000 on forged Bs/L, was caught Mar. 25 at Washington, D. C.

He had gone to the post office to get a letter from his nephew, and was seized by the police, who had been lying in wait for two days. He was in ill health and had to have hospital care.

He perpetrated his swindle by forming the Star Cereal & Milling Co. and actually operated the mill at Chicago.

## Wireless Telegraf as a Fire Hazard.

Dealers would do well to watch out for the amateur wireless operator that has his apparatus near his elevator with its easily exploded dust.

A French scientist with a small wireless apparatus communicating with large instruments great distances away was able to ignite cotton and paper a yard away from the apparatus.

In the "Blimp" airship disaster in Chicago last fall certain scientists condemned a wireless apparatus over which the airship was flying as the cause of the gas bag explosion.

In France many suits have been started alleging wireless companies have started fires thru the waves they transmitted.

THE MILLERS NATIONAL FEDERATION has chosen A. L. Goetzman, LaCrosse, Wis., as its new president. Previous to his election he had been vice president and was sec'y from 1905 to 1912.

GRAIN ACREAGE is on the decline in New Zealand. The wheat acreage for this crop is 142,960 acres compared with 210,775 acres in wheat a year ago. Oats acreage this year is 410,250; compared with 487,250 a year ago. The farmers have taken to cattle and sheep raising as more profitable.

A CONTRACT between Canada and Greece for 12,000,000 bushels of wheat, has been announced by the Canadian government. The price charged by the Canadian wheat board was \$2.16 per bushel. That price to prevail until the commission's control is removed. Thereafter Greece will pay the prevailing market price. Delivery was to be made between July 1, 1919, and July 1, 1920. Prices are figured on basis of Ft. William prices plus charges to the seaboard and a 1c commission.

## Nine Months Limit on Export Claims Not Retroactive.

A complaint was made by flour mill agents at New York and Kansas City that the four months limit in which to file claim for loss or damage to export shipments in effect Oct. 22, 1916, was unreasonable and that delivery by the railway should not mean port of export.

Complainant declared that when shipments were moved on tramp steamers that boats have waited in harbor as long as two weeks before they could even get a place to dock and that for this reason delivery by the railway should not mean delivery at the point of export. They claimed that such delivery was unreasonable.

Steamers are often on the water as long as 60 days on a trip to Africa and for that reason complainant said that the ordinary course of time between the arrival of the shipment at the port of export and the destination was so great that the four months limit in which to file claim for loss or damage after delivery at port of export was impractical. The complainant asked that the nine months limit now in force be made retroactive.

In reply to the complaint the Interstate Commerce Commission said that the limit in which to file claim had been increased by agreement between the carriers and the shippers and on June 1, 1916, from four to nine months. They also said:

"That there were difficulties in presenting claims on this flour for export within the four months is apparent. Delays in delivery to steamer, in clearing port, in passage, in unloading, in ultimate delivery, all tend to prevent as speedy presentation of claims as is possible in the case of domestic traffic. We are not convinced on this record, however, that the subsequently established nine months provision should be retroactive.

"An examination of the provision of the B/L as to the place of delivery leads to the conclusion that the defendants' construction that it is the port of export was not unreasonable.

"We are of opinion and find that as applied to the shipments specified by the complainants the four months limitation as applied to the claims here involved was not unreasonable or unduly prejudicial, and that the defendants' practice in construing delivery to mean delivery at the port of export was not unreasonable.

"The complaint will be dismissed."—F. H. Price & Co., and R. W. Lightburne vs. A. T. & S. F. R. R.

SASKATCHEWAN farmers no longer will have to depend entirely on the grain crop but will devote more time to the raising of cattle and live stock says the Provincial Livestock Commissioner.

THE POOR RICE crops in certain big rice producing countries, together with the restrictions made on its sale in those countries with a surplus has resulted in a rice shortage. Siam has declared an embargo on all rice and rice products. In Burma, India, on account of crop failure there is government control of all rice with little hope of much for export. Certain Chinese embargos have been removed only to be followed by an order specifying that all rice purchased by foreigners shall be paid for in at least one-fifth American gold or silver. It is not expected that under these conditions there will be much far eastern rice sold to certain European countries. In England rice and rice products have been placed under the Wheat Supplies Commission. Under the new control all rice brot into the country is to be pooled by the Wheat Commission and sold at the following fixed prices: rice 14c per lb.; glazed rice 16c and rice products from 15 to 17c per lb.



## Bonded and Unbonded Warehouses Alike.

The Uniform Warehouse Receipts Act has been adopted by so many states that decisions under this statute have more than ordinary force as precedents in other states.

One of the first decisions to be given under the Uniform Act is that by the Supreme Court Oklahoma, Dec. 23, 1919, in the suit by J. F. Gettig, a farmer, against the Citizens State Bank of Vici, Okla., operating a grain elevator under the name Vici Warehouse Co.

When Gettig delivered wheat for storage he was given storage tickets, but was informed that the warehouse was not bonded and was not permitted to receive wheat for storage, tho he might leave it until he desired to sell the wheat. For this reason the bank contended the deposit of the wheat amounted to a sale and not a bailment. The elevator was owned by J. S. Townsend, manager, with whom the bank had some difficulty.

When Gettig undertook to sell the wheat he was informed by Townsend the wheat had been sold and shipped out long ago, and that the bank would settle for it. The bank claimed it had nothing to do with the elevator, tho its vice-pres. Mr. Jones, worked for the warehouse Company. The jury found that the bank was running the elevator and gave Gettig judgment for the value of the wheat.

**The Supreme Court said:** Could it be said that the intention of the Legislature was that if you deposited grain in an elevator that was a bonded warehouse, the warehouseman would be considered a bailee, and if the grain was mixed or mingled with other grains that you would be entitled to your proportion thereof, as the amount deposited by you bears to the whole. While if you deposit your grain in an elevator that was not bonded, and the warehouseman had no license to operate a bonded warehouse, and the grain was deposited under the same conditions and the same terms, the warehouseman would not be a bailee; but the transaction would be considered a sale. With this we cannot agree.

The uniform warehouse receipts act defines and fixes the rights and liabilities of the parties in the storing of grain, and is a full and complete treatise on the subject, and makes no distinction between public and private warehouses or between bonded and unbonded warehouses, but regulates the storage of goods. True, the act defines a "warehouseman" as follows:

"A person lawfully engaged in the business of storing goods for hire for profit." It might be contended that the evidence disclosed in this case that no charge was to be made for the storing of the goods. That is true, but the profit anticipated was the expectancy of buying the goods in the future and the profit expected to be derived therefrom.

Sections 1089 and 1124 do not refer to the depositing of grain in elevators or warehouses but are sufficiently broad enough to include said subject. The uniform warehouse receipts act deals specially with the depositing of fungible goods in warehouses where it is anticipated that the same will be mixed with like goods and character and determines the rights of the parties.—187 Pac. Rep. 217.

## Railway Inefficiency Under Government Control.

Exponents of government ownership and operation will do well to study carefully the facts about the Pennsylvania Railway before and after it went under federal control.

The instances here cited are typical examples of Government operation but they do not beat the instance quoted in the Jan. 10 issue of the Grain Dealers Journal (page 60).

W. W. Atterbury, vice-president of the Pennsylvania Lines recently stated that when the Pennsylvania went under federal control there was in the company's employ 147,718 persons. When the roads were returned on March 1 there were 168,892 persons or an increase of 14%.

Within the same time operating units of all kinds decreased 11%.

This means that 14% more men did 11% less work under federal control or that the work that was performed by 100 men under private ownership in 1917 required 127 men under federal control in 1919.

Little wonder there were no cars, no service, corn heating in the elevators. Every one is dissatisfied except organized labor who want government control continued.

CORN PRODUCTS and corn flour are being bot at New York for Egypt to the extent of freight room available up to Sept. 30. Orders are coming thru large mercantile houses and financed by the English government. New York cash houses are bidding  $17\frac{1}{2}@22c$  over Chicago May c. i. f. New York for No. 3 yellow corn, 7 to 10-day shipment.—H. H. Lobdell of Lamson Bros. & Co.

## Treating Oats for Smut.

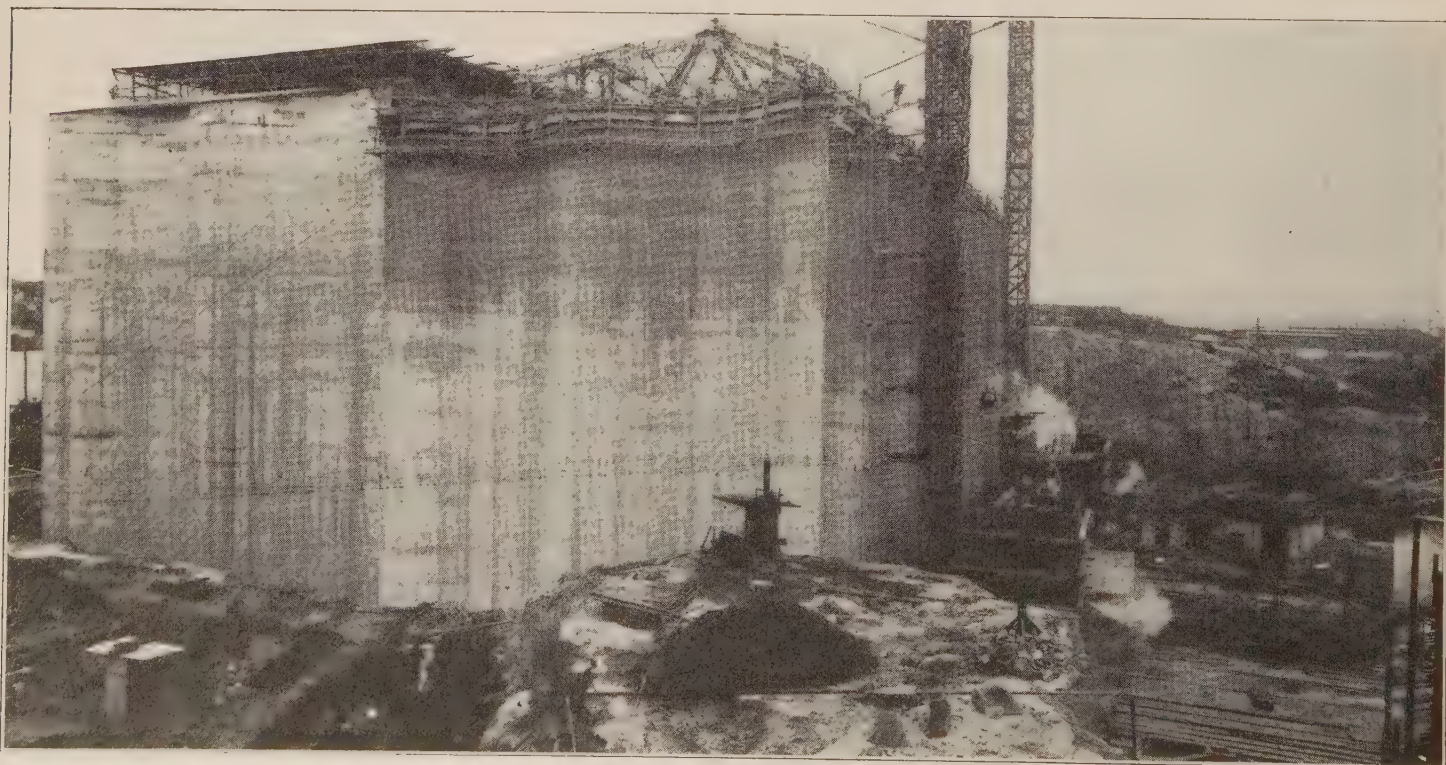
The treatment of oats for smut with formaldehyde has long been the subject of discussion. It has been a common practice to use as much as a gallon of solution to one bushel of oats. In tests that have extended over five years the University of Illinois has found that the very strong solution treatment is of no more value in preventing smut than is a weaker solution. It was also found by experiment that under controlled laboratory conditions, where the full effect of the formaldehyde was obtained, solutions of 1 pt. formaldehyde to amounts of water ranging from 3 to 40 gallons, applied at the rate of 1 pint per bushel, were effective in killing all smut spores. These laboratory experiments also indicated that the fungicidal action of formaldehyde as well as its effect upon the viability of the oats depends upon the actual amount of the formaldehyde gas applied per bushel of oats rather than upon the amount of solution used. One pint of a 1-10 solution of formalin contains approximately .005 of a pound of formaldehyde gas.

A solution of 1 pint of formalin, containing 40 percent formaldehyde, mixed with 10 gallons of water contained the most satisfactory amount of formaldehyde when used at the rate of 1 pint of solution per bushel of oats. This strength of solution permitted the development of a trace of smut two years out of the five, but this amount was insignificant and the average yield of oats was the greatest.

The Illinois Experiment Station recommended the following treatment for the prevention of smut:

Mix one pint of formalin (40 percent formaldehyde) with 10 gallons of water. Use one pint of this solution per bushel of oats treated. Sprinkle this evenly over the oats and thoroly but quickly mix until every grain is slightly moistened. Cover at once with a blanket, canvas, or sacks. Allow the oats to remain covered for two hours, after which time uncover. If the oats are not sown at once, thoroly air to allow the formaldehyde gas to escape.

THE CHAMBER of Commerce of the United States will hold its eighth annual meeting Apr. 27 to 29 at Atlantic City, N. J.



Just Completing the Second Section of the Storage Bins, Sydney, N. S. W., Terminal Elevator.  
[See pages 561 to 570.]



## Program Western Grain Dealers.

The 20th annual convention of the Western Grain Dealers Ass'n will be held at the Hotel Fort Des Moines, Des Moines, Ia., Thursday and Friday, April 1 and 2.

On Thursday at 11 o'clock an informal reception will be held on the seventh floor of the Hubbell Building, where the offices of the Western Grain Dealers Ass'n, the Des Moines Board of Trade, and the trading hall are located.

On Thursday at 2 o'clock the convention will be called to order for the general program, which will be continued until Friday afternoon.

The following addresses will be included in the general program:

P. E. Goodrich, Pres. of the Grain Dealers National Ass'n, will speak of the work of that organization.

E. L. Morris, divisional supervisor of federal grain inspection, will speak of the work of federal supervision of grain inspection, and accompanied by his assistants will conduct a continuous educational work during the convention in connection with an exhibition of type samples of different kinds and grades of grain and the different kinds of apparatus used in grain inspection work.

Hon. J. H. Henderson, Commerce Counsel for the State of Iowa, will explain the ruling by the Iowa Board of Railroad Commissioners in the Staceyville Grain & Coal Co. case, and in a general way regarding elevator site lease conditions and rental charges.

Owen L. Coon, loss and damage claims attorney of Chicago, will speak on claims for loss and damage of grain and discuss in detail the Interstate Commerce Commission Docket No. 9009.

Edward A. Pratt, of Chicago, will discuss grain trade accounting systems.

Professor Charles N. Kinney, Professor of Chemistry of Drake University, will deliver an address on the economic value of manufactured stock foods.

The entertainment during the convention will be provided by the Des Moines Board of Trade including a smoker and entertainment to be given in the banquet hall of the Hotel Fort Des Moines, Thursday evening at 8:15

and on Friday, attorney for the C. & N. W. Ry., will deliver the only address

of the evening on the subject, "The Railroad Situation," and it is the intention of the Des Moines Board of Trade entertainment com'te, of which Sec'y A. J. Cheeseman is chairman, that the grain dealers will not be permitted to entertain a single serious thought during the evening after the address has been delivered by Mr. Davis.

On Friday noon the Des Moines Board of Trade will provide a buffet lunch in the dining room adjacent to the convention hall in order that the grain dealers may spend a social hour.

The grain trade in general including all classes of country grain shippers and terminal market dealers regardless of whether they have membership in this Ass'n or not are cordially invited to attend and participate in the deliberations of the convention.

## Continue Rates Initiated Under Federal Operation Until Sept. 1.

The Interstate Commerce Commission, by an order issued Mar. 1 authorizes the railroad companies to continue charging rates and fares that are higher for shorter than for longer distances over the same route in the same direction. The order reads:

It is ordered, That the said carriers be, and they are hereby, authorized to continue until Sept. 1, 1920, all rates, fares and charges established by the Railroad Administration during the period of Federal control which, without same having been authorized by the Commission, yield greater compensation in the aggregate for the transportation of passengers or like kind of property for a shorter than for a longer distance over the same line or route in the same direction, the shorter being included within the longer distance, or greater compensation as a thru route than the aggregate of the intermediate rates, fares, or charges subject to the provisions of the act, provided that in instances in which applications for authority to continue any of the said rates, fares and charges are filed with this Commission on or before June 30, 1920, such rates, fares and charges covered thereby may be continued until the said applications shall have been passed upon by this Commission.

The Commission does not hereby approve any rates, fares or charges that may be continued under this permission, all such rates, fares or charges being subject to complaint, investigation and correction if in conflict with any provision of the act.

THE ANNUAL Grain Conference held at Pretoria, South Africa, indicates that the building of grain elevators in this country will continue.

## Contract Canceled by Failure to Accept Delivery.

The Kansas City Court of Appeals on Jan. 5, 1920, decided against O. A. Talbott & Co. in their suit against a farmer, M. A. Byler, to recover damages for failure to deliver corn on contract to their elevator at Wheeling, Mo.

The contract read:

"Wheeling, Mo., Oct. 26, 1917.  
"I have this day sold to O. A. Talbott & Co. 500 bushels of No. 3 white corn at \$1.02 per bushel, to be delivered at Wheeling, Mo., on or before the 10th day of December, 1917. If the grain should not grade according to this contract, then I agree to allow a reduction sufficient to make it the equivalent. Received \$20.00 as part payment on the above-mentioned grain.  
M. A. Byler."

Defendant alleged that he tried to deliver the corn, but was informed by plaintiff's agent over the 'phone that they could not take the corn and they had no room for it and could not get cars to move it. One load was then on the way and was taken in and paid for, the plaintiff's agent, it is alleged, promising to inform defendant when he could haul the remainder, but he never did.

The court said: As to whether time was of the essence of the contract, it will be observed that the contract specified the corn was to be delivered "on or before the 10th day of December, 1917." Of course, if it was to be delivered by that time, the corresponding obligation was on plaintiff to take it within that period. The phrase "on or before" a certain date in a contract, with reference to the time it is to be performed, limits the time of performance to the date last mentioned if the subject-matter of the contract and the circumstances are such that time may be deemed as of the essence. 6 Words and Phrases 4967; Wilson v. Bicknell, 170 Mass. 259, 49 N. E. 113; Ingram v. Wackernagel, 83 Iowa, 82, 48 N. W. 998. The contract was for the sale and delivery of a commodity, the price of which was subject to change and fluctuation on the market, and the contract fixed the time within which delivery and consequent acceptance were to be had. In such cases time is of the essence of the contract, and if the buyer refuses to accept delivery at or within the time designated, the seller is not obliged to deliver after that date. 35 Cyc. 175, 177. See also Interior Linseed Co. v. Becker Moore Paint Co., 273 Mo. 433, 448, 202 S. W. 566.

The judgment is affirmed.—217 S. W. Rep. 852.

THE TILLAGE act in force in the British Isles requires that every British land holder cultivate at least 15% of his acreage. This legislation is directed against those persons with large private estates.



Erecting Gallery Thru Which Grain Will be Conveyed to Wharf Galleries, Sydney, N. S. W., Terminal Elevator.  
[See pages 560 to 570.]



# New South Wales Building Bulk Handling Elevators

Australia, the great island of the south, has never been looked upon as an influential factor in the grain markets of the world, but the rapid increase now being made in the world's vessel tonnage is sure to bring all grain importing countries of the world nearer to the wheat fields of this great country.

Its area is 2,974,581 square miles or nearly the same as the United States, while its population is about five million.

New South Wales, the most densely populated province, has an area of 310,372 square miles and a population of nearly two million. Sydney, its largest city is the leading port of the island, and the terminal of the government's railway system. The latest figures available show that 44.61% or 849,159,173 acres of Australia are still unoccupied, while only 8.63% or 17,093,219 acres of New South Wales are unoccupied.

Every encouragement and assistance is being given to agriculture in hope of securing the intensive cultivation of a larger acreage. Providing a large number of concrete country elevators for receiving grain from farmers, a large concrete terminal elevator at Sydney and special box-cars to transport bulk grain to the Sydney elevator is only one more encouragement for the grain growers.

In 1910-1911 Australia produced 95,111,983 bus. of wheat from 7,372,456 acres; 13,041,081 bus. maize from 414,914 acres; 15,428,456 bus. oats from 676,688 acres; 2,226,368 bus. barley from 108,424 acres, and 3,175,887 tons hay from 2,258,405 acres. In 1915-1916 Australia had increased its wheat acreage to 12,484,512; maize was reduced to 323,637; oats increased to 721,644; barley acreage was increased to 169,514, and hay increased to 3,597,771 acres. The acreage sown to other grasses in 1915-1916 was 3,445,377. In 1917-1918 Australia produced 115,000,000 bus. wheat.

New South Wales, which thru the aid of irrigation is now producing large quantities of raisins, apples, pears and olives, has a climate much like Southern California. It never freezes and the temperature variation between mid-summer and mid-winter, seldom exceeds 18 degrees. Fodder grows the year around so winter feeding of live stock is unnecessary.

New South Wales in 1915-16 produced 66,726,459 bus. of wheat from 4,186,493 acres; 3,773,405 bus. of maize from 154,119 acres; 1,344,138 bus. of oats from 58,449 acres; 114,846 bus. of barley from 6,369 acres; 1,570,941 tons of hay from 1,107,228 acres and had 1,247,029 acres sown to other grasses and 2,864 acres sown to rye.

These crop reports show that the Australian farmers are disposed to favor wheat growing probably because it will net them a higher average price in the markets of the world.

The Government operates 20,738 miles of railroad in Australia, one-fifth of which is in New South Wales. The variation in the gauge of the different railroads contributes largely to the expense of shipping from one province to another because it necessitates unloading and reloading when shipping across boundary lines. New South Wales operates 4,148 miles of 4-ft. 8½-in. track and 40 miles of 3-ft. 6-in. track, while Victoria has 4,955 miles of 5-ft. 3-in. track.

The charge for transporting agricultural products over the railways of New South Wales ranges from 5s. 6d. (about \$1.34) for moving one ton 50 miles to 13s. 2d. (about \$3.16) for moving one ton 500 miles.

For several years before the World's great war agitation was rife for the building of bulk grain handling facilities in Australia

and different commissions were sent to the United States, Canada and Great Britain to investigate the methods in vogue. Nearly everyone interested fully recognized the advantages of the mechanical handling of bulk grain, but all of the governments hesitated at investing the large sum needed to build a system of bulk grain handling elevators.

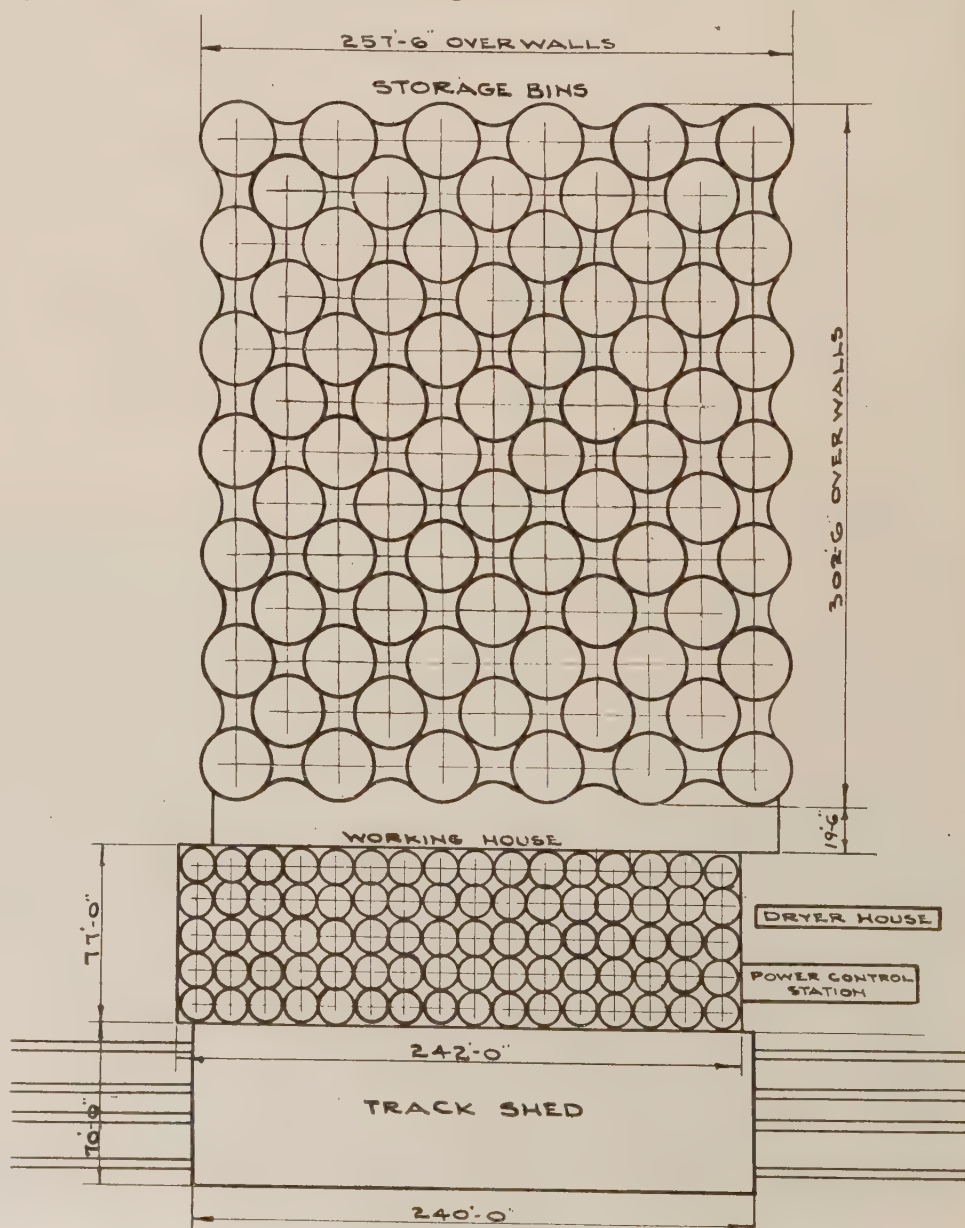
The shortage in shipping and the long voyage to European markets prevented Australia marketing much of its surplus grain during the war, so large quantities of bagged wheat were piled up at different points of advantage and covered with tarpaulins or galvanized iron. When the armistice was signed parts of four crops were still piled out in the open inviting attacks from mice and weevil. The destructive work of these pests during the war necessitated the re-bagging and the treating of much of the grain. Some of it was destroyed and much of it was made unfit for human food.

These large losses of grain, altho unusual, emphasized the need of modern storage

facilities, and hastened the adoption of the North American system of grain elevators.

New South Wales was the first state to contract for plans and specifications of a system of country receiving and terminal storage elevators. After intensive study of the needs of New South Wales and of the conditions prevalent in the grain surplus sections, the engineers of Jno. S. Metcalf & Co. recommended the building of a large number of reinforced concrete grain store-houses at shipping points of advantage in the grain producing sections and a large reinforced concrete storage and cleaning elevator in Sydney. The plans and specifications of Metcalf & Co. were adopted and 73 reinforced concrete country elevators are being erected. Some have been completed and are ready for operation.

THE TERMINAL ELEVATOR, which is nearing completion in Sydney, will afford storage room for 5,597,000 bus. and its working house will have room for 735,000 bus. This reinforced concrete plant which will be fireproof thruout is being erected on Glebe Island, Darling Harbor, Sydney. This island is of solid rock and has deep water frontage so is an ideal location. It was necessary to remove 250,000 cu. yds. of stone to provide a solid rock foundation for the 72 storage bins each of which is 31 x 108 feet and the 71 interstice bins. In the construction of the



Plan of Reinforced Concrete Working House and Storage Bins, Sydney, N. S. W., Terminal Elevator.  
[See pages 560 to 570.]



storage bins which are 257-ft. 6-in. x 302-ft. 6-in., 40,000 cu. yds. of concrete, reinforced by 1,600 tons of steel bars were used and 12,000 feet of belting will be needed to take grain to and from the bins.

The bins of the storage part were erected in three sections, yet, when completed they will form one monolithic cluster of 143 cylindrical and interstice bins. Heavy concrete piers were constructed on the solid rock to support the first section of bins and when the concrete had been poured for these bins the two mixers and the towers were moved back and piers constructed for the second section of bins. The same operation was repeated for the third section of storage bins.

The reinforcing of the outside walls of the middle section is bound to the reinforcing of the walls of the adjoining sections in a special manner so as to strengthen the joints and the walls are made thicker to form pilasters.

A record in pouring concrete was made in constructing the third section of these bins, when during one day of 24 hrs. the bin walls were raised 12 ft. 11 inches.

With the completion of the third section the mixers and 210 ft. towers moved further back to prepare and distribute the concrete for the working house.

THE WORKING HOUSE proper will be 242 x 77 x 200 feet high. It will be formed of 80 cylindrical bins, 60 interstice bins and 38 pocket bins. It will require 12,000 cubic yards of concrete, 350 tons of reinforcing steel and 2,500 tons of structural steel. The steel ladders in these bins will require 100 tons of steel.

Under the four tracks running thru the track shed will be 20 receiving sinks into which 20 pairs of traveling power shovels

will pull the grain from the cars. Five receiving legs will elevate the grain to the cupola, five shipping legs will loft the grain from the belts running under the storage bins and eleven auxiliary legs will supply the twelve cleaners, scourer and 750 bus. drier just outside the house.

The working house will have a complete dust collecting equipment, electric lights, signal lights, telephones and a passenger elevator. Power will be supplied to about 75 individual motors thru the adjacent transformer house.

Grain will be carried to the bins of the storage house on eleven conveyor belts and brot back on 6 large belts. In cupola of working house are two transfer belts, five 1,600 bus. hopper scales and five automatic shipping scales of 15,000 bus. per hour capacity.

Vessel loading will be done at five berths which will be served by belt galleries operating in connection with six moveable gantrys.

As soon as the splendid system of elevators being constructed by New South Wales is in operation, the Government of every Australian province producing wheat for export will be called upon to construct bulk handling facilities in order to cut out the expensive bags, wasteful and laborious handling and to increase the net price to the growers.

CANADIAN MILLERS are handicapped by the action of the Dominion government in shipping wheat to foreign countries, keeping the supplies out of the eastern mills. It is said the Wheat Board has placed no export flour orders since November. Eastern Ontario dairymen are suffering from the consequent shortage of mill feed.

### Traffic League Meeting.

At the annual meeting of the National Industrial Traffic League, held at St. Louis, Mar. 18 and 19, Mr. Barlow stated that in the west to return 6% on the value of the carriers there would have to be an advance of 20.36 per cent in rates; in trunk line territory 24 or 25 per cent.

J. H. Beek, chairman of the freight claim com'ite, reported that John H. Howard, manager of the claims and property protection section of the U. S. R. R. Administration, had informed him that up to Mar. 5 no arrangements had been made for settlement of claims arising under federal control.

Mr. McLaren of the com'ite on side track leases, reported that the attorneys of the League are preparing to file formal complaint with the Interstate Commerce Commission against unfair liability clauses.

C. J. Austin, chairman of the com'ite on import and export traffic, reported that the com'ite had obtained a modification of the former regulations on the 3% war tax on transportation charges, so that now shipments are moved from points in the United States to Mexico and Canada on a thru B/L exempt from all taxation. Formerly the shipments were taxed to the border and there rebilled. The com'ite suggested co-operation with other bodies looking toward the contemplated complete revision of the tax law, as the com'ite of Congress that framed the bill never intended to tax export freight.

AUTHORITY for the Grain Corporation to sell for cash or credit 5,000,000 barrels of flour for export to European governments was granted by the House of Representatives Mar. 15.



Cutting Away Solid Rock to Make Room for Sydney, N. S. W., Storage Elevator.  
[See pages 560 to 570.]



## Handling Pacific Northwest Wheat in Bulk.

BY S. W. BLUE.

The increasing cost of sacks and labor necessary to handle grain in sacks during recent years made it evident to the grain producers of the Pacific Northwest that a more economical method of handling must be found than the system of handling in sacks, which had been used since this section of the country first began marketing grain.

Tho a clever propaganda had been conducted, intended to prejudice the farmers against bulk handling, the more progressive growers began marketing their grain in bulk. Since handling the sacks furnishes a nice profit for the sack dealer, the bulk handling movement at its inception met with much opposition from those interests with pecuniary reasons for desiring a continuation of handling grain in sacks. The growers were also greatly handicapped by the fact that there were no mills or terminal elevators on the Pacific Coast equipped to handle shipments in bulk.

The Astoria Port Commission, believing in the future of bulk handling and of Astoria as a milling and grain export terminal, decided to meet the needs of the grain trade by erecting a terminal elevator on its property at Smith's Point Terminal. Construction was commenced in 1917 and the elevator was in operation to handle the 1918 crop. The developments of the past two years have proved their judgment was sound. From the small beginning a few years ago by a few far-sighted and progressive growers, the bulk handling method has steadily grown until the amount of bulk wheat being marketed at the terminals will soon equal the sacked wheat.

A large majority of the wheat of the Pacific Northwest is of the soft varieties and one of the principal objections advanced

by the opponents of the bulk handling method was that this wheat would not stand the long off-shore shipment to Europe in bulk. This argument might have been valid years ago when the carriers were wooden sailing vessels, which were then forced to make the long voyage around the "Horn," but with the opening of the Panama Canal and the use of fast modern steel steamers, this objection was entirely eliminated.

On July 16, 1919, the Port of Astoria sent out the first bulk wheat cargo from its terminal elevator, which was the first bulk cargo from the Columbia River. This wheat went thru the Panama Canal, via New York, to St. Nazaire, France, where it was discharged. The following is an extract from a letter received from Captain Bierd, of the S. S. "WEST ISLAY", which carried the cargo:

"The WEST ISLAY finished discharging at St. Nazaire October 11th. The grain came out in fine shape, with no sign of dampness anywhere. There was practically no settlement of the bulk cargo at all. \* \* \* There is absolutely no reason why all grain can not be carried in bulk."

In selecting the wheat for this cargo, White Club, Soft White and Hard White were chosen, as they were typical Pacific Northwest wheats, and the fact that the shipment came thru in such good shape is very gratifying to the Port Commission, for it proves the contention that wheat of the Pacific Northwest can be safely exported in bulk.

ANOTHER OBJECTION advanced by opponents of the bulk handling method was that grain stored in concrete tanks in this climate would sweat and take on an excess of moisture, causing it to bin burn and become damaged, but experience at Astoria has been to the contrary. The Port of Astoria Terminal Elevator is located at the Columbia River Gateway, ten miles from the Pacific Ocean, with an average annual rainfall of eighty-three inches, yet wheat stored in the

concrete tanks from Sept. 1918, to July, 1919, showed only 10.8% of moisture and no trace whatever of sweat or damage was found in any of the tanks when the wheat was shipped out.

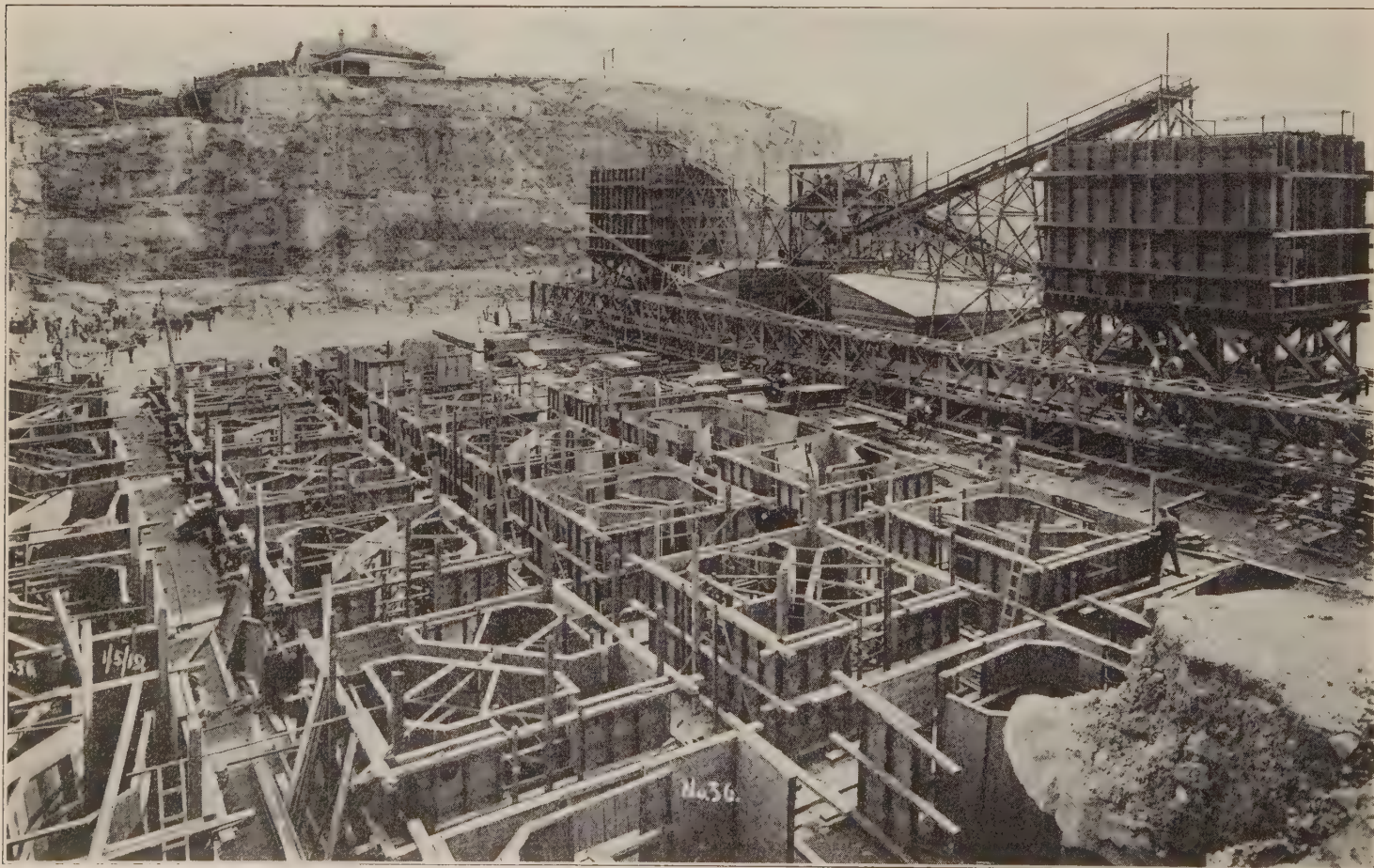
Of the wheat stored in the bulk plant during 1918-1919, many sales were made to eastern buyers. One firm purchased a half million bushels of Hard Red Spring and Hard Red Winter wheat, which moved east by rail. The buyers were so well pleased with its condition that, unsolicited, they wrote as follows:

"Referring to the wheat that you have loaded for our company for eastern shipment, I am pleased to say that this wheat has been delivered in the very best of condition. We have handled a great number of large lots of wheat but never before have we had wheat come to us in better shape than recent shipments from Astoria. The cars have all held up to your weights and grades. You certainly have a wonderful plant for handling bulk wheat."

These are only a few of many instances where it has been conclusively demonstrated that wheat of the Pacific Northwest can be handled in bulk as safely as in sacks and at a greatly reduced expense for handling. The Port of Astoria Terminal Elevator, which made this successful experiment in handling large quantities of Northwest wheat in bulk, is of re-inforced concrete, with 1,250,000 bus. storage capacity. It has modern equipment for unloading, loading and cleaning, all machinery being driven by electricity.

In the matter of receiving wheat, this plant is able to render as efficient service as in shipping. The State Grain Inspection Department of the Public Service Commission of Oregon maintains an office in Pier One and all sampling, weighing and inspection are performed by bonded employees of the State Department.

The Federal Grain Supervision also maintains an office at the terminal and the grades of all wheat, oats and corn received are



Forms for Constructing Columns Supporting Bins of First Section of Storage Elevator at Sydney, N. S. W.  
[See pages 560 to 570.]



checked by that office, thus giving the shipper a double check on his grain and insuring correct grading. In addition to the car test made by the State Sampler, the Port of Astoria also makes an examination of each car received and a detailed report is furnished the shipper, facilitating an early settlement of claim for loss.

While some problems are yet to be solved in the bulk handling system of the Pacific Northwest, both in the country and at the terminals, the experimental stage is now past. A careful check of the expense of handling grain in bulk as compared with handling in sacks, every operation considered from the time it is threshed until it reaches the rolls at the mills, shows such a margin in favor of the bulk handling method that it will be only a few years until the sack as a grain container will be obsolete in the Pacific Northwest.

THE WHEAT CLAIMS Com'tee of the Texas Grain Dealers Ass'n as recently appointed is composed of B. E. Clement, Waco; J. T. Stark, Plano; F. I. White, Yoakum; J. D. Hunter, Amarillo; Dick O'Bannon, Claremore, Okla.; E. W. Crouch, McGregor; and H. B. Dorsey, Fort Worth.—R.

THE UNITED STATES Grain Corporation has since its organization 2½ years ago had an operating expense .076 of 1% and never more than 1,169 employees. The corporation bot \$3,653,000,000 worth of foodstuffs and sold \$3,540,000,000 worth. This makes a total turnover of \$7,193,000,000.

## Sue Railroads on Clear Record Shortage Claims.

BY OWEN L. COON.

The decision in docket 9009 of the Interstate Commerce Commission is certain to bring a great deal of change in the handling of loss and damage claims. In that decision, the Commission goes over the entire field respecting the adjustment of grain claims. I receive so many inquiries asking what can be done with clear record claims, that it is wise to consider such inquiries in the light of this new decision.

All clear record claims arising during federal control were ordered refused by the Railroad Administration, and it is well to remember that Mr. Howard was head of the Claims section and was probably more responsible for that order than any other one man. Upon all such claims, there is no opportunity for recovery at the present time other than by starting suit. If such a claim is sued upon and the loading weight can be made out without a doubt, shippers are certain to recover in court. But it is a question of going into court to get the money. If more shippers would don their fighting clothes, probably the railroads could be made to understand that the shippers have some rights.

Some shippers think that if they can recover on a clear record claim, that will set a precedent which will result in the payment of all clear record claims, but such is not so. Each case is different and recovery in each

one depends upon making out the loading weight. As far as precedents are concerned, the precedent has long been established. Numerous cases can be found holding the carrier liable for any proved difference between loading and unloading weights, but the attitude assumed by the carriers, forces the shipper to make a precedent of each case and prove his loading weight. Remember that the precedents are all ready established and it is simply your task to make your claim and your loading weight come within the precedent established.

Under federal control, clear record claims have been refused. The carriers will probably persist in such a policy. The recent decision of the commission orders the carriers to pay clear record claims, but the condition upon which such is placed, is that the shipper shall have scale facilities of the kind approved by the commission. So many shippers do not have such scale facilities that the carriers will probably crawl out from under the decision, and refuse clear record claims as in the past until the shippers improve their weighing facilities. It is well to remember that the decision of the Interstate Commerce Commission puts a burden upon the shipper in this respect as well as upon the carrier.

As a practical proposition, however, the shipper should take steps to start suit upon all large clear record claims, where he knows from his records that his loading weight can be made out correct.

It is his only chance for recovery, and the more shippers doing this the more the railroads will respect the shippers rights.



Steel Reinforcing of Slab Supporting Bins of First Section of Storage Elevator at Sydney, N. S. W.  
[See pages 560 to 570.]



### Suit Involving Government Oats Contract.

The United States government, in Nov., 1917 called for bids for large quantities of oats and bran for the embarkation camp at Newport News, Va. The bids were to be opened on Nov. 15. The plaintiffs, Upton & Walker, who were wholesale dealers at Newport News, desiring to make a bid for these supplies, applied to the defendant, R. D. Holloway, a wholesale broker in such supplies, for a one-day option, at stated prices, on such supplies, and it was given. When the bids were opened, it was found that the plaintiffs were the lowest bidders, and it was supposed the contract would be awarded to them.

The call for bids required deliveries at Newport News. Supplies of this nature had to be purchased elsewhere and transported to Newport News, and the United States government had practically laid an embargo on private shipments, so that the dealers in such supplies in the Western states, from which the great bulk of them had to come, would not sell and deliver them in considerable quantities, except to purchasers for the use of the government, who could furnish a government permit for speedy shipment.

On Nov. 19, 1917, just four days after the bids were opened, the defendant entered into a written contract with the plaintiffs to furnish them 132 carloads of oats at 74 cents per bushel and 15 carloads of bran at \$36 per ton. After this contract was entered into, the defendant made purchases in Ohio to enable him to fulfill his obligation, and the sellers there were urging him to send on the government permit for transportation, or the "serial number" of the contract, so as to enable them to promptly forward the supplies, and thus avoid warehouse or storage charges, and the defendant claims that he made similar demands upon the plaintiffs, and the plaintiffs promised compliance from day to day. As the contract between the plaintiffs and the government had not in fact been entered into, they could not furnish the permit; but the defendant claims that he was ignorant of this fact, and expected daily that the plaintiffs would furnish the permit to him, so that he could forward it to his shippers.

Finally, on the 29th of November, the government rejected the bid of the plaintiffs, and gave notice that it would accept bids on Nov. 30th, for practically the same supplies mentioned in the first call. The plaintiffs were again bidders for the same amount of supplies as they had bid for under the first call, but at increased prices. The plaintiffs were again the lowest bidders, and the contract was awarded to them, and was subsequently executed by them.

The defendant repudiated the contract on the ground, as he states, that the plaintiffs had defrauded him and obtained the contract by false representations that they had the first contract and that he did not consider himself bound by it and would not deliver a pound of the oats. The plaintiffs, being under obligations to the government to deliver the oats and bran at specified times and prices, went upon the market and bought them at advanced prices, and charged the difference to the defendant. This difference amounted to \$59,668.18, for which the present action was brought.

About Dec. 15, 1917, before defendant alleges he first discovered the fraud, he obtained from the plaintiffs \$10,774.40 for eight carloads of oats then on the railroad tracks in Newport News, which was the value of the oats at 74 cents per bushel. After the alleged discovery of the fraud, the defendant refused to deliver these oats, but was compelled by the government to deliver them, without passing upon the rights of the parties, or prejudicing them in any way. Between Nov. 19, 1917, and Dec. 15, 1917, oats advanced in price on the market from 74 cents a bushel to 95 cents a bushel, making a difference on the eight cars of \$3,057.60.

The defendant denied any liability under the contract of Nov. 19, 1917, on the ground of fraud and false representations in its procurement, and also filed a special plea under section 3299 of the Code (1904), claiming a recovery over against the plaintiffs of the sum of \$3,057.60 aforesaid, for advance in the price of the eight carloads of oats. The plaintiffs denied all fraud and false representations, and further insisted that, if there had been any such, the defendant, with full knowledge thereof, had subsequently ratified the contract. The jury found for the defendant on both defenses offered by him, and rendered a verdict in his favor for the sum of \$3,057.60.

Defendant on Nov. 28 left Virginia for Oklahoma on a hunting trip and on Dec. 19 learned for the first time that there had been a second bidding and that the plaintiffs had been awarded the contract under that bid. He then promptly repudiated the contract and refused to furnish any supplies thereunder.—*Supreme Court of Appeals of Virginia. 102 S. E. Rep. 54.*

A \$60,000,000 loss to spring wheat growers is the result of the existing federal grain standards alleges H. N. Owen of Minneapolis who is in Washington with a delegation from Minnesota and North Dakota that is asking a modification of the federal rules.

### Must Place Cars on Side Track for Loading.

Two switch tracks served the plant of the McIntire Coal & Builders Supply Co., part of which plant was upon land of the Zanesville Terminal Railroad Co., as per contract.

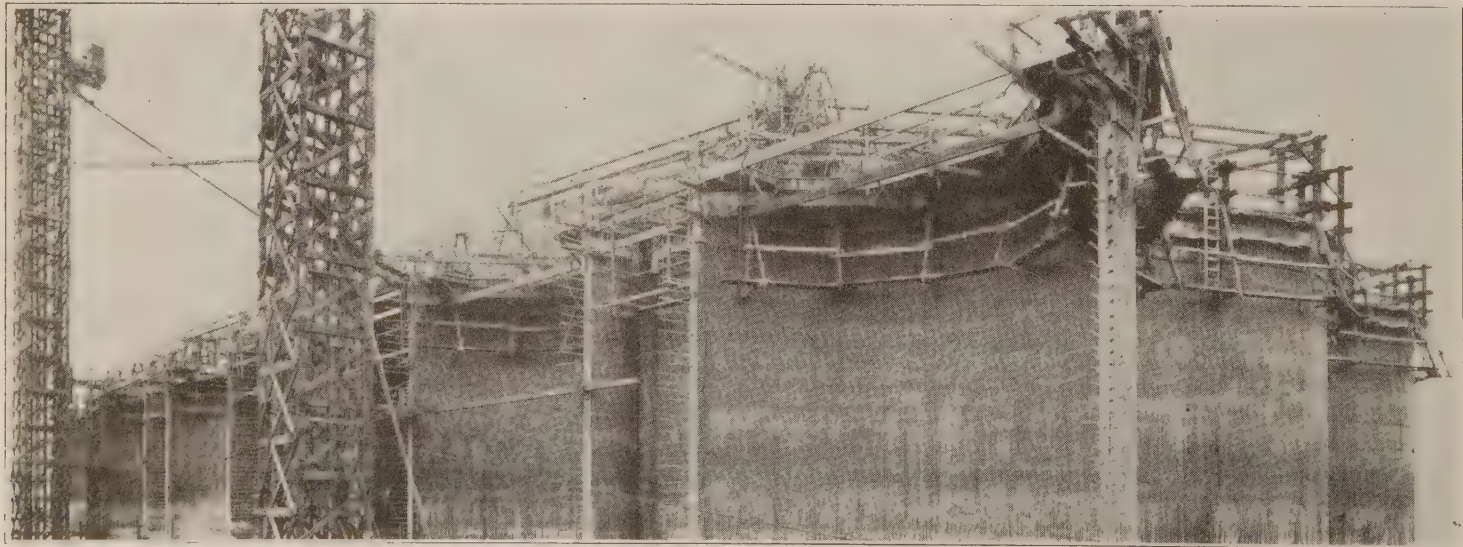
Contending that the Supply Co. was no longer entitled to the possession of the land or to service over the tracks the railroad company declined to place cars for loading.

The Supply Co. made complaint to the Ohio Public Utilities Commission, which issued an order requiring the Zanesville Terminal Railroad Co. and the Pennsylvania Railroad Co. "from and after the date of the service of this order, and for so long as complainant, said McIntire Coal & Builders' Supply Co. shall occupy its present premises, to place for unloading on the side track of the McIntire Coal & Builders' Supply Co. all loaded cars received at Zanesville, O., consigned to said the McIntire Coal & Builders' Supply Co., and, upon requisition therefore, to place upon the aforesaid side tracks for loading such empty cars, or its pro rata share of the available car supply, as said the McIntire Coal & Builders' Supply Co. may require for the forwarding of shipments."

The railroad company then went to the Supreme Court of Ohio but was worsted, the Public Utilities Commission being sustained.

The facts found by the Commission were: that the side tracks in question until a short time prior to the filing of the complaint were used for the purpose of furnishing proper and adequate shipping facilities to the complainant; that said side tracks were at the time of making said order, and theretofore, in a safe and proper condition to be operated over for the purpose of furnishing switching and shipping facilities to and from the complainant's plant; and that the railroad companies had refused to operate cars over said side tracks or place cars thereon. The Commission further found that complainant was occupying grounds belonging to the railroad company, but refused to consider or determine the question of the right so to do, and also refused to consider whether certain rentals were due from the complainant to defendants, or certain refunders were due from the defendant companies to the complainant, holding that those questions were not within the jurisdiction of the Commission and did not affect the rights of the parties in this proceeding. The Commission held that under the testimony disclosed by the record complainant was entitled to the service demanded by virtue of the provisions of section 504, General Code, and that said companies were required to provide such service in order to comply with the requirement that "each railroad shall furnish reasonably adequate service and facilities."

Chief Justice Nichols of the Supreme Court said. The Public Utilities Commission was right in refusing to consider and determine the questions growing out of such controversy. It has no such jurisdiction. It is sufficient that it appear, as it does in this record, that complainant is and has been for some time in possession of a plant adjoining these tracks, the



First Section Bins, Sydney, N. S. W., Terminal Elevator, Showing Lateral Reinforcing Protruding from Edge of Bin Wall.  
[See pages 561 to 570.]



operation of which requires the service and facilities sought. If it is not entitled to possession, an ejectment action must be prosecuted elsewhere, and such action is now pending. The Public Utilities Commission has only such jurisdiction as is conferred by statute.—126 N. E. 56.

## Our Export Flour Trade Handicapped.

A. P. Husband, who has completed a tour of Europe as sec'y of the Millers National Federation, writes as follows:

It is useless however, to expect business in any volume so long as we are working on the basis of the present guaranteed price for wheat, while Argentine is making hay while the sun shines, and is supplying the flour and wheat needs of Holland, Sweden, Norway, and to some extent of Denmark and United Kingdom. Our wheat price is prohibitive in competition with Argentine, even if we were not at a disadvantage as a result of the abnormal exchange situation.

The sum total of the situation is that we have lost, for this crop at least, our flour business in Holland, Sweden and Norway, with greatly reduced shipments of American flour to Denmark and United Kingdom, the business thus lost being diverted largely to Argentine. One naturally feels, after being over the ground, that the American miller has been compelled to make heavy sacrifice in order that our Government's wheat guarantee to the farmer might be fulfilled; and the question suggests itself whether our country, as well as our millers, would not have been benefited by drawing upon the billion dollars appropriated, in so far as necessary to meet competition abroad, and thus have enabled our millers to retain the business which they had developed in the countries named,—business which we have enjoyed for years and gained originally at considerable expense of time and money. There may be factors in the situation with which only those in official capacity are familiar, but I have been unable to explain to importers why some means were not adopted to protect the foreign interests of American millers during the present crop.

## North Dakota Farmers Grain Dealers Meet.

The ninth annual convention of the North Dakota Farmers' Grain Dealers Ass'n was held at Grand Forks, N. D., March 3 and 4. After the invocation an address of welcome was delivered by the mayor of Grand Forks.

Included in the speakers of the first day was Alex Carr, who spoke on the prevailing unrest of the day. He emphasized that of more importance than the grading and marketing of grain was the necessity of properly training the children and 10,000,000 aliens in the United States. Geo. C. Reader, sec'y of the State's Good Roads Ass'n told of the progress made on roads within the state.

In the evening session the convention was entertained by an Italian quartette.

In an open discussion the dealers expressed their personal views. It was found that they were pleased with their experiences with automatic scales. Dealers also spoke of the inability to get cars. Many of them expressed a feeling of confidence that conditions are sure to be better with the railroads under private control.

Ex-pres. Lee spoke on the progress made by the ass'n since its organization. A. F. Nelson, v.-pres. of the National Council spoke on the conditions of the Farmer's Grain Dealers of Minnesota. He also told of the reduction of bonds for managers from \$15 to \$3.50 per thousand.

Sec'y Gaulke spoke on conditions of the ass'n. He urged more funds so that the ass'n could do better work by co-operating with other states.

H. R. Meisch, pres. of the Minnesota Farmers Dealers opened the Wednesday afternoon session with a talk in which he urged the ass'n to raise its dues to get itself on a sound foundation; pay the sec'y a living wage and in speaking of the national organization urged that it hire good men and pay them accordingly. A. F. Nelson, sec'y of the Minnesota Farmers Dealers, also spoke on the national organization.

Legal matters of interest to the grain men were next discussed by J. T. F. O'Connor.

Dr. T. F. Kane, pres. of the state university in a talk on Wednesday evening told the dealers that the people expected a real service from the grain men.

THURSDAY was taken up mostly by com'te reports and resolutions. The com'te investigating car shortage and car distribution reported that because of the insufficient distribution of empty freight cars suitable to move grain a great loss resulted to the grain shippers. This also has caused the snipper a loss in items of insurance, interest, and inability to avail themselves of the terminal premium. This com'te was composed of J. F. Jacobson, C. S. Beighle, and F. Beier, Sr.

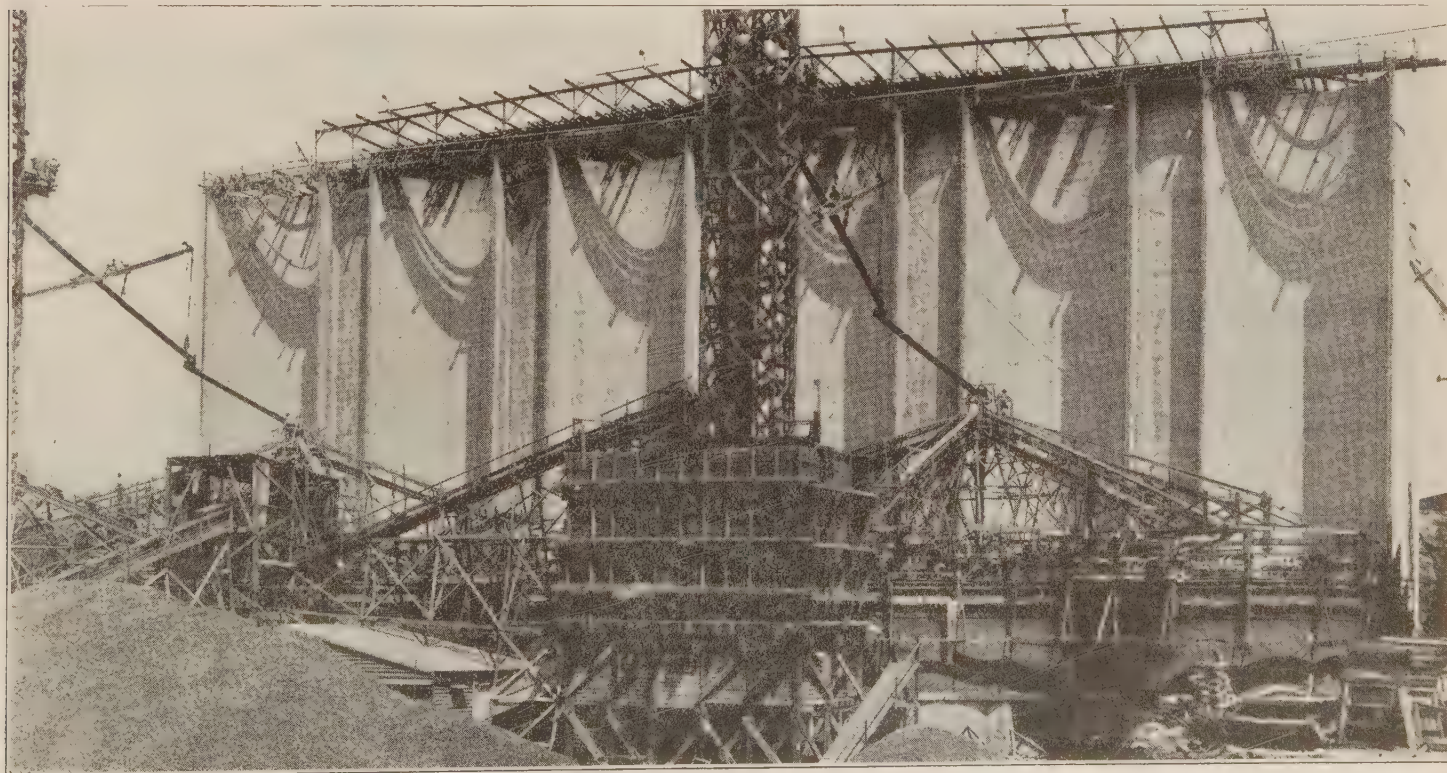
The following officers were chosen for the coming year: Pres., A. A. Lane; sec'y, M. C. Gaulke. Directors chosen were A. B. Hunt, R. McClaskey, L. B. Allen, and A. M. Thompson.

The treasurer's report by Mr. Ormsby showed cash on hand amounted to \$965.31 with amounts due of about \$800 making a total capital of about \$1,800.

Sec'y Gaulke gave report on the year's activities. He recommended the raising of dues; the employment of a sec'y who could devote his entire time to the organization; and the appointment of a legal and grievance com'te.

The resolutions adopted recommended that the officers be given power to organize a mutual insurance company, that no reduction of prices of farm products be made without a similar reduction of other commodities on the market at the same time; that as the bonding cost was lower more dealers should avail themselves of this privilege; and that the name of the national organization should be changed to the Farmer Grain Dealers National Ass'n.

On Thursday evening a banquet was held at the First Methodist Church. The dealers then went to the Commercial Club where a smoker and entertainment closed the convention.



During the First 24 Hours of Construction of the Third Section of Storage Bins of Sydney, N. S. W., Elevator. Walls Were Raised Eleven Feet.  
[See pages 560 to 570.]



# Scalemen's Association Solves Weight Problems

The 5th annual meeting of the National Scale Men's Ass'n was held at Chicago, Ill., Mar 16 to 19 at the Auditorium Hotel, where the opening sessions filled every chair, the varied program lending great interest to the more than 200 in attendance.

F. C. Maegly, A. G. F. A. of the Santa Fe, well known to grain men, made an address, saying in part:

Where commodities are weighed on the scales of the seller but are sold on the basis of the scale weights of the buyer, should not the weighing supervision departments which operate at both the buyer and seller ends of the transaction take more interest in excessive discrepancies when they see them, and in co-operation locate the causes and if possible correct them? There is an immense traffic in commodities sold by weight and shipped from one point having weighing supervision to another, commodities bot and sold by weight; either the seller's or buyer's weights govern. Some of these lots show overruns, others show shortages. Some of the variations are treated as normal, others are recognized as abnormal. Query: Are we doing everything we can, in co-operation with one another to locate the real causes of the abnormal discrepancies and to correct them where practicable, thereby removing the number of such abnormal discrepancies?

H. C. Boyden, of the Portland Cement Ass'n, made a talk on the points that must be observed in constructing cement concrete scale pits, from which we take the following:

## Correct Proportioning of Concrete.

Exhaustive studies at the Structural Materials Laboratory at Lewis Institute, Chicago, operated jointly by the Portland Cement Ass'n and the Lewis Institute, have shown that the

present practice of proportioning concrete aggregates in arbitrary proportions is wrong. There is only one single proportion peculiar to each aggregate that, when used, will give the greatest strength, density and wearing quality for that aggregate. The practice of specifying that a concrete shall be a 1:2:3 mixture without knowing that that proportion is the correct one for the particular aggregate to be used on the job should be abandoned, and it should be specified that the concrete shall have a certain compressive strength and wearing quality.

The particular aggregate available should be tested and used in the one proper proportion to give the results desired.

A wet, sloppy mixture gives a weak porous concrete. Concrete should never have one drop more water in it than will give a workable, plastic mixture. Add one pint more than this amount and you weaken the concrete just as tho you had taken out 2 or 3 pounds of cement.

Every aggregate and every mixture of aggregate requires a different amount of water than does another mixture, so it is impossible to give a table showing the proper amount to be used.

As a basis to work on, however, when the following mixtures are the proper proportions for your aggregate, a 1:2:4 mixture will require from 6 to 6½ gallons of water per sack of cement, a 1:2:3 mix 5½ to 6½ gallons, and a 1:1½:3 mix 5½ to 6 gallons. Test this out by carefully filling a sheet metal frustum of a cone 4 inches at the top, 8 inches at the bottom and 12 inches high. When this is filled and thoroly worked with an iron rod lift off the casing and measure the settlement or slump. The proper slump should be between 1½ and 2 inches.

You cannot make proper concrete and have it run on a chute on a flat grade.

Concrete must be kept moist or wet for from two weeks to 21 days, and should not be subjected to strains under 28 days.

J. F. Cruikshank of Kansas City delivered

an address on motor truck scales from which we take the following:

## Motor Truck Scales.

In the case of the motor truck, the loads are as a rule much greater than handled by wagons or drays, and the trucks are so designed that a very great percentage of the load is carried on the rear axle. This percentage ranges from 60% to 95% of the entire load, so when the entire truck is on the platform, the full load is not equally distributed on the four bearings or corners, but as much as 95% of this load may necessarily be sustained by two bearings on one-half the scale.

The motor truck scale must therefore be designed to sustain the maximum load that may be concentrated on any part of the platform, and the rated capacity cannot be based on four times the load that the scale is capable of carrying on each platform bearing but on 195% of this.

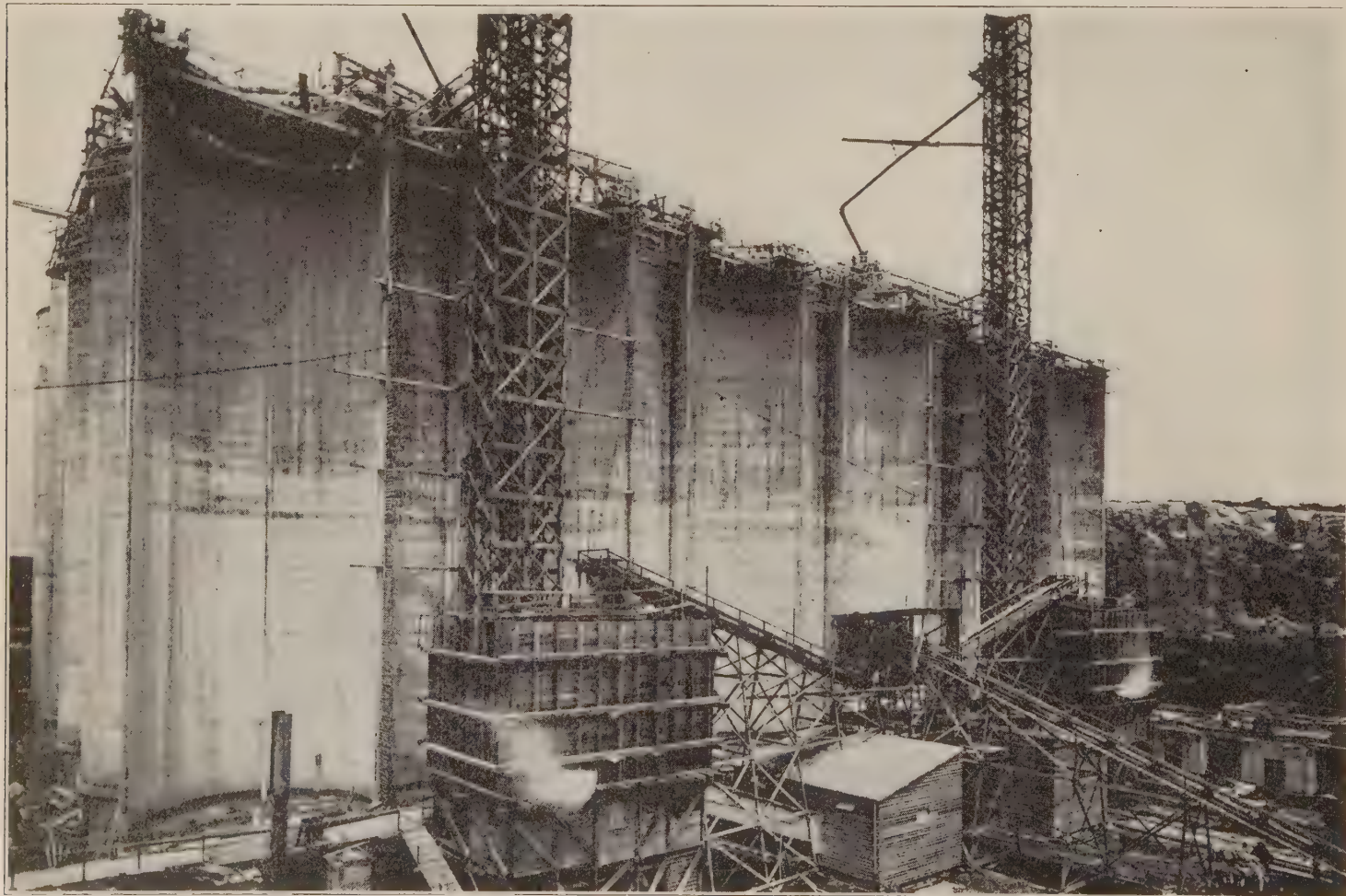
While it may seem that this high factor of safety is unnecessary, it must be remembered that the service condition of the motor truck scale is the most severe of any encountered for scale equipment used for weighing commodities transported on wheel vehicles. The scale platform is subjected to all kinds of external forces that tend to cause displacement on its points of supports, so in addition to providing a high factor of safety, it is very necessary that some provision be made to prevent the displacement of bearings on the knife edges or the shifting or scale levers and other movable parts from their proper positions, and as the most effective means for accomplishing this result has proven to be the suspended platform. Some type of suspension bearing is or should be necessary in the motor truck scale.

H. A. Foss, chief of the Board of Trade Weighing Department, said:

## The Association a School.

The Executive Com'te of our Ass'n, seemingly, has listed me as a regular performer at our annual meetings, for they have not failed, for some years past, to record my name on the program regardless of whether or no I have been willing to perform. Or, perhaps this talking each year is one of the prerogatives and duties of honorary members.

Now, the primary function of this organization of Scale Men is to solve many and diverse problems. In a measure, our Ass'n is a school



Showing Open Side of Bins of Second Section, Sydney, N. S. W., Terminal Elevator.  
[See pages 560 to 570.]



of instruction, where we, so called experts, assemble for post graduate work. For my part, being a father, and a grandpap, too, it especially behooves me to gather all the knowledge I can, and on as many subjects as possible, in order that I may instruct my offsprings correctly. And, in connection with these parents' problems, I want to say to you single men and bachelors that we grandpaps are often asked by our youngsters some very, very embarrassing questions of a highly technical nature, embarrassing because we are unable to answer them. For example, how would you experts answer questions such as these:

Why does the world go round?  
Why is the sea never still?  
Where does the wind begin?  
What makes an echo?  
Could the sky fall down?  
Why does a ball bounce?  
Why can we not see in the dark?  
Why do we dream?  
Why does the kettle sing?  
Why are tears salt?  
What makes a cat purr?

But, perhaps you scale men do not consider these questions technical questions when compared to the momentous problems with which you come in daily contact, such as monolithic construction, and stresses and strains, and Young's Modulus, and formulas for treating steel, and beam travel, its period and much mooted S. R.

As I listen to the many instructive papers, necessarily dry and technical, that are read at our meetings, I cannot help thinking that you are one of the most patient body of men that I have ever known to sit in a convention, days at a time, devouring reams of technical reports, and still retain your good nature and good digestion. And, it is well that it is so, for the knowledge that you scale men garner and absorb at these meetings is of inestimable value, not only to each individual member, but to the public as well. Why, my assistant, Joe Schmitz, does not consider a scale man worth while that does not attend our Ass'n meetings—and read the Scale Journal—and I don't know but what he's right. In any event, I would not dare to cancel my subscription for the Scale Journal unless I was prepared to move into a warmer clime.

C. L. Sommars, scale inspector of the Grain Exchange, Omaha, Neb., read a well received paper on "The Inspection and Test of Hopper Grain Scales," following:

## Testing Hopper Grain Scales.

The large percentage of our members are now or have been active in doing the best they could to the betterment of scales constructed and in operation for years, rather than the up-to-date equipment designed and installed under the rules adopted by the American Railway Ass'n and various other conferences. To the latter scales we have no general word of complaint, and very little trouble, but each in turn as referred to before have had largely the tremendous task of matching our wits and experiences to the rebuilding of foundations and resetting of scales in perfectly good elevators and warehouses, so far as storage is concerned.

We have all been at work where our authority was not great enough to order a two hundred thousand to one million bushel house of old construction torn down and a new construction made to replace it and as a consequence it has been a hard fight for perfection.

Looking from a distance I can see all of the terminal markets with their old troubles and their new pleasures, and I am frank to admit our Gateway, while it has had its many readjustments, is now working to within the tolerance as prescribed by any Weighing Bureau.

Long ago somebody conceived the idea of setting a scale on the top of an elevator and thus made hopper scales possible. Ultimately the result of this achievement has kept the scale inspector very busy at work ever since, to convince himself that the scale is really setting on a rigid and substantial foundation.

It has only been a few years ago that a grain hopper scale testing out to within one pound on a thousand was considered adequate.

Even the sensibility reciprocal in those days, generally speaking, was given very little consideration, and today, we all know that the S. R. is one of the most important items and one that is given the closest consideration by a scale inspector. Some of us have got down to such close refinements that we are even taking the period of the beam. Commercially speaking, the material effect that this would have on the accuracy of a scale, has the same meaning to the scaleman who applies this in his work, as it does to the physician who takes the pulse of a human being as a part of his general practice.

Hopper scales as a general rule that were constructed prior to the last four or five years and as far back as twenty or twenty-five years ago were sometimes installed and built on the cribbing, some were set on posts on concrete from the ground up. These in most cases only supported the four corners of the scale levers and the beam and shelf levers were set on separate studding independent of the scale founda-

tion proper, and in some cases the beam was set at a more desirable location by the use of extension levers. Incorrect weighing can be attributed to this sort of construction more perhaps than any other cause, and not until this construction is entirely eliminated, are we going to have reliable and dependable hopper scales.

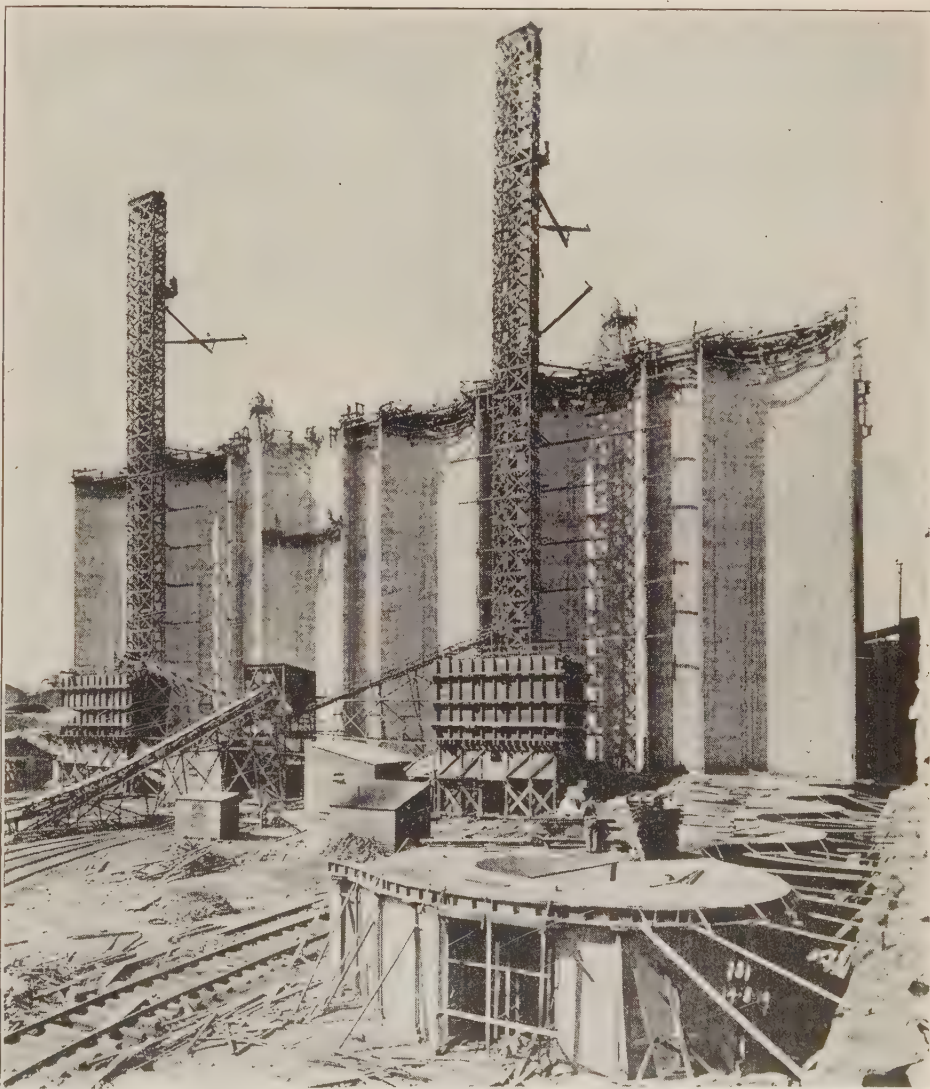
In testing a hopper scale today you should use at least test weights to within 8% of the capacity of the scale, and is the first thing to do in order to secure the most satisfactory results. Before applying any weights know that your scale is level, see that your beam rod is plumb, and that all parts of the scale are in perfect alignment. After this is done, you must be certain that every particle of dirt and foreign material is cleaned from around the knife edges, bearings and loops; and it is not a bad idea to space the loops in both the scale and the beam to eliminate any possible friction. Carefully examine the check rods and see that none of them are tight and look over and around the sides of the hopper to see that there is no rubbing; and where canvas is employed around the top of hopper, see that this is not bagged and full of dust. After you are sure of all these things then you are ready to go ahead with your test. Assuming that we are going to test a 120,000-lb. capacity scale, balance your beam and take the S. R. which should be less than 12 pounds. Ten thousand pounds of 50-lb. test weights that you know are all right is enough to do this work. Apply the weights in units of 1,000 pounds uniformly distributed over the scale and if the reading of the beam shows "Standard" then continue placing the weights in the same proportion until you have applied the whole 10,000 lbs., and if the reading of your beams shows "Standard" all the way through, the empty test is OK. Take your weights off in the same proportions as before and check back the readings and note if there has been any shift in your balance.

Then you are ready for a load of grain in your hopper (about 100,000 or 110,000 lbs. will be enough), balance your beam with this load, get the S. R. of your beam again which must

be less than 14 pounds and apply your weights (the same as on the empty test and if the reading of the beam shows "Standard" all the way through your scale is OK, and you have completed a thorough and reliable test.

Supposing that this scale with the same test empty would have shown 2 lbs. minus or plus on the 10,000 lbs. and show "Standard" with the same amount of weights on the loaded test, the error between light and heavy could be attributed to what is commonly referred to as spread. This spread might be in the scale, but is generally credited to yielding foundations, spreading of the bearing feet, out of level and plumb conditions, and a good many variety of other causes. A large percentage of hopper scales are subject to these conditions and as long as it doesn't show any more than referred to, you are safe in calling the scale OK. However, you could divide this error by cutting it down or adding to on the empty test (whichever the case might be), and doing the same on the loaded test; that of course would be entirely up to the judgment of the scale inspector. On the other hand with the light test showing an error of, we will say  $\frac{1}{4}$  of a pound on the first 1,000 pounds,  $\frac{1}{2}$  lb. on the second, and so on until all of the 10,000 lbs. were applied, you would have an error of  $2\frac{1}{2}$  lbs., or  $\frac{1}{4}$  lb. on a thousand, and then again with the loaded test you would find your error in the same proportion as the light test. Then you would have a multiplying error or a total error on 100,000 lbs. of 25 lbs.

This scale would have to be adjusted, and assuming that conditions are as already referred to, the best thing to do would be to make a corner test by applying individually two thousand pound weights. Four thousand pounds would be better to each corner of the scale to find out if the error is in any one of the scale levers and if it was found to be in lever No. 3, adjust that lever by moving the nose iron. If it was not in any of the levers it would naturally be in the beam or the shelf lever; that being the case, make this adjustment on the beam pivot and by this adjustment you would also be cutting down the S. R.



Having Completed First Section of Bins, Mixers and Towers Were Pulled Back and Started Pouring Concrete for Second Sections of Sydney, N. S. W., Elevator.  
[See pages 560 to 570.]



The equipment used in handling grain from the time it leaves the car and is delivered to the scale and from the scale back to the car, to the scale inspector is as important as the scales, for the reason that the efficient operation of the scales includes the safe delivery of all the grain to them; and the man who is responsible for the accuracy of the scale ought to be particularly interested in this equipment. He should examine carefully all machinery used in the handling of grain to the scale where the grain is received and from the scale where the grain is shipped.

Examine the loading hopper and see that it is grain tight, examine elevating leg and the elevator head, so you are sure that all grain elevated goes into the garner or scale hopper. Examine garner and scale hopper slides, examine spout through which the grain flows when it leaves the scale to be loaded into the car. Also know that the grain is all swept clean out of cars at the point of unloading.

W. E. Thompson, supervisor of scales for the Minnesota Railroad & Warehouse Commission, explained his system for the removal of dust, in his paper entitled "Dust as an Element in Grain Weighing" along the lines of his address published in the Journal Oct. 25, 1919, page 739.

M. H. Ladd, weighmaster of the Chamber of Commerce, Milwaukee, Wis., delivered an address on "Vessel Grain Weighing" from which we take the following:

### Vessel Grain Weighing.

The hopper scales used in weighing grain for loading into vessels at water front elevators are located on the scale floor at the tops of the elevators. After being weighed, the grain is dropped from the scales into bins along the water front side of the elevator, known as the shipping bins. These bins are about half the depth of the regular storage bins, and are hopped toward the outside wall of the elevator, and the vessel loading spouts extend from the bottoms of these bins; these spouts must, of course, have sufficient slope to enable the grain to run freely. The space below the shipping bins is generally occupied by storage bins or "pockets" of which more will be said later.

The accuracy of the hopper scales is, of course, of primary importance. The agencies having supervision over the weighing of grain at terminal markets all maintain a scale department, and tests are made of all scales about twice each year, or oftener if necessary.

The size of the drafts weighed depends entirely, of course, upon the capacity of the scales, and we make it a rule to see that the drafts are as large as the capacity of the scale will permit.

The weighing of grain in even drafts is not permitted; in other words, no two consecutive drafts shall be for the same amount. You can readily see that the practice of weighing grain in even drafts, all drafts being for the same amount, is conducive of error.

Our vessel tally books are so arranged that

the supervising weighers obtain a sub total after every tenth draft. The tally sheets are kept in duplicate by the use of carbon paper. The original tally sheets are perforated and when the loading of the vessel is completed these originals are sent to the Weighing Department office, where they are carefully audited before a weight certificate is issued. The duplicate tally sheets remain bound in the book.

A draft is never entered in the tally book until it has been released from the scale; if for any reason a draft is held in the scale it is not entered in the book until the hopper has been opened and the grain is discharging into the shipping bin.

It is the duty of the supervising weighers to see that no grain is trapped in the scale hoppers. In other words, each draft must run entirely out of the scale hopper before the slide is closed preparatory to drawing down the next draft. This can best be accomplished by removing the counterpoise weights and placing the sliding poise at zero after each draft. This also gives the weigher an opportunity to detect any change in the balance of the scale, which must be examined at regular intervals during the loading of a vessel.

Immediately after the releasing of a draft from the scale hopper both the elevator weigher and the supervising weigher make the necessary entry on their tally books. After the entry has been made they call back the amount and number of the draft to each other, and after each tenth draft they compare sub totals.

The receiving hopper above the scale hopper is called the garner. The slides at the discharge openings in these garner as well as the scale slides, must, of course, be grain tight. Any leakage in these slides can be detected by holding an occasional draft in the scale for a short time.

Supervising the delivery of grain from scales to vessels is equally as important as the supervision of weighing. The discharge spouts leading from the scales must be properly placed in the stationary spouts which lead into the shipping bins; we have these spouts equipped so as to permit of sealing and the supervising weigher applies consecutively numbered car seals. These seals can only be removed by the supervising weigher. If the discharge spout is attached to the scale the weigher must see that there is no bind between the scale spout and the stationary spout. Spouts leading from scales to shipping bins should not pass over any bins which are not covered. At elevators where belt conveyors are used to convey the grain from scales to shipping bins we station a supervising weigher on the conveyor floor during the entire loading of the vessel.

The bottoms and sides of shipping bins should be inspected several times each season in order to guard against any loss of grain, particularly in old wooden elevators. If the construction of the elevator is such that there are mill bins or pockets under the shipping bins, these bins should be empty and opened at the bottom during the loading of a vessel, whenever possible. Any loss of grain from the shipping bins into these lower bins can then be readily detected. We have not experienced any difficulty with the shipping bins at the newer concrete houses, although I presume it would be possible,

in a poorly constructed elevator, for concrete to develop cracks along the sides or in the bottoms of the shipping bins. The discharge spouts at the bottoms of the shipping bins should also be inspected during the loading of each vessel. If these are turn spouts provision should be made for sealing them in the proper position.

It sometimes happens that an elevator operator desires to fill shipping bins before the arrival of the vessel. In these cases we apply seals to the shipping bin slides, which prevents the opening of the bins until the seals are broken by an authorized deputy of the Weighing Department.

Shipping bins which are open on top should have the sides built up higher than the outlets of any of the spouts leading into the bins, in order to prevent grain from flowing over into adjoining bins.

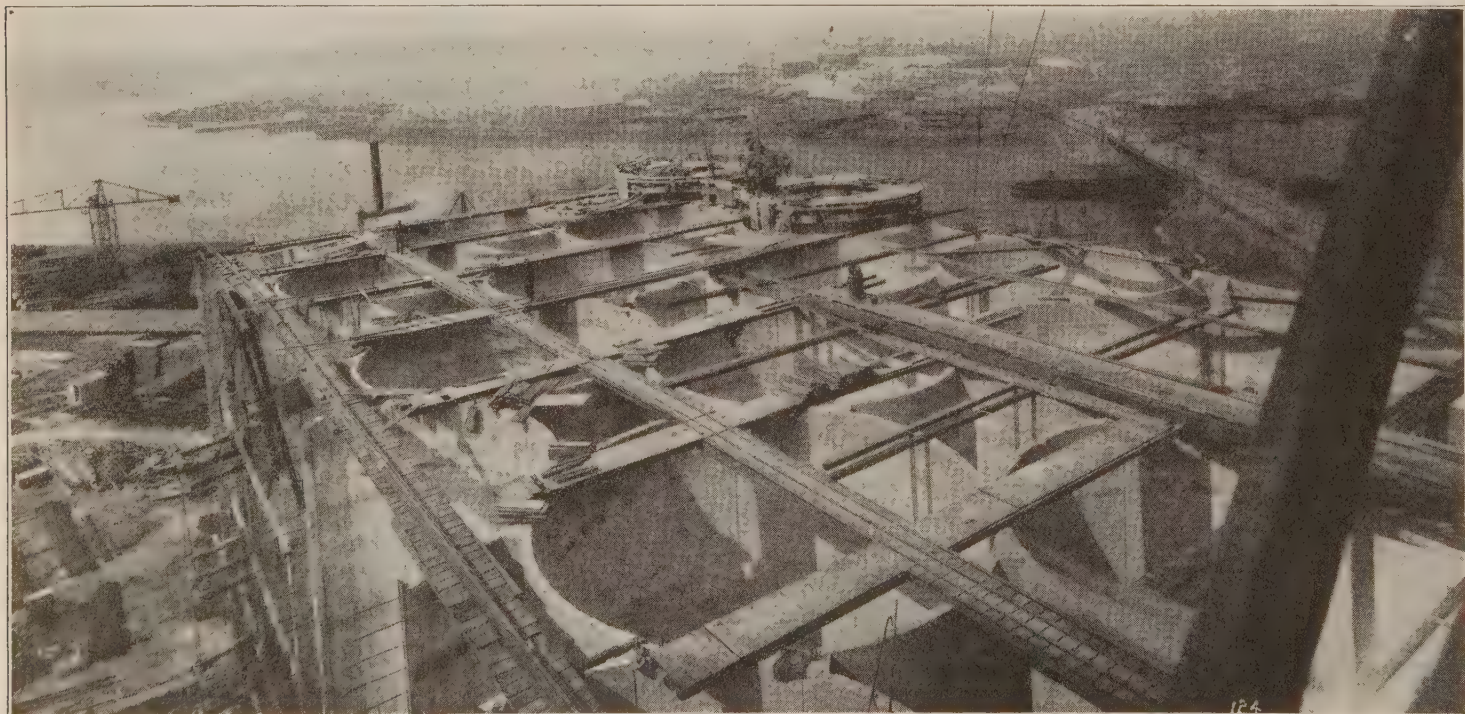
Before any grain is loaded into a vessel we make a thoro inspection of the vessel to be certain that all of the holds are grain tight and that no grain can escape into the bilge or tanks. In steel vessels, particularly those vessels which have been engaged in the coal or ore trade, we frequently find sprung bilge plates and open rivet holes that require repairing before the vessel will be in condition to receive grain.

A Weighing Department deputy is stationed on each vessel during the entire process of loading to guard against loss. It is his duty to see that the outlets of the loading spouts are kept as close as possible to the open hatches. On a windy day a considerable quantity of dust and chaff could blow away if this rule was not observed, which would, of course, cause a shortage in the weight of the grain. All grain that is carelessly scattered on the deck must be cleaned up immediately and thrown into the hold. After loading is completed, if there is any damaged grain scattered about the decks, which the grain inspector will not allow to go in with the cargo, this grain is put into bags and left on board the vessel, and a report of the amount thus handled is shown on the weight certificate.

The shipping bin must be examined to be certain that all grain has been delivered to the vessel. By dropping an extension light into these bins from above we can determine whether or not there is any grain lodged in the corners or on the bottom of the bins.

J. A. Schmitz, editor of the Scale Journal and assistant weighmaster of the Chicago Board of Trade, delivered an inspiring address saying, in part:

A good scale man is not known by his equipment but by the results he obtains. I want particularly to emphasize the fact that, unless all of the equipment supplied is used with efficiency, the results are problematic. A careful, painstaking, intelligent inspector will fully justify a reasonable expenditure for scales, equipment, etc. On the other hand, a careless, indifferent man who fails to use his equipment with care, skill and ef-



A View From the Top of First Section of Storage Bins, Sydney, N. S. W., Terminal Elevator.

[See pages 560 to 569.]



iciency, will not only nullify the effectiveness of the equipment placed in his charge, but he will also bring discredit and loss of prestige upon the entire craft. It is probably needless for me to say that being handy with tools is, no doubt, an aid to any scale inspector, but his ability to correctly diagnose trouble and his good judgment in applying the remedies are things that really count the most. There is no royal road to learning in the scale business.

H. A. Foss, chairman of the com'te on grain weighing, reported on the conference he had called at St. Louis, Mo., Oct. 12, the proceedings of which were published in the Grain Dealers Journal Oct. 25, page 739.

The scalemen made an interesting visit to the Chicago shops of Fairbanks, Morse & Co.

Officers elected for the ensuing year are, pres., B. B. Gordon, Logansport, Ind.; v-pres., C. G. Stoddy, Chicago, Ill.; sec'y-treas., A. G. Zeibel, Dallas, Tex.

It was voted to hold the next annual meeting at Chicago.

## Missouri Farmer Dealers Organize.

At the call of Ralph Loomis, extension agent in marketing of the University of Missouri, representatives of 17 elevator companies met at Moberly, Mo., on Feb. 25 and organized the Farmers Grain Dealers Ass'n of Missouri.

Mr. Loomis acted as chairman until G. A. Brown was selected as temporary chairman.

Officers selected were as follows: Pres., P. E. Donnell, Waco; 1st v-pres., G. A. Brown, Versailles; 2nd v-pres., F. A. Walton, Troy; treas., M. B. Runyon, Barnett; sec'y, E. E. Evans, Monroe city.

Directors chosen were Homer Musselman, Jamesport; W. W. Fuqua, Monroe city; J. F. Parker, Laddonia; W. D. Walkup, Greenridge; Wm. E. Wagenbreth, Jr., Chesterfield.

It was decided to make the dues amount to 25c per car charge in or out but not less than \$20.00 per year.

The slogan adopted by the directors of the new organization was "One hundred per cent of the farmers elevators in Missouri in our association in one year."

## Machine for Separating Wild Peas From Wheat.

An improved separator for taking wild peas from a mixture of wheat and peas is a recent invention based on the fact that peas, being round, will roll out of a mixture of wheat and peas.

It consists of a framework, 2, at the top of which is a spout, 3, which leads into a hopper, 4, thru a feed roller, 7, shown in the engraving herewith. The inclosed hopper has a small door in the top of the frame and has a spout in the bottom which leads into a compartment below containing a series of separating pans.

These pans extend nearly the width of the framework. They are in two series, each series being controlled by its own shaft, 23, which are at opposite ends of the frame. Each pan is preferably composed of a flat sheet metal plate having an upwardly projecting flange, 10, at its rear longitudinal edge, and said pans are provided with depending ears, 11, mounted in shafts, 12 and 12a, which are journaled in the end walls of the casing and extend horizontally thru the same. The pans are preferably mounted eccentrically on these shafts, that is, the shafts are at one side of the longitudinal center of the pans. There may be any desired number of pans in each group, as experience may determine to be necessary to effect the best possible separation of the peas from the wheat. By means of set-screws, 13, mounted in the brackets, 11, and engaging the shafts, the pans are supported for rotary adjustment on their shafts to vary their relative position until the desired degree of inclination or movement is obtained.

The shafts, 12, are provided with crank arms, 14 (Fig. 2), and a connecting bar, 15, has a loose connection at 16 with said arms for simultaneous movement thereof. The lower end of said bar is pivotally connected at 17 with an arm, 18, that is pivoted at 19 on the machine frame. Intermediate to its pivots the arm, 18, has an anti-friction roller, 20, in the path of a cam, 21, that is revolved by suitable means, as a pulley, 22, belted to a suitable source of power.

The operation of this machine is very sim-

ple considering the many parts it has. The mixture of peas and grain is poured in the spout, 3, from where it is fed into the hopper, 4, in small amounts by the feed roller, 7. From here it passes thru the spout, 6, upon pan, 9. As this pan is slightly inclined (as in Fig. 5) the peas that are on the outside of the little pile of peas and wheat will roll down the pan on account of their being round. The peas roll into a sort of half funnel, 30, into a small passage along the side of the frame, 29. The pan is then abruptly tipped by the controlling shaft, 23 (Fig. 2), causing the mixture of wheat and peas to fall upon a second pan (Fig. 6 and 7). This pan is also slightly inclined. The pile of wheat and peas is now in a different formation (Fig. 7) than when it was on the first pan (Fig. 5). Peas that are on the outside of this pile will roll away and the mixture will then be tipped by the shaft upon a third pan. This operation can be repeated on as many pans as desired. The roller, 20, and the cam, 21, provide the means for tipping one series of pans, while a similar cam and roller at the other end move the other series of pans. A pulley can be attached to cam for operating this machine at the pulley, 22. A similar pulley at the other end of the machine operates the other pans.

The advantage of this separator is its simplicity. Any man could take it apart and put it together again in a very short time. The hole in the top of the hopper provides a means to clean the apparatus should it become clogged. On account of having the pans adjustable by screws, 13, the amount of peas in the wheat will determine the number of pans to be used.

For this invention the letters patent No. 1,328,819 have been issued to Clarence W. Carter.

## Sale of Distillate Stopped.

Suspension of the sale of distillate by Pacific coast oil companies, including the Standard Oil Co., of California, has become effective and has caused no little comment and will cause an increase in the operating expense of those dealers who have been running their engines with distillate.

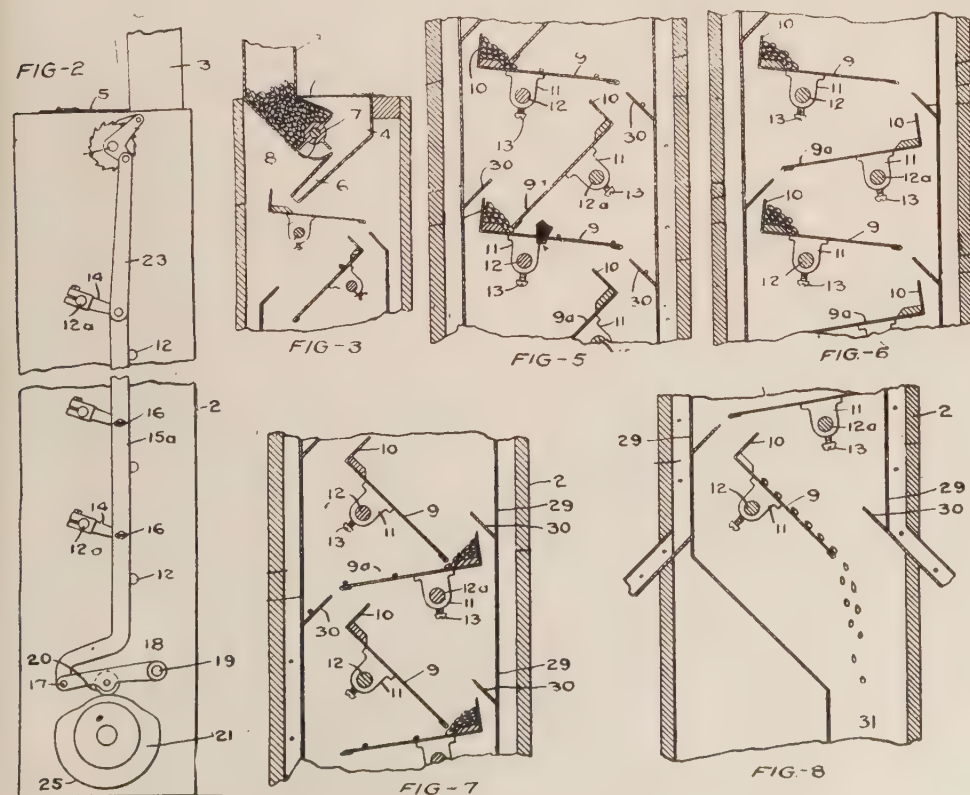
Distillate is a product of western crude oil and has been produced in large quantities since about 1880. Because it is more easily vaporized than kerosene it has been extensively used at a big saving in many engines that specify gasoline as fuel.

A statement made recently by C. H. Hamilton, sales mgr. of the Standard Oil Co. follows: "The company was, with great regret, forced to suspend the sale of engine distillate. It is to be hoped that the sale may be resumed, but conditions are such that very little encouragement in this direction can be held out."

"Engine distillate comes from refinable crude oil. The gasoline is first taken from the refinable crude, then distillate, then kerosene. The danger of the company being compelled to suspend the sale of engine distillate has existed for a considerable time. Only because of the fact that the company has had on hand a stock of the kind of oil which would yield adequate quantities of engine distillate, and because it was possible for the company to purchase distillate elsewhere, has it been possible to continue the sale of distillate even up to this time. Now this stock of oil utilized for manufacturing distillate has been consumed and at the same time the hitherto available source of distillate to be had by purchase has disappeared."

AUSTRALIA has sent nearly 100 men to this country to study intensive farming methods at the University of California.

THE SEED and farm implement dealers of the New England States say that on account of the shortage of farm help much of the acreage formerly devoted to garden products will be sown this year to grain.



Cross Sections Thru Wild Pea Separator.



# Grain Trade News

Reports of new firms, changes, deaths, casualties and failures; new elevators, improvements, fires and accidents are welcome. Let us hear from you.

## ARKANSAS

Rogers, Ark.—I am not in the grain business at present.—C. R. Latto.

Rogers, Ark.—The Rogers Milling Co. is the new name of the Ozark Milling Co.

## CALIFORNIA

Oakdale, Cal.—T. M. Maxwell, S. S. Stewart and H. E. Sandoval have engaged in the grain business here as representatives of the interests of the Frank A. Guernsey Grain Co., Stockton.

## CANADA

Clair, Sask.—The Clair Grain Growers Ass'n, Ltd., has been registered.

Farnham, Que.—Clifton M. Bought of the Farnham Grain Co. is dead.—B.

Melaval, Sask.—The Melaval Grain Growers Co-op. Ass'n has been registered.

Sovereign, Sask.—The Sovereign Grain Growers Ass'n, Ltd., has been registered.

Lucky Lake, Sask.—The Lucky Lake Grain Growers Ass'n, Ltd., has been registered.

Port Stanley, Ont.—The L. & Pt. St. Ry. Co. is interested in the erection of an elvtr. here.

Moose Jaw, Sask.—The Robin Hood Mills Co. will build a concrete elvtr. at a cost of \$150,000.—B.

Montreal, Que.—A. Pilon & Fils. hay and grain dealers, have recently been registered.—B.

Vancouver, B. C.—The Vancouver Grain Co. has been incorporated with a capital stock of \$1,000,000.

Toronto, Ont.—A. H. Brownlee, Ltd., grain dealers, incorporated with a capital stock of \$40,000.—B.

Verigin, Sask.—The grain elvtr. of the Verigin Grain Co. was destroyed by fire. It contained 40,000 bus. of wheat.—B.

Ottawa, Ont.—W. H. Dwyer, Ltd., which was recently incorporated for \$1,000,000, has taken over the Dwyer Elvtr. Co., Ltd.

New Lowell, Ont.—John A. Bell & Sons are considering the reconstruction of their grain elvtr. recently destroyed by fire.—B.

Calgary, Alta.—The United Grain Growers of Alberta will build 10 new elvtrs. this spring and many annexes will be added to the elvtrs. already built.

Wakaw, Sask.—Wm. Cantlon has been apprehended on the charge of destroying account books, etc., with the intent to defraud the grain elvtrs. Six other men have already been arrested and are said to be serving time for complicity in the job.

## WINNIPEG LETTER.

Winnipeg, Man.—The Ruthenian Farmers Elvtr. Co. plan to erect elvtrs. in Manitoba and Saskatchewan.—B.

Winnipeg, Man.—The Dominion Elvtr. Co. plans to erect 3 to 5 elvtrs. in the province of Manitoba. H. Cavaner is the gen. mgr.—B.

Winnipeg, Man.—The International Elvtr. Co. plans to erect elvtrs. in the provinces of Manitoba and Saskatchewan this year. The manager is J. C. Gage.—B.

Winnipeg, Man.—A new record price for a car of grain was recently made when a local company sold a car containing 1,921 bushels of No. 1 Northwestern flax at \$5.58 per bushel, bringing \$10,819.—B.

Winnipeg, Man.—At least 3 members of the Grain Exchange, Henry Lehmen, John Maloney and Ed. Cavanaugh, proved that they were Irish, St. Patrick's day. They paraded the pit smoking long church warden pipes tied with green ribbons, but alas! the next day the penalty of smoking on the floor was imposed on them.

Sam Scott, one of the pioneer grain dealers of the Province, retired from the grain business Mar. 19, after 34 years' service. He has been in ill health for some time and doctors ordered him to retire. Fellow members and friends of the exchange presented him with a handsome traveling bag. In 1886 he commenced buying grain for the Hudson's Bay Co. and was located at Morden and the following year at Larivière. In the years 1887-8 he was Ogilvie's representative at Boissevain, and the following year bought grain for the Lake of the Woods Milling Co. at Hartney. In 1890, he commenced business on his own behalf, building an elevator at Lauder, where he resided for the following 15 years. In 1905 he sold his elevator and removed to Winnipeg, opening a grain commission and broker's office, which he has conducted up to within the past year. He will devote his time to his many beautiful farms in Manitoba and vicinity.

## COLORADO

Peeetz, Colo.—The Farmers Co-op. Co. is building a new office and installing a 10-ton Fairbanks-Morse Truck Scale.

Dailey, Colo.—I am now mgr. for the Longmont Farmers Milling & Elvtr. Co., Longmont, at this station.—H. E. Combs.

Sterling, Colo.—McAtee Milling & Elvtr. Co., incorporated; capital stock, \$200,000; incorporators C. L. McAtee, Lee L. Siefert and J. H. Bauer.

Delta, Colo.—We are putting in a Richradson Cleaner and an alfalfa meal mill in connection with our other mill. We are not connected with the Delta Elvtr. Co. in any way.—Delta Flour Mills Co.

## IDAHO

Heyburn, Ida.—The Heyburn Produce & Grain Co. incorporated; capital stock, \$20,000; incorporators, C. M. Hardy, J. J. Connor and Frank M. Snyder.

## ILLINOIS

Elwin, Ill.—Farmers Elvtr. Co. now has W. W. Coogan as mgr.

Brimfield, Ill.—The Brimfield Elvtr. Co. is installing a new scale.

Woodford, Ill.—Farmers are making great efforts to organize an elvtr. company.

Pekin, Ill.—We expect to paint the elvtr. soon.—James Heck, mgr. Farmers Grain Co.

Sterling, Ill.—E. J. Bonner is local mgr. for Sam Mincer, who has opened an office here.

New Philadelphia, Ill.—Farmers will have a new company formed here in a very few days.

Lanesville, Ill.—R. L. Leonard has resigned as mgr. for the Farmers Lanesville Grain Co.

Leonard, Ill.—The Farmers Elvtr. Co. recently bot coal sheds and installed new motors.

Meredosia, Ill.—H. D. Berger has succeeded J. A. Martz as mgr. for the Farmers Grain Co.

Earlville, Ill.—The Earlville Farmers Co-op. Co. has succeeded the Earlville Grange Elvtr. Co.

Rockford, Ill.—Will build no elvtr. at Perryville in spite of all reports.—Joseph F. Froehlich.

Princeton, Ill.—L. F. Spaulding has bot the elvtrs. of W. C. Brokaw at Walnut and at this station.

Smithshire, Ill.—We succeeded Shontz-Davis & Co. last summer.—Davis Hyde & Co., M. B. Hyde, active mgr.

Springfield, Ill.—E. S. McDonough is now at the office of James E. Bennett here, having transferred from the Jacksonville office of the same firm.

Lindenwood, Ill.—The recently organized Farmers Elvtr. Co. may buy the elvtr. here of the Holcomb-Dutton Lbr. Co.

New Berlin, Ill.—The Farmers Elvtr. Co. is installing 2 motors in its elvtr. The plant will also be lighted by electricity.

Belleville, Ill.—The Belleville Co-operative Farmers Co. expects to put up a new elvtr. later on.—Schlinger Grain Co.

Peoria, Ill.—T. A. Grier recently sold a membership in the Board of Trade which he bot for \$1,000 before Jan. 1, for \$1,840.

Stockton, Ill.—R. L. Coombers has sold his elvtr. to the Farmers Co-operative Co. Possession given April 1.—W. G. McPeck.

Belvidere, Ill.—Farmers Co-op. Elvtr. Co. incorporated; capital stock \$50,000; incorporators A. Tengman, F. M. Davis and E. H. Barney.

Cairo, Ill.—Earl M. Allen, M. L. Falconer, H. E. Halliday, Jr., and W. E. Kreitner have been admitted to membership in the Board of Trade.

Virden, Ill.—We will erect a 60x40 ft. addition to the elvtr. and install a 10-ton scale when the addition is completed.—J. W. Bradway, mgr., Virden Grain Co.

Clarion (Mendota p. o.), Ill.—Charles E. Gallagher has bot the grain and coal interests of Brokaw & Spaulding and will retain John Schneider as mgr.

Worden, Ill.—We will not build an elvtr. now as we have bot the elvtr. of A. J. Keiser. May make some repairs in the spring.—F. W. Stoecker, mgr., Farmers Elvtr. Co.

Lincoln, Ill.—We have bot the elvtr. and the coal and lumber yards of Larnitz & Ploog. We do not expect to make any changes.—F. M. Lockwood, mgr., Farmers Co-op. Elvtr. Co.

Denver, Ill.—The Farmers Co-op. Elvtr. Co. at its last annual meeting decided that it would not build an elvtr., at least this year, on account of the high cost of material and labor.

West Point, Ill.—Mr. Carlisle, of Hamilton, will be mgr. for the Farmers Co-op. Elvtr. Co. at this point. It was formerly the Major Elvtr. and Mr. Major has been in charge since Mar. 1.

Plainfield, Ill.—The Plainfield Grain Co. has accepted plans drawn by Miller, Holbrook & Warren for a new concrete elvtr. When the new house is completed the old wooden houses will be torn down.

Lostant, Ill.—Henry Linder, brother of the operator of the elvtr. of Milla, has bot the elvtr. of C. N. Strong, who will leave for Earlville to engage in the same line of business with his brother, R. A. Strong.

Streator, Ill.—We are not operating an elvtr. at this station but are conducting a brokerage and track buying grain business. We buy for all the leading terminal markets.—H. A. Stotler, pres.-treas., Stotler Grain Co.

East St. Louis, Ill.—The new elvtr. of the Kehlor Flour Mills Co. has a capacity of 500,000 bus. and a handling capacity of 1,500,000 bus. It was built by the R. C. Stone Engineering Co. and will soon be completed.

Cerro Gordo, Ill.—The recently reincorporated Farmers Elvtr. Co. now has an up-to-date elvtr. equipped with a Trapp Truck Dump and new driveway, pits and dumps. The cost was \$18,000. The Decatur Constr. Co. had the contract.

Arthur, Ill.—Dr. C. Phillips has bot the elvtr. of Dr. T. G. Wells, known as the West Side Elvtr. Dr. Wells bot it from C. E. Davis. Dr. Phillips will continue the practice of his profession and will retain Chas. Hamilton as active mgr.

Opheim, Ill.—We will build a fireproof office soon to replace the old one burned in January. We saved our books but some merchandise and the scale were partly damaged. We may also build a new elvtr.—R. S. Gustus, mgr., Opheim Grain Co.

Alexander, Ill.—Our firm, when operating at this station as Greenleaf & Obern, sold our elvtr. to F. B. Six Grain Co. last June and was sold again Mar. 20 to the Alexander Farmers Co-operative Grain Co.—M. E. Greenleaf, Jacksonville, Ill.

Peoria, Ill.—The Rothschild Grain Co., of Chicago and Omaha, as well as Atlantic, Ia., has added this city to its list to care for its growing business. Joseph E. Tigue, mgr. of the company here, has been admitted to membership in the Peoria Board of Trade, and already is enjoying a good business in the new office recently opened, which affords an added facility to its many friends and customers.



Chenoa, Ill.—Farmers Elvtr. Co. has very nearly completed its organization and has under consideration the offer of Ballard, Boughton & Harlan for the elvtr. here and the one at Ballard for \$45,000. The C. W. Perry Grain Co. has offered its 2 elvtrs. here for \$25,000.

Armington, Ill.—The Hittle Grain Co. has been organized and is already in possession of the elvtr. of J. C. Britt. No new elvtr. is needed. Officers of the company are: J. C. Britt, pres.; D. A. Medbery, vice-pres.; O. R. Adams, sec'y-treas., and myself mgr.—W. B. Rossman, mgr., Hittle Grain Co.

Fairland, Ill.—The Farmers Elvtr. Co. has let contract to the Decatur Constr. Co. for additional storage and bin rooms, new foundations, galvanized siding and roofing, and new equipment including sheller, cleaner, transmission machinery and a new 10-ton wagon scale. The machinery will cost about \$18,000.

Timewell, Ill.—We may build an elvtr. of from 30,000 to 35,000 bus. capacity, concrete or vitrified tile, and with 2 dumps and 2 legs. We will also build a large warehouse. Our officers are: James O'Connor, pres.; Robt. Shields, vice-pres., and Joe Bordenkircher, treas.—H. K. Patterson, Farmers Co-operative Co.

Tabor, Ill.—The old elvtr. of the Tabor Co-operative Grain Co. was sold at auction, Mar. 6. It was the first elvtr. in the town and was more or less of a fire trap. The company has a new 65,000-bu. house and so decided to sell it for old lumber. A farmer bid \$125 for it and won. He will wreck it and haul it away.

East Fort Madison (Fort Madison, Ia., p. o.), Ill.—The Farmers Elvtr. & Supply Co. reported to be contemplating building at Niota, will build here instead, and will put up a 20,000-bu. concrete elvtr. equipped with automatic scale, grain cleaner, corn sheller and feed mill as well as concrete coal bins.—R. E. Bertschie, mgr., Farmers Elvtr. & Supply Co., Niota.

Blue Mound, Ill.—The Farmers Elvtr. Co. has just completed its new 30,000-bu. elvtr. It is equipped with new Fairbanks-Morse Engine, sheller, cleaner, transmission machinery, 2 stands of elvtrs., a 10-ton Howe Sonander scale, etc. It was also resided and roofed. The estimated cost was \$10,000. The Decatur Constr. Co. had the contract for the work.

Decatur, Ill.—W. H. Suffern, for so many years in the grain business here and known to the grain trade at large, has decided to take a rest for a year or two anyway. He has many farms and like interests to keep him busy but we shall no doubt hear of him again in the old trade lines. Mr. Suffern is looked upon as one of the best posted men in grain rates and grain business intricacies in the middle west and to him more than to any other one man or dozen men should go the credit of securing to Decatur that advantage in grain rates which puts this city on a par with other large grain shipping centers such as Indianapolis, Cairo and St. Louis. For many years Mr. Suffern fought the fight and only last year was it really won with the result that Decatur is already taking a new position of strategic strength in the handling of grain. During the interim he has turned his entire elvtr. interest over to the Dewein-Hamman Grain Co. which is settled in his office and using his office force.

## CHICAGO NOTES.

F. M. Rogers has become associated with J. S. Bache & Co. and will help in the development of the grain department of the company. Geo. W. Beaven is mgr. of the department.

Mrs. Mary S. D. Updike, widow of Furman Updike, who was well known in grain circles in the old days, died Mar. 8. She had been ill for about 3 months, her death being due to cancer.

A special car at 7:10 p. m., Mar. 31, over the C. & N.-W. Overland Limited to the Western Grain Dealers meeting at Des Moines is being arranged for by J. W. Radford, to carry Board members and their friends.

S. E. Squires, former manager of the Terminal Elvtr. at Des Moines and for the last seven years with the Sawers Grain Co. of Chicago, has given up the grain business and taken up farming near Ashland, Wis., where he intends to grow clover and to raise sheep.

John W. Dickson has withdrawn from the firm of Lederer & Dickson Co. and has opened an office in his own name. N. Lederer has formed a partnership with W. A. Walters and will carry on a general grain receiving and shipping business under the name of Lederer & Walters.

The following have been elected to membership in the Board of Trade: Geo. Ferguson, T. M. Owsley and W. A. Herr. Applicants for membership were J. H. O'Connor, J. G. Steuer and G. F. Powell. D. E. Stott has posted his membership for transfer.

W. H. and E. L. Merritt, of the Merritt Grain Co., have been suspended from the privileges of the Board of Trade for 30 days for undertaking to apply lower grades on contracts. The facts are stated elsewhere in the Journal under the head "Tender of Lower Grades on Contracts."

J. F. Pritchard, veteran grain expert, is dead. He was apparently in the best of health when he retired, Mar. 23, but did not answer when called in the morning. Investigation showed that he died in his sleep. Mr. Pritchard has been a reporter of the Chicago grain markets for over 25 years for the Examiner and the American and other papers. He was vice-pres. of the Modern Miller Publishing Co. He was known by every man and boy in the exchange and the older traders will miss him sadly. He was 67 and leaves a widow.

Fire starting in the cleaning machinery at night, Mar. 12, did \$3,000 to \$4,000 damage to the machines, floor and spouting in the cupola of the "Concrete Elvtr." at Kenton Av. and Cortland St., formerly operated by the Northwestern Malt & Grain Co. and now by the Hales & Edwards Co. Responding to a call the city fire department threw water that did more damage than the fire. The house is full of wheat, barley and malt, and water flowed into several bins, so that the total loss by fire and smoke can not be ascertained until the grain is run out. Insured. The plant was not put out of commission by the fire.

## INDIANA

Veedersburg, Ind.—A large elvtr. is being built here.

Geneva, Ind.—The Geneva Elvtr. Co. will repair its elvtr.

Galveston, Ind.—Farmers are making an effort to build an elvtr.

Crumstown, Ind.—Farmers are making an effort to build an elvtr. here.

Butlerville, Ind.—A company is being formed here to build a 25,000-bu. elvtr.

M'Coy (Greensburg p. o.), Ind.—Farmers have formed an elvtr. company in this vicinity.

Boswell, Ind.—The Boswell Grain Co. is tearing down its old elvtr. and building coal bins.

New Haven, Ind.—The recently organized Farmers Equity Ass'n will build an elvtr. here.

Kendallville, Ind.—The Nathan Grain Co., of Ft. Wayne, has leased the elvtr. of J. Keller & Co.

Evansville, Ind.—The Akin Erskine Milling Co. has doubled its capital stock, it now being \$500,000.

Columbia City, Ind.—Eight concrete tanks will be erected by James Portman, in the old brewery building.

Cullom, Ind.—We have sold our interests at Cullom.—Harold R. Meents, M. R. Meents & Sons, Clifton, Ill.

Uniondale, Ind.—Geo. Lish has succeeded W. P. Scott as mgr. for the Equity Exchange.—Miller & Brickley.

West Baden, Ind.—Logan & Bryan have opened their branch office here under the management of J. J. McLean.

Elwood, Ind.—The Jay Grain Co. incorporated; capital stock \$60,000; incorporators D. W. Jay, A. S. and C. W. Miller.

Ockley, Ind.—Haslett & Son has succeeded Mattix & Smith. The new firm is composed of Wm. Haslett and son Lutie.

Avilla, Ind.—Stiefel & Levy have installed a new motor, a new Sprout Waldron Attrition Mill, No. 20, and a New Process Corn Sheller.

Peabody, Ind.—Meetings are being conducted in this part of the county for the formation of a co-op. farmers' elvtr. company to build an elvtr.

Coburg (Alida p. o.), Ind.—The Farmers Elvtr. Co. has bot the elvtr. of Rogers & Link at this point.—C. H. Johnston & Sons, Pinola (La Porte p. o.).

Hartford City, Ind.—Much interest is being manifested among the farmers around here in regard to organizing an elvtr. company and building a house.

Shadeland (LaFayette p. o.), Ind.—Shadeland Co-operative Elvtr. Co. incorporated; capital stock, \$40,000; H. G. Lutz, James Crowe and W. W. Beed, incorporators.

Nappanee, Ind.—Glen Stauffer has succeeded Clay Syler as mgr. of the Nappanee Grain & Merc. Co. Mr. Syler recently bot an elvtr. and resigned to take charge of it.

Tocsin, Ind.—Willis White, former owner of the elvtr. now belonging to the Farmers Co-op. Elvtr. Co., has succeeded Oscar Way as mgr.—Miller & Brickley, Uniondale.

Thorntown, Ind.—The Thorntown Grain Co. has bot the elvtr. of the R. S. Stall & Co. for \$20,000 and will take possession Apr. 1. Nat Claybaugh negotiated the deal.

Nappanee, Ind.—The recently organized Nappanee Milling Co. has bot the elvtr. of Coppes Bros. & Zook, owners of the property. It is really merely a change in names.

Trafalgar, Ind.—The Farmers Elvtr. Co. has been fully organized with a capital stock of \$50,000 and has taken over the elvtr. of the Trafalgar Grain Co., formerly owned by Wm. Suckow & Co., of Franklin.

Lapel, Ind.—Wm. B. Fisher, of Wolcott, recently bot the elvtr., grain and lumber business of Woodward Bros. for approximately \$40,000. Woodward Bros. have been in business here and at Anderson for over 40 years. They have sold only the property here.

Indianapolis, Ind.—The recently incorporated Midwest Elvtr. Co. has bot the new elvtr. and all property of the Springer-McComas Co. at an understood price of \$100,000. W. R. Evans is pres. of the company; G. W. Evans, sec'y-treas. Other incorporators are Fred R. Hathaway, John A. McComas and Wm. D. Springer.

Ft. Wayne, Ind.—Maurice Niezer was married recently to Miss Emily Kettler and the happy two are spending the honeymoon in Honolulu. Mr. Niezer, an ex-pres. of the National Hay Ass'n, served during the war as Government Hay Inspector for the army camps. Miss Kettler, whom he had known in childhood days, was a nurse in the same camp.

Gadsden, Ind.—We are just starting a substantial remodeling of our elvtr. We are going to put in a new Howe 10-ton Wagon Scale, Huntley Combined No. 6 Warehouse Separator, new drive, two new legs, new boots, and are also building a commodious office and new flour room. Ballinger & McAllister have the contract.—Goodrich Bros. Hay & Grain Co., Winchester.

Francesville, Ind.—In February, 1919, the Farmers Elvtr. Co. at this place went into receiver's hands. The receiver paid all debts and had a few hundred dollars left, but the stockholders lost all their stock investment. Desiring to learn the cause of the company going into bankruptcy the stockholders had a grand jury investigation and indictments were returned against Manager Melvin Maxwell and Pres. Albert Gudeman for forgery, trial to be in the April term of court. The receiver sold the plant to 8 farmers who organized what is now the Co-operative Elvtr. Co., with \$25,000 capital stock. This company has nothing to do with the Farmers Elvtr. Co., only that the plant is the same. I have been manager since May, 1919, and we have been doing a good business. It seems to me the loss of \$20,000 in five years by the old company was due to mismanagement.—G. T. Morrow.

Ft. Wayne, Ind.—Morris Rothgeb, an employe and the son of John Rothgeb, was caught in the machinery of grain elvtr. belonging to the firm of Stiefel & Levy, headquarters at Ft. Wayne, Ind. There is a clutch under the first floor and a lever to the first floor to operate the same, but it seems that the father had been in the habit of throwing this from the basement, so the son was doing the same, and his sleeve caught on a set screw and he was whipped around the shaft for about five minutes or better. A young friend of his was in the basement at the time of the accident and he tried to shut the engine down, but could not as he did not understand how. He then ran to the first floor and tried to throw the clutch out, but it was bound so tight with Rothgeb's clothing it would not work. He got another young man from the outside to shut the engine down. This happened on the 4th of March and the young man died on the 6th. They say the young man was conscious all the time; no one understands how he escaped alive, being whipped around for so long. His father is manager for Stiefel & Levy.



New Paris, Ind.—Plans have been made by the Farmers Equity Exchange for a modern up-to-date fireproof elvtr. The exchange has already bot the warehouse of James Barnett and will be capitalized at \$50,000.

Roachdale, Ind.—We are going to do some repairing, putting in a 10-ton scale and some kind of motor to pull our machinery. The scale we have on hand is Fairbanks-Morse but the motor we have not bot.—Albert C. Lockridge.

## IOWA

Gilmore, Ia.—Farmers are organizing a co-op. company.

Shannon City, Ia.—G. E. Prentis is now mgr. for the Farmers Co-op. Co. here.

Essex, Ia.—The Farmers Elvtr. Co. will increase its capital stock to \$50,000.

Dés Moines, Ia.—F. H. Hall was elected to membership in the Board of Trade recently.

Judd, Ia.—H. F. Addema, of Manteno, is reported to be moving here to operate an elvtr.

Carroll, Ia.—The Farmers Grain & Lumber Co. has increased its capital stock to \$20,050.

Woodward, Ia.—R. L. Friend, of Omaha, is now ass't mgr. of the Stokley Lumber & Grain Co. here.

Beloit, Ia.—I still have an elvtr. here but did not operate it this year.—J. A. Carpenter, Canton, S. D.

Irrington, Ia.—A. T. Montgomery, for a time mgr. of the Farmers Co-op. Ass'n, has settled at Cherokee.

Fremont, Ia.—The Fremont Co-operative Ass'n will be organized here with a capital stock of \$75,000.

Riverton, Ia.—Farmers are organizing an elvtr. company and will soon be in a position to begin operations.

Elliott, Ia.—The new firm of Kinney & Hillhouse have taken over the elvtr. recently bot from D. C. Reynolds.

Mediapolis, Ia.—L. G. Giner has been transferred from Rome, Ia., to succeed Mgr. L. A. Lutes, by the A. D. Hayes Co.

Van Cleve, Ia.—I am now located at this point.—Dan Glidewell, former mgr. for Le Grand Elvtr. Co., Le Grand, Ia.

Archer, Ia.—The Co-op. Grain Co. has let contract for the 30,000-bu., 4-tank concrete elvtr., work to be started in July.

Sioux City, Ia.—The Grain Belt Co. has again announced that work will be started on its new cereal plant with its 250,000-bu. tank storage.

Dayton, Ia.—R. A. Friend has succeeded S. F. Carlson as mgr. for the Farmers Elvtr. Co. Mr. Carlson is mgr. of his own elvtr. at Lehigh.

Larrabee, Ia.—We operate 2 elvtrs. at this point and I am mgr.—Louis Wissenburg, Farmers Elvtr. Co., former elvtr. agt. at Kramer, Neb.

Jefferson, Ia.—Albin Wahlin, employed by the Armour Grain Co. at this point, and Miss Hope Huffman, of Ames, were quietly married recently.

Clarion, Ia.—W. W. Burt, of Galt, has bot the elvtr. of E. J. Funk and will operate as the Burt Grain Co. Possession will be given July 1.

Wesley, Ia.—We are planning to build a new warehouse and office this summer. I am now mgr. here.—Kyle Melick, mgr., Farmers Co-op. Society.

Macedonia, Ia.—We have sold our elvtr. to the recently incorporated Macedonia Grain & Lbr. Co. F. D. Smith is the mgr.—Mickelwait & Young Co.

Ware, Ia.—The Quaker Oats Co. will add 50,000 bu. storage to its plant. It will be of wood construction. D. F. Hoag & Co. have the contract.

Albert City, Ia.—The Farmers Elvtr. Co. is remodeling its elvtr. and will install a 7½-h.p. motor, a 22-ft. Fairbanks Scale and Globe Combination Dump.

Kingsley, Ia.—The Farmers Elvtr. Co. is building a 25,000-bu. concrete elvtr. D. F. Hoag & Co. has the contract and expect to complete it about May 1st.

Strawberry Point, Ia.—We have been in business for the last 5 years but have only recently started in the feed and grain trade. We built a \$20,000 house last year but doubt if we do any building this year.—C. D. Wolcott, sec'y and mgr. of Strawberry Point Co-op. Com. Co.

Batavia, Ia.—We bot the elvtr. here and are operating it. Will make no changes at present.—Farmers Elvtr. Co. (The elvtr. belonged to Walker & Freebler.)

Toledo, Ia.—J. B. Maricle has resigned as mgr. for the Farmers Elvtr. Co. and will leave for Roland, where he bot the elvtr. of Ben Swenson last summer.

Lamont, Ia.—The Commission Co. will build a 20,000-bu. elvtr. and has also bot the building it has been using as a warehouse. Ansy Stone is mgr. of the lumber dept.

Albia, Ia.—The Wilkins Grain Co. has bot a valuable tract of ground here and it is reported will put up buildings and a warehouse for its grain, seed and feed business.

Bradgate, Ia.—Farmers have organized an elvtr. company and will soon be in the grain business. The capital stock will be \$35,000. It will either build or buy an elvtr.

Galbraith (Luverne p. o.), Ia.—Chas. Sankey, mgr. of the Kunz Grain Co., is recovering from a serious attack of blood poisoning due to the neglect of a slight cut on his finger.

Sheldon, Ia.—There is a move to build a new elvtr. here but on account of present conditions we do not know what will be done as yet.—H. Huibregtse, mgr., Farmers Co-op. Ass'n.

Spencer, Ia.—The houses in this town are so scarce that Mgr. H. A. Wilson of the elvtr. of the Wilson Grain Co. will have to live in Langdon and ride back and forth to Spencer.

Pocahontas, Ia.—The Farmers Grain & Coal Co. will remodel its elvtr. New conveyors, spouting and a Trapp Auto Dump will be installed. D. F. Hoag & Co. have the contract.

Luther, Ia.—We have reorganized under the same name. We have bot the elvtr. and lumber business of the Armour Grain Co. and will transfer June 1.—C. H. Patterson, mgr., Farmers Co-op. Elvtr. Co.

Manson, Ia.—The Farmers Elvtr. Co. has applied for a site on I. C. right of way and if it is granted, the company will build a new elvtr. at once. It has a house on the R. I., but claims poor car service renders it useless.

Des Moines, Ia.—The Blair Elvtr. Co. will open an office here very soon. The company recently bot a membership in the Board of Trade at this market. The office is in the Hubbell bldg. and Carl W. Yount is mgr.

Sheldon, Ia.—We have not been able to decide about building yet. We did think of tearing down the old elvtr. and building a new one, but we have not settled as to just what we are going to build.—Farmers Co-op. Ass'n.

Atlantic, Ia.—Frank Lenior, of the Rothchild Grain Co.'s office here, was married Mar. 11 to Miss Thelma Hertage, one of the stenographers of the firm in the Sioux City office, where Mr. Lenior was formerly stationed.

Clemons, Ia.—We built a new 50x60 ft., 2-story warehouse with basement built over brick and backed with hollow tile, equipped with elvtr., electricity and furnace. It cost us \$9,000.—L. H. Ambrecht, mgr., Farmers Co-op. Co.

Titonka, Ia.—The Quaker Oats Co. has let contract to D. F. Hoag & Co. for the remodeling of its elvtr., putting in a new foundation and adding 20 ft. of cribbing. A new leg, manlift and Richardson Automatic Scale will be installed.

Pomeroy, Ia.—I shall build an elvtr. just as soon as I can get the materials and weather conditions improve. I was out of the grain business here for 11 years but have decided to go back into it. Quit in 1906.—Malcolm Peterson.

Titonka, Ia.—The Quaker Oats Co. has let contract to D. F. Hoag & Co. for the remodeling of its elvtr., putting in a new foundation and adding 20 ft. of cribbing. A new leg, manlift and Richardson Automatic Scale will be installed.

Midland (Rock Rapids p. o.), Ia.—Farmers Elvtr. Co. incorporated; capital stock, \$50,000; Theo. Sindt, pres., incorporator. We will build or buy an elvtr. at this station and will equip it with a corn drier. The farmers in the vicinity of Rock Rapids are also organizing and will build a house too.—E. B. Brommes, sec'y-treas.

Odebolt, Ia.—The elvtr. of Reuckheim Bros. & Eckstein, which they recently bot from A. C. Petersmeyer, is being remodeled and put into perfect condition. A 20 h.p. Fairbanks-Morse Engine, corn sheller, cleaner, new rope drive and Globe Combination Dump are being installed. The Birchard Constr. Co. is doing the work.

Solon, Ia.—The big elvtr. of J. J. Fiala burned Mar. 8 with a loss of \$25,000. It is believed that the blaze started in the engine room, spreading from there to the rest of the building. The distance from the water mains made it impossible for the Solon fire department to fight the blaze effectively. Mr. Fiala estimates his loss on the elvtr. (which was 70x40 ft. and 50 ft. high) at about \$15,000, and on the 9,000 bus. of oats, corn, wheat and rye, and high class machinery at \$10,000. This \$25,000 loss is lessened by only about \$6,000 insurance. Mr. Fiala's hands were burned a little in his futile fight against the fire. He bot the elvtr. about 3 years ago and believes that his total insurance is only about \$6,000. However, he hopes to be able to rebuild in the near future.

## KANSAS

Wellsville, Kan.—The elvtr. of the Star Grain & Lbr. Co. will be repaired.

Denison, Kan.—J. L. Turner is now mgr. of the Farmers Union Elvtr. Co. here.

Larned, Kan.—The Larned Milling Co. will put new siding and roof on its plant.

Hamilton, Kans.—Urich Bros. have sold their elvtr. to the Farmers Union Co-op. Ass'n.

Garland, Kan.—The Associated Mill & Elvtr. Co. is reported to be planning on an elvtr. here.

Susank (Hoisington p. o.), Kan.—Peter Rothe is now mgr. for the Farmers Union Co-op. Grain & Supply Co.

Lenora, Kan.—I am not mgr. of the Baker-Crowell Grain Co. at this station at present.—W. L. Turner.

Salina, Kan.—The new elvtr. and seed warehouse of L. O. Baber will be completed and opened by April 1.

Plymouth, Kan.—The Boston and Plymouth Granges are reported to be planning a 20,000-bu. elvtr. at this point.

Gem, Kans.—The Gem Farmers Co-op. Elvtr. Co. is building a concrete power house and installing a new engine.

Flains, Kan.—R. C. Moore is no longer mgr. for the Equity Exchange, but is now in business on his own account.

Lyons, Kan.—C. C. Schumacher is now active in the management of the Lyons Milling Co. M. B. McNair will be the direct mgr. here.

Garfield, Kan.—I have bot the elvtr. of the Gano Grain Co. and will be in charge there April 1.—W. S. Prather, Prather Grain Co.

Abilene, Kan.—C. E. Chase is no longer mgr. for the Farmers Co-op. Elvtr. Co. but has accepted a similar position at Herrington, Kan.

Salina, Kan.—J. R. Miller is now ass't in the grain dept. of the H. D. Lee Flour Mills. He was recently with Dilts & Morgan, Kansas City, Mo.

Lyndon, Kan.—We have succeeded the Lyndon Grain Co. and have been operating the plant since Jan. 1.—W. A. Schroeder, Schroeder Bros. Grain Co.

Burlington, Kan.—A permit for a switch track from the M. K. T. Ry. to the Farmers Co-op. Elvtr. & Merc. Co. and Lathrom & Sons has been granted.

Atchison, Kan.—The following have applied for membership in the Board of Trade: J. R. Schmidt and J. M. Paul. There are only 11 memberships left in the exchange at present.

Junction City, Kan.—The new addition which we recently built is of frame, covered with stucco, instead of being concrete like our elvtr.—S. Bucknell, mgr. Garey County Farmers Union Co-op. Exchange.

Hutchinson, Kan.—The L. H. Pettit Grain Co. in East Hutchinson is putting in a receiving and shipping leg, new wagon dump and arranging to dump grain from cars. The W. H. Wenholz Constr. Co. will do the work and generally overhaul and repair the plant.

Wichita, Kan.—The long desired Merchants Exchange has been organized and a charter granted. The membership is 50 to start with. The present trading and sample rooms of the Board of Trade in the Sedgwick Bldg. will be occupied by the new exchange as soon as the B. of T. lease runs out. The Executive Board at present is composed of the following: P. P. Miller, A. L. Griswold, R. L. Dunmire, E. M. Monson, E. Gorvin, D. M. Flickinger, W. W. Weiss, with C. B. Gaunt, Tem. Chairman, G. M. Snavely, Tem. Sec'y, and S. B. McClaren, Tem. Treas.



Ellsworth, Kan.—The dismantling of the mill recently bot by the H. D. Lee Flour Mills Co., Inc., from the Ellsworth Mill & Elvtr. Co., has commenced and the new plant will go up rapidly. The elvtrs. will not be taken over till July 1.

Wichita, Kan.—The recently incorporated Cameron Grain Co. has opened offices in the Board of Trade Bldg. with W. A. Teter as mgr. Mr. Teter is associated with Bouquot & Ludwig. The company will operate a line of elvtrs.

Saxman, Kan.—O. A. Clark, of Lyons, has bot the interest of E. S. Leonard and will be associated with G. A. Leonard in the Leonard Mill & Elvtr. Co. The latter will be sec'y-treas. of the company and grain buyer, while Mr. Clark will be mgr.

Hutchinson, Kan.—The Security Elvtr. Co. is adding 16 concrete tanks, 15x100 ft. with 17 interspaces and pocket bins to its elvtr. This will give the company, including the head or working house, nearly 500,000 bus. capacity divided into 64 bins, 31 in the work house completed this fall and 33 in the addition to be built. There seems to be a call for great capacity on small lots and this house will be a good example. It will cost close to \$200,000. All machines will be run by separate motors.

## KENTUCKY

### LOUISVILLE LETTER.

New motors have been installed in the elvtrs. of the Kentucky Public Elvtr. Co. and also in those of Ballard & Ballard.

Louisville, Ky.—Garnett Zorn is now in full control of the business of S. Zorn & Co., Thos. G. Williams retiring April 1.

Louisville, Ky.—Andrew Edinger, for many years prominent in the grain and flour circles, operating as the Edinger Co. at this market, died Mar. 17 from the effects of acute indigestion, at Sarasota, Fla., where he was spending the winter. He started in the business in 1878 and when he retired in 1914 he turned the business over to his sons.

## LOUISIANA

New Orleans, La.—Robt. M. McMillan, who has been in the grain brokerage business for the last 18 years, died Mar. 5, at the age of 77. He was one of the 2 honorary members of the Board of Trade and was deeply honored and revered by all who knew him. He is mourned by many friends in the grain trade as he was for many years chief grain inspector.

## MARYLAND

### BALTIMORE LETTER.

The directors of the Chamber of Commerce called a general meeting of the members yesterday to vote upon various changes in the by-laws.

Baltimore, Md.—The interests of the Western Maryland and Baltimore & Ohio elvtrs. should be pooled, if the seriously considered plans of the 2 roads are matured. The plans would involve the enlargement of the Port Covington Elvtr. and the closing for the time of the B. & O. house until the new interests could build a huge elvtr.

Baltimore, Md.—The first car of grain to be placed in the new elvtr. of the Penna. Ry. Co., which was opened for business Mar. 1, was received the next day, consigned to Chas. England & Co. It was a car of No. 2 red winter wheat and was sold to E. Steen & Bro. at \$2.55 a bu. There was considerable friendly rivalry among the firms on 'change for the honor of the first carload.

## MICHIGAN

Conklin, Mich.—The Farmers Shipping Ass'n will buy the elvtr. of E. Z. Albright.

Nashville, Mich.—Farmers are planning to build the first co-op. elvtr. in this county.

Ypsilanti, Mich.—The Amendt Milling Co. is the new name of the Ypsilanti Grain & Elvtr. Co.

Morris, Mich.—Notice of the dissolution of the Mt. Morris Elvtr. Co. has been filed.—Seth V. Johnson.

Chelsea, Mich.—The Chelsea Co-op. Ass'n is considering the advisability of buying an elvtr. and warehouse.

South Lyon, Mich.—I am the successor to C. L. Bailey as prop. of the South Lyon Elvtr.—Walter Marshall.

Niles, Mich.—We are still working on the elvtr. proposition.—E. S. Arney, mgr., St. Joe Valley Shipping Ass'n, Buchannon.

Chelsea, Mich.—The Wm. Bacon Holmes Co. have let contract for an addition to its mills including 4 modern fireproof grain tanks of 42,000 bus. capacity.

Saginaw, Mich.—The State Farmers Buro has called a meeting of about 75 farmers co-op. elvtr. companies in this city Mar. 30, with a view of forming a federation.

Allegan, Mich.—Maurice E. Harvey, who with his brother at one time owned the Constantine Mill, Constantine, has purchased stock in the Allegan Milling Co. and will be sec'y of the company.

Pinconning, Mich.—We will rebuild the elvtr. burned Feb. 26 and will install new machinery thruout. We operated the plant as a branch of the Cass City Grain Co., Cass City.—Walter Hess, mgr., Kimball-Martindale Co.

Detroit, Mich.—The entire 6th floor of the Murphy Bldg. has been leased by the Board of Trade as a permanent home. A large trading hall and offices for nearly all of the members are included. Moving day will be about June 1.

Lansing, Mich.—We have succeeded Hankins Bros., of this city and Elsie. Mr. J. J. Wickens, formerly sec'y-treas. of the Wickens Grain Co., sold his interest back to the other stockholders and bot back his interest in Hankins Bros.—Wilkins, Hankins-Wickens Grain Co.

Howard City, Mich.—We have an elvtr. of 20,000-bu. capacity and a 10,000-bu. potato cellar here now.—C. M. Gates, mgr. Howard City Marketing Ass'n. (The current report has been that the above company will build an elvtr.)

Detroit, Mich.—The following officers were elected at the annual meeting of the Board of Trade, held recently: Pres. for the 2d time, Theo. W. Swift; H. C. Carson, 1st vice-pres., and Henry Mok, 2d vice-pres. The directors are: F. Wm. Lichtenberg, Frank T. Caghey, Gerald Hulett, C. M. Martin, Fred W. Blinn, H. B. Simmons, A. S. Dumont and R. C. Huston.

## MINNESOTA

Dennison, Minn.—The Co-op. Elvtr. Co. will install an auto truck scale.

Rochester, Minn.—We have succeeded Leonard Bros.—Leonard-Utten Co., Inc.

Howard Lake, Minn.—Farmers are organizing an elvtr. company at this point.

Clara City, Minn.—The Farmers Elvtr. Co. has let contract for a 30,000-bu. elvtr.

Stillwater, Minn.—The Northland Rye Mills is having its plant repaired and new dumps installed.

Pipestone, Minn.—C. S. Tobias and I. L. Tobias are now in possession of the elvtr. and business of the I. L. Demaray Co., which they bot last fall.

Hallock, Minn.—The Hallock Grain & Supply Co. has let contract to the T. E. Ibberson Co. to remodel its elvtr. and install a Howe Sonander Scale and a new dump.

Sleepy Eye, Minn.—The Kansas Flour Mills Co. has let contract for a 300,000-bu. concrete storage addition to Barnett & Record Co. Work will be started at once.

Bagley, Minn.—The new name of the Coldwater County Co-op. Produce Co. has been changed to the Bagley Co-op. Co. and the capital stock has been raised to \$25,000.

Spooner, Minn.—The Spooner Elvtr. & Trading Co. has been formed and will build an elvtr. here. The corporation has a capital stock of \$50,000. Nels P. Olson is sec'y-treas.

St. Paul, Minn.—E. R. Dibble of the Dibble Grain & Elvtr. Co. is suffering from a slip on the pavement as he went home to dinner. He fell and broke both bones in one leg which will keep him still for some time.

Stewartville, Minn.—Farmers Elevator Co. is adding a 25,000-bu. concrete storage addition to its elvtr. and will install belt conveyors above and below the tanks, each operating on a separate motor. D. F. Hoag & Co. will do the work.

Beltrami, Minn.—The Carl O. Lekve Grain Co. is building a 40,000-bu. iron clad, cribbed elvtr. with 16 bins. It will be equipped with 2 legs, electric drive, using Link-Belt Silent Chain, and cleaner. A Globe Combination Dump on Fairbanks Scale will be installed.

Albert Lea, Minn.—At the recent annual meeting of the Speltz Grain & Coal Co. all of the officers were re-elected. The company has elvtrs. in Lyle, Stacyville, Ia., Clarks Grove, Ellendale, Hope, Conger, Glenville, London, Walters and Albert Lea, Minn.

Rochester, Minn.—It does not look as tho the farmers will engage in the grain business this year altho there has been considerable agitation. They now have a "Jonah" in the form of a co-operative store which I think will keep them interested for a while.—Sheldon & Richardson.

Porter, Minn.—Frank C. Metcalf, on the road for D. R. Wagner, and I. H. Peterson, buyer for the Equity Elvtr. Co. at Dawson, have formed a partnership and bot the elvtr. of G. O. Miller here. They will operate as Metcalf & Peterson with Mr. Peterson as mgr. and with Mr. Metcalf still a "Wagner" knight of the road.

### DULUTH LETTER.

The following memberships have been posted for transfer: E. Stockman to Garfield Meyer; H. J. LaBree to Frank W. Falk, and John H. Ball to R. M. Sellwood of Duluth; J. Olsen to N. R. Olsen, and F. W. Falk to W. L. Grandy, connected with H. S. Jones.

H. J. LaBree, of Becher-LaBree & Co., has been appointed mgr. of the Lamson Bros. & Co.'s office at Minneapolis but he will continue with his own firm. The Duluth mgr. will be Frank W. Falk, while Mr. Becher will be mgr. at the Minneapolis office of the firm.

M. G. Wisted has succeeded J. P. O'Shaughnessy as ass't registrar of the registration dept. of the Board of Trade, the latter having been appointed ass't traveling passenger and freight agt. for the Omaha R. R. Mr. Wisted was formerly floor man for the Van Dusen-Harrington Co. at this market.

### MINNEAPOLIS LETTER.

John L. Ewing, on the road for 8 years for Brown Grain Co., has resigned and is now at Miller, S. D., with the Standard Oil.

N. Smokstad is now representing the Wm. Dalrymple Co. in South Dakota and in this state. He was formerly with Lamb, McGregor & Co.

J. Q. Adams died recently at his son's home in Englewood, N. J., at the age of 83. For many years he was identified with the grain trade in this city and is remembered by the older generation.

More or less of a fight is being made by the big millers and the grain commission and terminal elvtr. firms over the attempt of the latter to secure new regulations which will permit winter wheat to apply on contracts for future delivery.

O. T. Griffiths, for more than 20 years in the grain business, and lately traveling representative for McCabe Bros. Co., has resigned and will be pres. and gen. mgr. of an oil company in Fargo, N. D.

J. B. Becher will take charge of the Minneapolis office of Becher-LaBree & Co., while Frank W. Falk will be mgr. at Duluth. Mr. LaBree has accepted the management of the office of Lamson Bros. & Co. here, but will still be connected with his own firm.

The following memberships have been transferred in the Chamber of Commerce: George J. Reed to Harold C. Whitten, R. S. Stephens to Edwin M. Wilcox, R. M. Johnston to L. L. McCulloch, J. E. Stair to J. Russell Stair and from C. M. Case to L. B. Poppleston. The following requests for transfer of membership have been posted: From A. J. Wagner to Roy H. Wagner, William Hudson to H. J. LaBree.

## MISSOURI

Linn Creek, Mo.—S. W. Croll is completing his elvtr.

Cowgill, Mo.—I am out of the grain business.—O. McNew.

Salem, Mo.—I am the proprietor of the Salem Roller Mills.—E. S. Durham.

Eldon, Mo.—I am now mgr. for the Co-op. Ass'n No. 16.—H. E. Callison.



Lowry City, Mo.—I am now mgr. for the Farmers Grain & Elevtr. Co.—Geo. Bee.

Pickering, Mo.—The elevtr. of Wilbur C. Smith was greatly damaged by a storm Mar. 14.

St. Joseph, Mo.—The Taylor Grain Co. has closed its office here and sold its membership to W. M. Huff.

Junction City, Mo.—Farmers are planning to organize a company on the plan of the Grangers' Warehouse Ass'n.

Hermann, Mo.—An elevtr. and warehouse are among the new buildings the Farmers Shipping Ass'n plans to erect here.

Koshkonong, Mo.—The Kenyon-Swain Com'n Co. has installed a complete elevtr. system and is now prepared to handle all kinds of grain.

Moscow Mills, Mo.—The new Farmers Elevtr. Co. has bot the elevtr. of Fred W. Schulze and will not build as reported. H. W. Humphrey will be mgr.

Bois D'Arc, Mo.—The Bois D'Arc Farm Club has bot the elevtr. of the John F. Meyer & Son Milling Co., paying \$4,000, and will conduct it on a co-op. basis.

Sweet Springs, Mo.—The contract for the new 50,000-bu. reinforced concrete elevtr. of the Sweet Springs Milling Co. has been let to the Monolith Builders, Inc.

Merwin, Mo.—H. B. Owen, banker and grain dealer, has a comparatively new elevator of tile construction. It is a splendid type for country stations, as it is nearly fireproof.—H.

Buffalo, Mo.—The 2 concrete tank elevtrs. I am going to build will not be on any R. R. and will be used only in connection with my mill. It will, however, be fireproof.—James A. Bonner.

Canton, Mo.—The Canton Co-operative Ass'n has bot the elevtr. of J. H. Wiss & Sons here for \$30,000. The Ass'n had bot a site for a new elevtr., but later came to terms with Mr. Wiss.

Silex, Mo.—Jno. J. Dwyer is mgr. of the elevtr. of the Silex Grain Co. Besides possessing rare business judgment, he has that "way" about him that makes friends. His plant is kept in good order.

Montgomery City, Mo.—The Producers Grain Co. has let contract for the building of a reinforced concrete 20,000-bu. elevtr., containing 9 square bins. The R. C. Stone Engineering Co. has the contract.

Wellsville, Mo.—The Wellsville Milling Co. has let contract to the R. C. Stone Engineering Co. for a reinforced concrete elevtr. of 12,000 bus. capacity. A new 50x20 ft. concrete fireproof warehouse, 1 story in height will also be built.

Springfield, Mo.—J. G. Schmitz, auditor of the John F. Meyer & Sons, located here and at St. Louis and other points, states that thru consolidation, the company now has charge of the Camp Springs Milling Co.'s plant and now operates 4 mills and a string of elevtrs.

Jefferson, Mo.—State Warehouse Inspector James T. Bradshaw, of Kansas City, has filed his annual report with Governor Gardner, and the report shows that the last year has left the department in somewhat of a crippled condition. It shows that the total receipts of the St. Louis, Kansas City and St. Joseph grain inspection offices for the year totaled \$220,758.57. Salaries paid totaled \$143,295.74, and the net earnings were \$33,663.09, leaving a total of about \$70,000 for the present year which would pay expenses for about 6 months. In his summary Mr. Bradshaw estimates that about \$78,000 will be necessary to pay the salaries of the St. Louis, Kansas City and St. Joseph offices from August 1 to January 1 of next year and he asks the governor to authorize the department to use sufficient of its net earnings for the two years. This must be done, he urges, for the department cannot retain its employees on credit or promises to pay for the six months, or until the legislature passes a deficiency bill. The present monthly payrolls of the department at the terminal markets are: St. Louis, \$4,806.65; Kansas City, \$5,686.35, and St. Joseph, \$1,162.50. Commissioner salaries, \$375. Total monthly payroll, \$12,027.50. Financially, the Kansas City office shows a net gain of \$36,919.85; the St. Louis office a net loss for 1919 of \$3,725.20, and St. Joseph a net gain of \$5,692.63. During the year state inspection points were established at the following southeast Missouri points: Sikeston, Charleston, Dexter, Oran, Jackson and Malden, and a separate appropriation was made of \$25,000. The total receipts from these points for inspection were for the year \$10,669.34, and the total expense was \$8,372.70.

## KANSAS CITY LETTER.

The Board of Trade has declared that grain weights at Cedar Rapids are unofficial.

A car of No. 4 yellow corn was received by C. V. Fisher Grain Co. from Prague, Neb., which weighed 151,664 pounds and contained 2,708 bus.

Fred L. Bedell, sec'y of the Kemper Grain Co. and a most popular member of the Board of Trade, has been nominated on the "anti-boss" republican ticket for the lower house alderman of the 10th ward. He has been supt. at many of the large elevtrs. here for 16 years.

## ST. LOUIS LETTER.

N. Weinand has applied for membership in the Merchants Exchange on transfer of certificate of J. H. Ware, of Ware & Leland, to him. Mr. Weinand is with the firm.

Geo. S. Siddons, recently general salesman for the Halliday Elevtr. Co., Cairo, Ill., and well known to the grain trade at large, is again gen. wes. agt. of the A. B. & A. Ry. Co. with offices in the Pierce Bldg.

Plans for a slight reconstruction of the Belt Elevtr. for the accommodation of river transportation are under consideration. The Belt Elevtr., erected a number of years ago and controlled by the Mason-Hawpe Grain Co., a Missouri corporation, has been transferred to the Mason-Hawpe Elevtr. Co., a corporation organized and existing under the laws of Illinois. An order permitting this transfer was entered by the Illinois Utilities Commission.

The directors of the Merchants Exchange have authorized the construction of 8 private wire booths on the floor of the exchange. Bert Forster won first honor for J. S. Bache & Co., being the first man to have a private wire office on the floor. His office is No. 3. Others taken are: No. 1, next to the rostrum, Jackson Bros. & Co., Eugene Hunn representative; No. 2, Sam Mincer; No. 3, J. S. Bache & Co.; No. 4, Farnum, Winter & Co.; No. 5, J. Rosenbaum Grain Co.; No. 6, Armour Grain Co.

## MONTANA

Hobson, Mont.—C. Wm. Olson has succeeded C. C. Roberts as mgr. for the McCaull-Webster Elevtr. Co.

Rudyard, Mont.—The elevtr. of the St. Anthony & Dakota Elevtr. Co., containing 800 bus. of oats, 800 bus. of corn and 300 bus. of flax, burned Mar. 3 with a loss of approximately \$50,000. No cause for the blaze is known and a thoro investigation will be made. It is that that the elevtr. will be rebuilt at once.

## NEBRASKA

Broken Bow, Neb.—The elevtr. of F. J. Bahr burned recently.

Arapahoe, Neb.—The elevtr. of the Farmers Equity Exchange burned Mar. 15.

Atlanta, Neb.—The Seldomridge Grain Co. will install a Howe Scale and a truck dump.

Merna, Neb.—A Hall Signaling Distributor will be installed in the Central Granaries Co.'s elevtr.

Laurel, Neb.—We will continue to operate as the Laurel Milling & Grain Co.—J. R. Durrie, prop.

Holdrege, Neb.—The Seldomridge Grain Co. will install a Howe Scale and Globe Truck Dump.

Bertrand, Neb.—The Seldomridge Grain Co. will install a Howe Scale and 2 Globe Truck Dumps.

Palisade, Neb.—The F. C. Krotter Co. corporation is not in business here but Krotter & Ward.—Ward.

Randolph, Neb.—I recently purchased the elevtr. of D. S. Lord and will operate it.—C. L. Graves, Graves Grain Co.

Lehara, Neb.—Ed Fetz has resigned as mgr. of the Farmers Elevtr. Co. and will be succeeded by Mr. Hopple of Fremont.

Fremont, Neb.—Three Hall Signaling Distributors are going to be installed in the elevtrs. of the Nye-Schneider-Fowler Co.

Gladstone, Neb.—The Farmers Union Co. is remodeling its elevtr. and installing a Fairbanks-Morse Engine and Hall Distributor.

Rescue, Neb.—The Farmers Co-operative Elevtr. Co., under the management of Mr. Joe Texel, is very busy now shipping corn.—H.

Johnstown, Neb.—The Nye-Schneider-Fowler Co. will build a concrete elevtr. to take the place of the old wooden one it is now operating.

Trenton, Neb.—The Trenton Equity Exchange is building a 15,000-bu. cribbed iron clad elevtr. The Birchard Constr. Co. has the contract.

Kimball, Neb.—J. V. Casper, formerly mgr. for the Farmers Union Co-op. Co., Prague, is now with the Farmers Union Co-operative Ass'n here.

Foley (David City p. o.), Neb.—Sam Allen, formerly mgr. of the Dawson Grain Co. at Brainard, Neb., is now mgr. for the Farmers Grain Co. here.

Elk Creek, Neb.—I have resigned as mgr. of the Farmers Elevtr. Co. after 24 years in the grain trade. W. T. Waidley has succeeded me.—Ben. Hall.

Firth, Neb.—J. Petz, the mgr. of the elevtr. of the Farmers Co-operative Co., has resigned and will probably go west to engage in the grain business.—H.

Barnston, Neb.—C. C. Storey, former mgr. for the Farmers Elevtr. Co. here, has bot the elevtr. of C. F. Blauer at Oneida, Kan., Mr. Blauer retiring from the grain trade.

Milford, Neb.—We are doing some repairing on our hopper scales and building new coal sheds so that we can handle coal.—J. C. Welch, mgr. Farmers Union Co-op. Ass'n.

Beatrice, Neb.—Howard C. Dobbs, who is engaged with his father in the grain business here as the John Dobbs Grain Co., was recently married to Miss Marie Goodreau of Liberty.

Fullerton, Neb.—I am mgr. for the Farmers Union Co-op. Ass'n. We are building 5,000-bu. addition to our elevtr. and a new office. We will also install a 10-ton Fairbanks-Morse Truck Scale.—S. S. Kissell.

Prague, Neb.—Our company is being incorporated for \$50,000 and we will operate as the Farmers Union Co-op. Co. and will have a store here. J. V. Casper, formerly mgr., has resigned and is now in Kimball, Neb., with the Farmers Union Co-operative Ass'n there. I used to be 2d man here but am mgr. now.—Anton Peterzelka, mgr., Farmers Co-op. Co.

Yanka (Brainard p. o.), Neb.—The Farmers Grain Co. has bot the recently completed \$15,000 cribbed elevtr. of the Dawson Grain Co., of Omaha, at this station and will use it for storage. Sam Allen, formerly mgr. for the Dawson Grain Co. at Brainard, is now in charge of the Farmers Grain Co. of Foley (David City p. o.). Neb.—Frank Chapin, mgr., Farmers Grain Co. Yanka.

Touhy, Neb.—Frank Tejal, mgr. of the Nebraska Elevtr. Co. here, has resigned and will go to Valparaiso as salesman. The elevtr. is closed. A. J. Janovy died recently and I have succeeded him as mgr. of the Farmers Co-op. Grain Co. here. I was formerly mgr. of the elevtr. of the Ulysses Grain Co. at Ulysses. The deal we were considering in regard to the taking over of the D. R. Phelps Lbr. Co. has fallen thru.—R. J. Jelinek, mgr., Farmers Co-op. Grain Co.

Lincoln, Neb.—The new Grain Exchange has elected the following officers: C. G. Crittenden, Pres.; J. S. Ewart, Vice-Pres.; W. S. Whitten, Sec'y-Treas. Directors, C. G. Crittenden, J. S. Ewart, H. E. Gooch, E. N. Mitchell, B. M. Ford. The Board of Directors has named the following standing com'ites: Membership com'ite, L. O. Wilsey, Chairman, M. T. Cummings and C. J. Slater. Arbitration com'ite, John M. Paul, Chairman; Edward Leet and J. M. Hammond. Appeals com'ite, John D. Taylor, Chairman; J. S. Ewart and T. M. Waxman. Transportation com'ite, W. S. Whitten, Chairman (Mgr. Traffic Bureau, Lincoln Commercial Club); L. J. Thurn and H. H. Wintersteen. Discount com'ite, Edward Leet, Chairman; John D. Taylor and C. J. Slater.—W. S. Whitten, sec'y-treas., Lincoln Grain Exchange.

## NEW ENGLAND

Worcester, Mass.—The elevtr. of E. A. Cowee Co. burned about 8 p. m., Mar. 10, the loss and damage amounting to \$175,000. The plant contained 1,600,000 lbs. of grain and feed which is a total loss of \$35,000, the new electrical machinery just installed is damaged to the extent of \$60,000 and warehouse and elevtrs. were held at \$85,000. A crossed wire in a transformer in the elevtr. caused the blaze. The only car of hay in the town was standing near the elevtr. and was partially destroyed.



Bridgeport, Mass.—The National Grain Corporation has succeeded the Susman-Feuer-Brownstein Co., the change being in name only. The company owns a 12,000-bu. elvtr., a 4-story warehouse with 150,000 sq. ft. of space, and a large mill. David Feuer is pres. and gen. mgr. The company has been admitted to membership in the Produce Exchange, New York.

North Adams, Mass.—Plans for the new grain elvtr. and warehouse for the Berkshire Coal & Grain Co. have been completed. The contracts for material and for the wrecking of the old plant have been made. The plans call for a 128x60 ft. warehouse and a 96 ft. elvtr. An office will also be built. The slow movement of freight has caused the delay in starting the work.

## NEW YORK

### BUFFALO LETTER.

Industrial Grain Products incorporated; capital stock, \$200,000; incorporators, E. Widmer and others.

Godfrey Morgan, E. T. Douglass and J. W. Stevenson have been appointed as a com'te to attend the hearing of the legislature on the question of a state elvtr. in the harbor. The com'te was appointed by the grain interests at this market who did not approve of the plan.

Local grain and marine men who are thoroly conversant with the grain and elvtr. service here, are very much opposed to the building of another large harbor elvtr. and will make every effort to prevent it. They say they have elvtrs. enuf and canal and rail needs sufficient to handle what they get.

Eldad Milling Co. suffered \$20,000 loss and damage when its elvtr. was set on fire by a blaze from the drying kiln adjoining the elvtr. The fire is believed to have started from an over heated kiln setting fire to some flaxseed oil and then spreading to the elvtr. proper. One bin of oats containing about 20,000 bus. was badly damaged by water but the building is not in bad condition. It is owned by the Churchill Grain & Seed Co., Leroy S. Churchill, pres.

James W. Parsons, special master in bankruptcy, has recommended that Geo. R. Pierce be adjudicated a bankrupt, in his latest report to Judge Hazel. The decision exonerates the special com'te accused of forcing Mr. Pierce by illegal means to make an assignment of his properties for the benefit of his creditors. Mr. Parsons placed the amount of his indebtedness to be \$300,000. Mr. Pierce in the meantime has entered into a contract with Geo. E. Pierce, Inc., to turn over to them the Evans elvtr. and all grain interests together with all real estate personally owned or connected with the grain elvtrs. as a protection to the preferred stock issue.

### NEW YORK LETTER.

Carl Sommer Larsen is now with Milmine, Bodman & Co., Inc., having left the Chesapeake Export Co. He is at present in Europe.

Edmund N. Giles, Chas. Anhalt, Wm. H. Meeder and F. C. Vincent have been admitted to membership in the Produce Exchange and Edward S. Galloway is an applicant.

Chas. F. Watt is now actively engaged with Sanday & Co. He was identified with the Wheat Export Co. during the war and prior to that was in the grain business with Henry Johnston.

Robert F. Straub has cast his lot with recently organized P. N. Gray & Co. He was formerly connected with the office of the Armour Grain Co. here and has been succeeded there by T. C. O'Brien, former mgr. of the Erie Elvtr. in Jersey City.

## NORTH DAKOTA

Jud, N. D.—The Farmers Elvtr. Co. may do some repairing this spring.—V. W. Steele.

Bisbee, N. D.—We expect to install a loading spout in our elvtr.—Farmers Elvtr. Co.

Stanley, N. D.—G. A. Wenderlin has become a member of our firm.—N. G. N., Nelson Grain Co.

Bordulac, N. D.—The engine room of the elvtr. of the Farmers Elvtr. Co. burned recently.

Heil, N. D.—We have not as yet decided to build another elvtr. to replace the one burned last July. Crops will have to be greatly improved if we do.—W. R. Henly, pres., Heil Equity Exchange.

Wimbledon, N. D.—H. H. Belcher has resigned as buyer for the Osborn-McMillan Elvtr. Co. at this station.

Dwight, N. D.—The Farmers Elvtr. Co. has been planning to increase its capital stock and build another up-to-date elvtr.

Venlo (Anselm p. o.), N. D.—The elvtr. of O. M. Johnson here has been bot by A. E. and Allen Carter and will be operated as A. E. Carter & Co.

Kongsberg, N. D.—We have bot the elvtr. of W. W. Whipple and are now operating it with H. P. Perry as agt.—Cargill Elvtr. Co., Minneapolis, Minn.

Bismarck, N. D.—Bids for the construction of the state elvtr. and mill will be opened Apr. 5 instead of Mar. 15, the N. D. Industrial Commission, W. A. Anderson, sec'y, having postponed the letting to help late bidders. The detailed request for bids was published in this column Mar. 10.

Osnabrock, N. D.—L. H. Smith, who has operated the elvtr. of Wild Bros. & Co. at Easby for the last 5 years, and Otto Rassmussen have bot the elvtr. of A. P. Kezma, known as the Independent Elvtr. here, and the one at Easby, operated by Mr. Smith. The elvtr. here will be raised and given a thoro overhauling and new coal sheds will be built.

Greene, N. D.—Fire was presumably caused by exploding stove in office, as fire was first discovered in that part. It occurred 2/24/20, 6:30 p. m. Elvtr. fully covered. Grain all covered but \$195.00. This constitutes all loss. No action has yet been taken as to what kind of building will be erected in its place. Sentiment is, however, in favor of a 10,000-bu. concrete house.—Mouse River Farmers Co-op. Elvtr. Co., J. Scotland, mgr.

Ayr, N. D.—We completely repaired and overhauled our elvtr. We put in two 20 h.p. Worthington Oil Engines equipped with rotary pumps and Madison Kipp self feeders, two 1,600-bu. Richardson Auto. Scales, whole platform dump scales, two double legs, fast carrying new spouting, window screens on all windows, new pits, new ropes and a new system of counter-shafting. We also painted the house and it cost us \$9,800.—J. M. Montgomery, agt. Farmers Co-op. Co.

Harlow, N. D.—On Mar. 7 D. J. Geary, agt. of the Spaulding Elvtr., built a fire in the stove and after getting it well started went down town without shutting the dampers. In a few minutes the wall caught and it was only a second till the whole office was ablaze. It was Sunday and most of the men at home so that in a few moments the fire was put out and his elvtr. as well as mine, which stand close together, were saved.—Ing. Iverson, agt., O. & M. Elvtr. Co.

## OHIO

Westerville, O.—Farmers are working hard for an elvtr. here.

Waldo, O.—An effort is being made to build a co-op. elvtr.

Ostrander, O.—Farmers are organizing an elvtr. company.

Elmira, O.—We will increase our capital stock to \$30,000.—Elmira Elvtr. Co.

Willshire, O.—The Willshire Grain Co. will install a grain drier, to cost \$10,000.

Haskins, O.—The Farmers Grain Co. is increasing its capital stock to \$50,000.

Lewis Center, O.—Farmers are forming an elvtr. company to buy the elvtr. here.

Hyattsville, O.—A farmers elvtr. company will be organized here to build an elvtr.

Jeromesville, O.—The Plank Milling Co. has succeeded the firm of A. R. Plank & Son.

Homeworth, O.—The Canton Feed & Milling Co. has bot the elvtr. of S. L. Hill at this station.

Urbana, O.—C. E. French has succeeded James C. Robinson as mgr. of the J. I. Blose Grain Co.

Port Washington, O.—The Canton Feed & Milling Co. will build a 20,000-bu. elvtr., starting work next month.

Frazeysburg, O.—Frazeysburg Mill & Elvtr. Co. has bot the elvtr. of McMillan & Co. Hanley Milling Co., Coshocton.

Sharon Center, O.—The recently organized Sharon Center Co-op. Elvtr. Co. has bot the elvtr. of Brown & Haight.

Pulaski (Bryan p. o.), O.—We are planning a new warehouse this spring.—J. C. Kitter, mgr. Pulaski Farmers Elvtr. Co.

Powell, O.—Co-op. elvtrs. and warehouses will be built here if the present movement to organize a company continues.

Elkton, O.—We will make some improvements in our elvtr. here, installing large hopper bin.—Geo. N. McCamon, mgr. C. W. Coffee & Co.

Millersburg, O.—The Farmers Equity Exchange has been organized with Ed Toms as mgr.—Plank Milling Co., Jeromesville.

Defiance, O.—The Farmers Co-operative Co., which has just completed a new house, has bot the 3 elvtrs. of the Farmers Grain Co.

Columbus, O.—The annual meeting of the Ohio State Millers Ass'n will be held at the Deshler Hotel, Thursday and Friday, April 29 and 30.

Middleport, O.—We have opened a grocery store only and are not in the grain business as reported.—F. H. Johnston, sec'y Middleport Co-op.

Foraker, O.—The elvtr. of Peter Bridenbach has been sold to the Farmers Exchange Co. Mr. Bridenbach also represented the Sneath-Cunningham Co. here.

Walhonding, O.—Farmers in this vicinity are organizing a company to build a mill and elvtr. Walhonding is 20 miles west of here.—Hanley Milling Co., Coshocton.

Burbank, O.—The Farmers Equity Co. has bot the elvtr. here, succeeding the Burbank Elvtr. Co. The farmers are operating now.—Plank Milling Co., Jeromeville.

Nevada, O.—The Bucyrus Mill & Elvtr. Co. will add new bins to the elvtr. and put it into first class condition for handling all grains. It is equipped for handling wheat only at present.

Greenville, O.—We have nearly completed the organization of our company but have not looked for a site or that as yet of buying.—H. E. McEowen, sec'y-treas., Farmers co-operative Co.

Clark (New Madison p. o.), O.—We have bot the elvtr. of Jeffries & Harris and are making some improvements. We may also do some repairing and install a motor.—Richards & Hollinger.

Thurston, O.—We have bot a Monitor Cleaner, a sheller, Hall Distributor and the U. S. line of elvtr. machinery. Will start on our new elvtr. April 1st.—H. I. Turner, mgr. Thurston Elvtr. Co.

Glen Karn, O.—We have sold our elvtr. and other business interests to Richards & Simpson, formerly of Crete, Ind., and will give possession April 1.—H. L. Welch, Glen Karn Grain Co., Hollansburg.

Bucyrus, O.—The Bucyrus Mill & Elvtr. Co., which recently bot the mill of Zeigler & Co. here, has bot the elvtr. of the company at Nevada and also the elvtr. of H. D. Miller here and will operate all of them.

Mansfield, O.—F. N. Barton is sec'y and L. C. Chase, vice-pres. and mgr. of our company. We understand that the Shelby Flour Mills Co. will build a new mill, not an elvtr. as reported.—Mansfield Elvtr. Co.

Tiro, O.—We have just completed the installations of an 800-bu. separator and are now installing three 14-ft. elvtrs. and a 22-ft. wagon scale. We will put in a wagon dump in the spring.—J. F. Libby, Tiro Equity Union Exchange Co.

Cleveland, O.—Thomas W. Burnham, pres. of the Star Elvtr. Co., died on Mar. 4. He had been ill for some time and had given up actual management of the plant, which has been for some time managed by Frank W. Blazy. Many members of the Grain & Hay Exchange paid honor to his memory at the funeral Mar. 8.

Genoa, O.—The old firm known as the Powers Elvtr. Co. will continue in the lumber, coal, ice and harvesting machinery business but will not handle grain. We will attend to the grain business under the recently incorporated C. A. Powers Grain Co., with elvtrs. here and at Martin. Our capital stock is \$20,000.—C. A. Powers, pres.

### CINCINNATI LETTER.

Cincinnati, O.—Ralph H. Brown is again with Henry W. Brown & Co., having resigned as U. S. Grain Supervisor.

Cincinnati, O.—J. B. Helwig has "gone back" to his first love and is now representative here for the freight dept. of the M. & St. L. He was for a time with the D. O. Cross Co.



Cincinnati, O.—The following memberships have recently been added to the list of members of the Hay & Grain Exchange: G. C. Beuler to active membership; H. J. Miller to clerk membership to represent Henry M. Brouse; Geo. F. Dieterle to represent The Federal Products Co.; Geo. A. Dieterle to clerk membership to represent Geo. F. Dieterle; Ralph H. Brown to clerk membership to represent Wm. G. Stueve of Henry W. Brown & Co.

## OKLAHOMA

Welch, Okla.—It is rumored that an elvtr. will be built here.

Buffalo, Okla.—The Farmers Elvtr. Co. has been organized here with a capital stock of \$40,000.

Lenapah, Okla.—The Nowata County Co-op. Grain Dealers Ass'n has been organized and L. C. Denton has been elected sec'y-treas.

Garber, Okla.—Farmers Grain & Supply Co. incorporated; capital stock, \$30,000; incorporators, T. A. Snoddy, H. J. Botts and others.

Neodesha, Okla.—Farmers Elvtr. Co., incorporated; capital stock \$10,000; incorporators C. G. Kennedy, D. E. McClure and J. W. Wagoner.

Imo (Enid p. o.), Okla.—Imo Terminal Elvtr. Co. incorporated; capital stock, \$20,000; J. S. Anglin, W. H. Beckman and others, incorporators.

Bliss, Okla.—We have bot the elvtr. of D. J. Donahue and will remodel and rebuild it.—Jno. W. Woolery, sec'y, Bliss Co-operative Grain Co.

Kremlin, Okla.—The Goltry Grain Co. and the Farmers Elvtr. Co. are installing 10 ton Fairbanks-Morse Scales.—G. S. Rhodes, mgr., Farmers Elvtr. Co.

Forgan, Okla.—We have bot Bouquot & Ludwick here and will build a 50-bbl. mill in connection with our elvtr.—Farmers Mill & Elvtr. Co.

Biss, Okla.—The Bliss Co-operative Grain Co. incorporated; capital stock, \$300,000; incorporators, Claiborne Collier, J. E. Carson and J. W. Woolery.

Enid, Okla.—Co-operative Grain & Supply Co. incorporated; capital stock, \$30,000; H. J. Botts, pres.; John Hayes, treas.; and W. A. Treackle, sec'y-treas.

Waukomis, Okla.—Farmers are making an effort to form a company to build a \$400,000 mill and elvtr. Fred Atherton and C. H. Hockler are among those interested.

Muskogee, Okla.—The elvtr. of the Davidson Mill & Elvtr. Co. burned recently with a loss of \$60,000. This is the 3'd time an elvtr. of the company on the same site has burned.

Selman (no p. o.), Okla.—Farmers Co-op. Grain & Supply Ass'n incorporated; capital stock \$25,000; incorporators, Fred Schneider, Clay Johnson and J. S. Gilfillian, Charleston.

Hooker, Okla.—Sallee Bros. bot the Reno Flour Mills Elvtr. last July and sold it to us about Sept. 15 and we have sold the house to the Hooker Elvtr. Co. which operates it now.—C. A. Dennison Grain Co.

Ellis (Breckenridge p. o.), Okla.—The recently organized Grange has made no plans for a new elvtr. that I know of. We are getting ready to enlarge our elvtr. and to install auto truck dumps.—J. W. Marshall, agt. Blackwell Mill & Elvtr. Co.

Cyril, Okla.—Our firm is a co-partnership between T. J. Brooks and E. H. Stewart. We do not operate an elvtr. However, we will construct in the near future, warehouse and storage capacity sufficient to take care of our business. Commenced business November 1st, 1919.—Caddo Grain Company, E. M. Stewart mgr.

Oklahoma City, Okla.—I have been crippled up and practically out of working condition for the past month on account of 4 fractured ribs and a broken wrist, which still makes it impossible for me to sign my own name. This accident has thrown me back with my work in more ways than one. I am getting along nicely, being at the office every day, and will soon get caught up with my correspondence which has been sadly neglected.—C. F. Prouty, sec'y, Oklahoma Grain Dealers Ass'n.

Broken Arrow, Okla.—The elvtrs. of the Bowser, Brown, Bixley Elvtr. Co., which was recently dissolved, have been apportioned as follows: The elvtrs. at Oneta and Catoosa go to J. W. S. Bower, the Bixby Mill to G. A. Brown, and the new elvtr. at Bixby to E. B. Baxter. The houses will all be operated under the names of their respective owners. G. A. Brown and E. B. Baxter will have temporary offices here, while Bower is at Muskogee.—A. B. Baxter.

## OREGON

Pendleton, Ore.—J. E. Montgomery, formerly mgr. for the Pacific Grain Co. will succeed J. A. Rae as agt. for H. W. Collins at Walla Walla, Wash.

Portland, Ore.—Walter L. A. Johnson, one of the most active and best known grain men in this part of the country, has resigned as 2 v. p. of the U. S. Grain Corp. He will now enter the firm of Satter & Johnstone.

## PENNSYLVANIA

Spartansburg, Pa.—I am not at present in the grain business. Isaac D. Obet succeeds me.—W. W. Walford.

Philadelphia, Pa.—C. Herbert Bell, pres. of the Commercial Exchange, called a meeting, Mar. 15, in an effort to get members of the grain trade at this port interested in the building of an elvtr. here. H. J. Horan, chairman of the meeting, said we were not in proper condition to handle grain and much more storage was needed. Efforts were made to co-operate with the city authorities and the railroads for increased elvtr. facilities. George F. Sproule, city director of wharves, docks and ferries, assured the grain and shipping men that his department would do all possible to co-operate.

## SOUTH DAKOTA

Plana, S. D.—J. E. Jones has succeeded G. W. Hussinger as mgr. for the Farmers Elvtr. Co.

Firesteel, S. D.—A. W. Batchellor is now mgr. for the Farmers Elvtr. Co.

Fairfax, S. D.—B. H. Shoenrogge is now mgr. for the Farmers Union at this point.

Doland, S. D.—We contemplate no repairs on our plant as has been reported.—Farmers Elvtr. Co.

Blunt, S. D.—We are out of the grain business, having sold to H. H. Persons.—Garrett & Lepper.

Emery, S. D.—The elvtr. of J. Driscoll & Son is being equipped with a 10-ton Howe Scale and a Globe Dump.

Meckling, S. D.—The Farmers Union Co-op Elvtr. Co. has bot the elvtr. of the McCaull-Webster Elvtr. Co.—Le Roy Dickey, mgr.

Yankton, S. D.—J. P. Kelly, of Sanborn, Ia., is reported to have bot a grain and elvtr. business here which he will take charge of April 1.

Reville, S. D.—We will do some repairing this season, fixing driveway, shingling roof and building an addition to the office.—Farmers Elvtr. Co.

Scotland, S. D.—The Farmers Grain & Stock Co. has let contract to D. F. Hoag & Co. for a 25,000 bu. concrete elvtr. to be complete May 1. E. B. Kast is mgr.

McLaughlin, S. D.—The McLaughlin Equity Exchange will duplicate the elvtr. it built last spring, making a total storage capacity of 80,000 bus. or 40,000 bus. for each house.

Howard, S. D.—The Farmers Elvtr. Co. will build a 40,000-bu. iron clad elvtr. Three legs will be installed and it will be electrically equipped. T. E. Ibberson Co. has the contract.

Tripp, S. D.—A heavily loaded coal car jumped the track about 6 ft. from the elvtr. here and smashed its way thru the corner of the elvtr. The entire corner of the elvtr. had to be taken out and recribbed as far up as it had been damaged.

De Smet, S. D.—The Farmers Co-Op Ass'n has let contract to The Younglove Constr. Co. for a 35,000-bu. re-inforced concrete elvtr. to have 14 bins and 3 legs. Five Fairbanks Motors with silent chain drive (Link Belt) will furnish the power. Richardson 2,200-bu. Automatic Scale, Globe Dump and a 10-ton scale will be installed as well as 10-ton wagon scale outside. The office will be strictly modern and up to date.

## SOUTHEAST

Danville, Va.—The elvtr. of the Westbrook Elvtr. Co. burned recently; loss \$75,000.

Atlanta, Ga.—J. L. Frazier & Co. are planning the erection of a larger warehouse for handling their grain and hay.

Columbia, S. C.—The new warehouses of the Kirkland Distributing Co. Inc. have been completed and are now in operation.

Dublin, Ga.—Farmers Elvtr. has received its first wagon load of corn. Formal opening of the plant will be held about April 1.—J. H. S.

Atlanta, Ga.—The new elvtr. of the Atlanta Milling Co. will be handling grain by Apr. 1. The plant will have a storage capacity of 200,000 bus. in addition to the present bin capacity in the mill.

## TENNESSEE

Newbern, Tenn.—J. W. Trout, of Newbern, Tenn. has bot an interest in Cole Newbern Grain & Milling Co.

## TEXAS

Umbarger, Tex.—The Oklahoma Milling Co. has bot my elvtr.—W. L. Brodie, Hereford.

Lockney, Tex.—The Lockney Grain & Coal Co. has let contract to White Star Co. for a new elvtr.

Mesquite, Tex.—The new plant of the Mesquite Mill & Elvtr. Co. has been completed. M. Bennett is mgr.

Lockney, Tex.—We are figuring on building a \$50,000 grain elvtr. and may also put up a gin.—Farmers Exchange.

Galveston, Tex.—The annual meeting of the Texas Grain Dealers Ass'n will be held in this city May 21 and 22.—R.

Floco (Aitken p. o.), Tex.—We are building a 60,000-bu. addition to our elvtrs. here.—J. E. McAvoy, mgr., Floyd County Elvtr. Co.

Longview, Tex.—H. H. Watson has been reinstated and his license, which was revoked Sept. 13 by the Food Administration, restored.

Fort Worth, Tex.—The Texas Grain Brokerage Co. incorporated; capital stock, \$1,500; W. L. Goodwin, H. G. Johnson and G. C. Henderson, incorporators.

Electra, Tex.—We have just completed a 20x100 ft. feed warehouse and a 20x30 ft. hay house. We also built additions and repaired our coal houses.—Farmers Elvtr. Co.

Floydada, Tex.—The South Plains Grain Co. is building a 30,000-bu. elvtr. at this station. The White Star Co. has the contract and work is progressing nicely.—J. H. Hohlaus.

Stamford, Tex.—Burglars made an attempt to blow open the safe in the office of the Stamford Mill & Elvtr. Co., recently but the blast was premature, frightening the robbers away.

Taylor, Tex.—I am the sole owner and mgr. of the Gottlieb Milling Co. which is successor to the Taylor Milling Co. and the Gottlieb Grain Co.—Gottlieb Milling Co., J. Gottlieb.

McKinney, Tex.—The members of our firm are L. C. Voelkel, who was formerly connected with grain firms at Brownwood, Sherman and McKinney, and B. H. McLain of Dallas, who was recently discharged from the navy and was connected with other lines of work prior to joining the navy.—Voelkel-McLain Co.

Ft. Worth, Tex.—The plant of the Western Grain Co. and the Southern Hay Co. was destroyed by fire, Mar. 8, with a loss of \$12,500 and an insurance of \$1,000. One car of hay and one of corn were badly damaged. No report as to rebuilding has been given out.

Houston, Tex.—The Houston Mill & Elvtr. Co., recently incorporated, will build at once on a 10-acre tract it has just bot. The building will be constructed of reinforced concrete. The contracts for the actual construction of the mill and machinery have already been let. When completed, about August 1, 1920, the mill will have a daily capacity of approximately 1,500 bus. and the elvtr. capacity of 500,000 bus. Further units will be added as needed, according to the present plans. The mill and elvtr. will be connected with the ship channel by a grain conveyor. The company is capitalized for \$600,000, and has been organized for the manufacture of flour and all grain by-products. The officers are R. S. Sterling, pres., F. J. Becker vice pres. and Robert I. Cohen, secy. and treas. Mr Becker will be gen. mgr.



Follett, Tex.—The Farmers Equity Union has bot the elvtr. of L. G. Ewing and will be in possession in a few days.

Floydada, Tex.—We are carlot shippers and tho at present we do not own an elvtr., we are well fixed for handling grain. J. G. Rainer, formerly in the grain business here, quit it some time ago.—S. D. Furguson & Co.

## UTAH

American Forks, Utah.—The officers of this concern are pres., W. H. Chipman; v. pres., M. E. Bezzant; treas. and mgr., J. H. Storrs.—American Fork Co-op. Co.

Brigham City, Utah.—The Brigham City Roller Mills Co. and the Jensen Bros. Mfg. & Elvtr. Co. contemplate a consolidation. The question will come up before the stockholders for ratification or rejection in the near future.

## WASHINGTON

Olympia, Wash.—Brewer & Co. incorporated with a capital stock of \$15,000.

Toppenish, Wash.—Renz Bros. & Co. have increased the capital stock to \$250,000.

Colfax, Wash.—The Colfax Milling Co. has let contract for a 300,000-bu. re-inforced concrete elvtr.

Aberdeen, Wash.—Wishkah Grange is planning to build a warehouse for storage of grain and feed to be purchased through the state grange system.

Seattle, Wash.—Work has been started on the new plant of the Centennial Mill Co. The elvtr. adjoining will be made up of 23 concrete tanks and will have a capacity of 400,000 bus.

## WISCONSIN

Elmwood, Wis.—The Farmers Grain Co. will build a 15,000-bu. vitrified tile elvtr. J. H. Brown Co. has the contract.

Fond du Lac, Wis.—The Fond du Lac Equity Co. incorporated; capital stock \$6,000. Fred W. Fisher is sec'y and mgr. The new company will build an elvtr. and warehouses.

Sharon, Wis.—There will be no elevator built, repaired or remodeled at this point this year. Our elvtr. is 12 years old with bins for bulk feeds. There is only one elvtr. here.—F. G. Dangerfield & Sons.

Ettrock, Wis.—This concern was organized in October and incorporated in February for \$50,000. We finished our elvtr. in January and know of no new enterprises for this town at the present time.—J. V. Hage, mgr., Ettrock Elvtr. Co.

Green Valley, Wis.—The Farmers Elvtr. & Lumber Co., incorporated \$50,000, S. A. Goska, Lewis Klastermann and William Berner, incorporators. They will deal in lumber and farm products.

Green Bay, Wis.—The Badger Grain Co. has increased its capital stock to \$50,000. The Badger Grain Co. purchased the elvtr. of the Cargill Grain Co. several years ago. It is now operating a second elvtr. on Cedar street in this city, also elvtrs. at Mondovi, Bear Creek and Pulaski. The company deals in wholesale grain, seed and feeds. All of the stock is owned in the Debreaux family.

Rubicon, Wis.—An elvtr. that holds grain unnecessarily and in the meantime the market goes down is liable for damages declared the circuit court at Juneau when damages of \$260 were awarded P. Jacobi and J. Schwerbel because the Rubicon Grain & Malting Co. held grain until a loss had been sustained by the owners. The defendants expect to appeal the case because the grain was not moved on account of the car shortage.—M.

Burkhardt, Wis.—Fire on the roof of the flour and feed mill belonging to the mill here caused a loss of only \$150 because the blaze was noticed at once and efforts to get the best of it started without delay. The elvtr. was full to the top, having about 50,000 bus. or \$150,000 worth of grain in it. A defective chimney is the cause given. So many fires have started, as this one did, during the noon hour when the office stove was probably left with dampers wide open, with no one in the office.

## MILWAUKEE LETTER.

LeRoy LaBudde and Miss Thea Otteson were recently married. Mr. LaBudde is with the Franke Grain Co.

An especial com'te has been appointed by the pres. of the Chamber of Commerce to make a thoro investigation as to plans, land and so forth for a new building for the exchange.

The death of Major Thos. E. Balding was announced on the floor of the Chamber of Commerce, Mar. 22. He had been a member since 1866 and was in the old days a member of Chas. Ray & Co.

The following nominees have been named for the coming election: Pres. H. M. Stratton. First vice-pres H. H. Peterson. 2d vice-pres Albert R. Taylor. Sec'y H. A. Plumb. Directors—L. J. Kerfe, Walter Stern, Hugo Stolley, G. W. Kruse, A. R. Templeton. Arbitration, A. L. Johnstone, Linus Beck, J. J. Murphy, F. Foey, Walter Holstein, C. F. Coughlin, William Young, William Zahn, A. E. Bush. Appeals, E. J. Furlong, W. M. Bell, S. G. Courteen.

The Stratton-Ladish Milling Co has increased its capital stock to \$1,500,000 and changed the name to the Ladish Milling Co.

Wm. J. Sullivan, for 10 years a grain inspector on the Board of trade, sailed Mar. 17 for Buenos Aires, where he will be an inspector for the Campo Grain Co. one of the largest grain firms in South America.

## WYOMING

Worland, Wyo.—The Washakie Elvtr. & Mill Co. incorporated; capital stock \$25,000; incorporators J. F. and J. L. Hampton and Thos. F. Carr.

## Cuningham Commission Co.'s Elevator at Little Rock.

Organized in 1890 and surviving the loss of three elevators on the same site in twenty years, the Cuningham Commission Co., now enjoys a large wholesale grain, feed and flour business thruout the south, dealing as far west as El Paso, Tex., and as far east as Jacksonville, Fla.

The present plant at the foot of Ninth Street, Little Rock Ark., comprises a 35,000 bus. elevator, a 450 bbl. corn meal mill, 120 ton sweet feed mixing plant, 100 bbl. flour mill and a 150 ton dry feed mill. The sacking arrangement for chops is 5,000 sacks daily. The location is on the Missouri Pacific and Rock Island lines with three side tracks, giving a daily handling capacity of 20 cars.

The equipment of the plant includes two receiving legs, Sonander receiving scales, automatic hopper scale, seven sacking machines, cleaning machinery, Sprout-Waldron corn meal mill, oat crusher, automatic corn cracker and mixed feed mill. The plant is electrically operated by eight motors of Westinghouse and General Electric make.

The officers of the firm are G. E. Cuningham, Pres.; J. S. Eggleston, Vice-Pres.; John W. Baxter, Sec'y and Treas.

SETH BARRETT, of Frederick, Okla., has had his license as elevator operator revoked by the Wheat Director for failure to appear when summoned to a hearing before D. F. Piazzek, second vice-pres. of the United States Grain Corporation and special agent for the United States Wheat Director at Kansas City.



The Cuningham Commission Co.'s Plant at Little Rock, Ark.



## Feedstuffs

GOSHEN, IND.—The feed business of Marion J. Yoder has been taken over by Charles A. Hostetler.

OIL CAKES and pressed maize are included in the list of articles that Finland has declared no longer free from import restrictions.

ST. PAUL, MINN.—The St. Paul Molasses Feed Co. has been incorporated with a capital stock of \$300,000 by R. J. Johnstone, L. De Bonton, and F. E. Lange.

OGDEN, UTAH.—The Globe Grain & Milling Co. contemplates building an alfalfa mill and a feed yard after its flour mill is completed says Manager J. E. Barnhardt.

MILWAUKEE, WIS.—The C. O. Dickelman Co., Milwaukee, Wis., has registered the words "Poultry Gossip" as its trademark No. 126,112 for use on poultry feed.

COLUMBUS, WIS.—Cull peas are undoubtedly fine feed. We grind them and feed to lambs and cook them to feed to hogs with the best results.—Leonard Seed Co.

LA CROSSE, WIS.—The Gund Brewing Co. has let contract to Sprout, Waldron & Co. for the construction of a feed plant that will have a capacity of 100 tons per day.

NEBRASKA CITY, NEB.—We have moved our general office from Ft. Calhoun, Neb., to this city where we have just completed a mixed feed plant.—Wash-Co. Alfalfa Milling Co.

LOUISVILLE, KY.—The Kentucky Feed & Grain Co. has recently purchased an old tobacco plant in this city for \$60,000. It will convert the buildings into a mixed feed plant.

ST. LOUIS MO.—H. G. Beck, sec'y and mgr. of H. W. Beck Feed Co. recently shot himself. Friends say that he had been dissatisfied because he could not get enough help to operate his business.

KANSAS CITY, MO.—Roberts A. Potts, a member of the Triangle Milling Co., manufacturers of alfalfa and mixed feed, passed away at Allen, Kan., while on a business trip. He leaves a wife and three sons and a daughter.

A CONFERENCE was held recently at Louisville, Ky., between representatives of the State Department of Agriculture and grain and feed dealers for the purpose of having a change made in the standards of feed now in effect in Kentucky.

IRISH FARMERS have been advised by Irish Department of Agricultural and Technical Instruction to insure against a stock feed shortage next year by increasing their acreage devoted to the production of rye, rape, vetch and Italian rye grass.

CEDAR RAPIDS, IA.—Despite the protest of ninety citizens of Cedar Rapids an alley was vacated by the city to make room for the new Penick & Ford plant that has taken over the old Douglas Starch Co. for the manufacture of starch and stock feeds.

### Exports of Feedstuffs.

January export of feedstuffs, compared with January, 1919, and for the seven months ending January were reported by the Bureau of Foreign and Domestic Commerce as follows:

	January—	7 months ending Jan.
	1920	1919.
Bran and mids, tons	48	1,250
Corn oil cake, lbs.	2,350	468,300
Cottonseed cake, lbs.	52,158,796	22,509,710
Cottonseed meal, lbs.	66,984	414,319
Dr. gr. mlt., spts., tons	239	1,286
Linsed cake, lbs.	31,267,168	4,851,135
Lins'd meal, lbs.	264,775	5,445,342
Mill feed, tons	517	683
		8,250
		3,194

CALDWELL, KAN.—John and Ross Smead, operators of the stock yards at this point and who at present have a mill, intend to erect an alfalfa mill with a daily capacity of 40 tons. They will supply food for the stock they now feed as well as supply feed for the general market.

OAKLAND, CITY, IND.—The Columbia Feed & Grain Co. has been incorporated for \$12,000. They will manufacture feed for shipment as well as engage in the grain business. The directors of the new firm are: Edgar Redman, Joseph M. Summers and C. E. Mangrum.

MINNEAPOLIS, MINN.—The Upton Mill & Elevator was recently fined \$50 in the Municipal court of this city for offering for sale a prepared feed that did not agree with analysis printed on the package and which was not registered with or licensed by the state dairy and food department.

THE SEASON for shipping feedingstuffs into the Southeast has been longer this winter than for 2 years past. The crop was short and the long winter held up the farmers in their plowing and seeding, which is just commencing. After plowing the cattle are permitted to graze in the pastures.—S.

MADISON, WIS.—No crop is superior to peas as feed for hogs and sheep, because of their being very high in protein and total nutrients. The vines can also be used for feeding young stock and horses. Peas should be fed in conjunction with other feeds.—University of Wisconsin, College of Agriculture.

TOLEDO, O.—The Larowe Milling Co., which has a 33 acre plant on the Toledo Belt Line, in Rossford a suburb of Toledo has taken over the Larowe Milling Co., a New York corporation. A \$750,000 issue of preferred stock has recently been placed on the market. The proceeds will be used for increased working capital and for additions to the plant.

KANSAS CITY, MO.—Twenty-five members of the Flour & Feed Club discussed rules for the inspection of flour and feed at a meeting held in the Savoy Hotel March 9. Rules for the inspection of bran and shorts are now being drawn up by a number of feed handlers for the trade rules committee. If approved by them will be submitted to the feed and flour men for final adoption.

THE M. C. PETERS MILL CO., of Omaha, Neb., has been given judgment by the Court of Appeals of the District of Columbia against the American Feed Milling Co., of North Carolina, sustaining the Commissioner of Patents in denying registration of a trademark. It was held "Where an application to register the words 'Big Chief,' associated with representation of an Indian with a gun on horseback, was opposed by a concern previously using the word 'Arab,' with a representation of an Arab with a spear on horseback, the horses being similar and the men similar in size, position, and manner of holding weapon, altho their dress was different, and the marks were used on burlap sacks, where the differences would be difficult to detect, the decision of the Commissioner of Patents denying registration was correct."

MEXICO has 700,000 bus. of corn on its farms over local requirements, says a message from Mexico. A cargo of Mexican corn is also reported now in transit to San Francisco to be delivered for \$1.55 per bu.

ENGLAND is purchasing wheat from countries other than the United States, from whom the last purchase was made in January, at \$2.53 per bu. says McCurdy, of the Royal Commission. When asked in Parliament whether wages of farm laborers were not being kept down because the English farmer received less for his wheat than the imported product, Mr. McCurdy would not answer.

### Rice Bran as a Stock Food.

Rice bran is made by exactly the opposite process used in the manufacture of wheat bran where the whole wheat kernel and cove are crushed and then separated. The rice kernels are scoured to remove the outer bran covering, without affecting the heart of the rice.

The rice kernel is covered with seven layers of bran cells. At one end of the kernel is an oil germ or collection. These three outer layers and the oily collection are removed in the process of scouring and milling and are the parts most commonly found in the product commonly sold as rice bran.

Stone-reel bran is composed of those kernels which become broken during the process of removing the rice coverings.

Rice bran found on the market is composed of 26 per cent stone-reel bran, 35 per cent from the first scouring machine and 39 per cent from the second scouring machine. These three parts are blended as they are produced, and the final product is brown in color, mealy in texture, full of invigorating vitamins, and with a fresh sweet odor of bran bread.

During the past two years the main objection found with rice bran by stock feeders and to a lesser extent by manufacturers of mixed feeds has been its tendency to heat and become rancid.

Rice bran is eaten greedily by live stock when fed alone or in any combination. It contains approximately 8 per cent digestible protein, and 65.8 per cent total digestible nutrient, as compared with 12.5 per cent digestible protein and 60.9 per cent total digestible nutrient in wheat bran.

The "Statistics of the Rice Crop for the Years 1918-19" compiled by F. B. Wise, and issued by the Rice Millers' Ass'n, shows that the total supply of rice for milling in the United States during the year ending July 31st, 1919, amounted to 9,087,186 bags of approximately 190 pounds each. In the milling of this rice there was an average mill yield of 9.3 per cent commercial rice bran (15 pounds per barrel of 162 pounds rough rice). Ordinarily this bran is sold in bags of 14 pounds each so that fourteen such bags make a ton. The production of this feedstuff is found to be of considerable importance when we learn that there were 1,123,990 bags produced, packed and sold during the last milling season.

THE TEST of concrete tanks made of 1:2:4 mixture and used for water storage, undertaken by the Bureau of Standards, Department of Commerce, has been discontinued after 150 days because the day-loss curves have taken the form of a straight line, showing that the daily loss has become constant. These tanks, altho showing some actual loss, due to water penetrating the concrete, remained dust dry on the exterior. It would seem from this that altho the head of water amounted to 35 feet, the 1:2:4 concrete is sufficiently waterproof for this purpose.

NOW we are approaching warm weather with over five million barrels of flour in the hands of the Grain Corporation, bought on the guarantee basis. We have exhausted our efforts to sell this flour for cash, at home and abroad. I have told the House Rules Committee that under the obligations imposed on me by the Wheat Guaranty Act, I must use the authority to sell for credit, when sales for cash are not possible. Markets abroad needing this flour are those of nations that are stripped of money, but whose credit, resting on their National character and National resources, will undoubtedly be redeemed in time. In conference, therefore, with the Treasury and State Departments, the Grain Corporation will work out the method of delivering this flour to those needy sections abroad and accepting their obligations for it.—Julius H. Barnes.



## The Grain Man's Fix.

BY SALT LAKE VIEW.

Shipping regulations now are driving men insane. Permits and embargoes give the grain men a pain. To move a pound of freight today it's dynamite you need. Nothing else will move the stuff with any kind of speed. Sherman said that "war was hell," but shipping grain is worse; it makes the mildest tempered man throw down his pen and curse.

Orders keep on coming from distant eager buyers. Who keep pressing messages over the burning wires; "Goods are badly needed" and "Ship by expedite." The grain man bites his fingernails and then begins to fight.

He rushes here and rushes there and swears to beat the band. And wishes he were resting in the good old Promised Land. One grain man from too much work kicked up his heels and croaked. He could not stomach trouble and the beggar simply choked.

Of course, he had his worries, and St. Peter knew it, too. So when this grain man came, he knew just what to do. He thot at first he would send him where the railroad men were frying. As railroad men from lack of work in thousands then were dying.

The roads to Hades were all blocked, the yards, too, were congested. Moving cars of railroad men who died from over-resting. This grain man, he had suffered some, and St. Peter out of pity. Opened wide the Golden Gates and let him in the city.

Poor, weary soul, his rest had come, and with him there on high. Were lots of other grain men who found it paid to die. Why slave on earth, these shippers thought, accumulating money. When shipping regulations they are anything but funny?

If others who are shipping still by railroad and by truck. Could only see these happy chaps, they'd get there quick or buck. Why live on earth and suffer so from shipping regulations. When other friends have cashed their checks and left for lofty stations.

No railroad men are there; they have had their rest below. And those who still are resting will go where sinners go. "War is hell," as Sherman said, and when war was declared. The railroad men all raised their rates, which made the shippers scared.

Every man with grain to ship had this thrown in his face: "If you'll pay and do just what we say, we'll let you have some cars." For that and many other things the railroad men who die. Will never pass the Golden Gate, for they deserve to fry. They had their heaven while on earth, protection and high freights. While the grain men had nothing but Hell and increased rates.

THE EXPORT of rice from Bengal, India, has been restricted because of the crop shortage. Under this rule no country may purchase rice in Bengal if their wants can be supplied in Burma.

## Large Elevator at Montgomery, Ala.

While grain elevators in the Southeastern States are not large as measured by Northwestern standards, the elevator of the H. M. Hobbie Grocery Co., at Montgomery, is one of the largest in Alabama, and is well adapted to the requirements of the trade.

It is well constructed of reinforced concrete and has a capacity of 80,000 bus. A detailed description of its equipment was given in the Journal Mar. 10, page 468. The plant is shown herewith.

## Farmers National Council Given New Life.

The National Council of Farmers Co-operative Ass'ns, formerly headed by John G. Shorthill and somewhat loosely organized between the secretaries of the different farmers state ass'ns, was reorganized at a meeting in Chicago Mar. 16 by the election of H. R. Meisch of Argyle, Minn., as pres.; and Frank Myers of Fort Dodge, Ia., as sec'y.

Radical movements that attempt to upset the present marketing machinery were condemned. Arrangements were made to establish a national railroad claim department.

The new president of the organization is thoroly well posted on all branches of the grain trade, terminal marketing as well as country buying, and can be expected to work together with the regular dealers for the advancement of the interests of the grain growers.

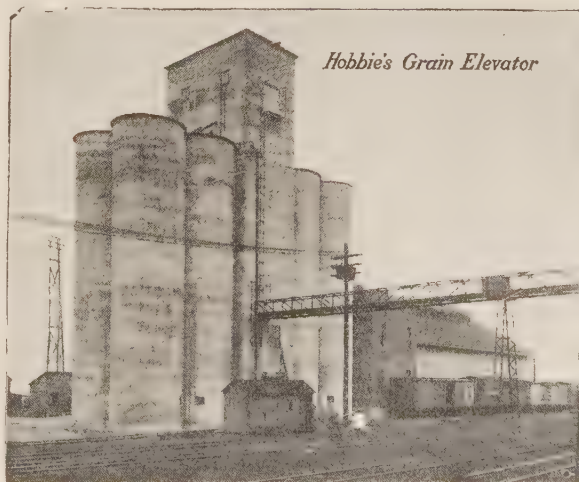
## Tenders of Lower Grades on Contracts.

The Merritt Grain Co., of Chicago, Ill., and the Parker Corn Co., of Kansas City, Mo., are alleged by the Secretary of Agriculture to have represented shipments of corn to be of a higher grade than shown by the official inspection certificates.

As published by the Buro of Markets in Service and Regulatory Announcements No. 60, issued February 21, 1920, the Secretary accuses W. H. and E. L. Merritt of having made four different contracts with the Dayton Milling Co., of Towanda, Pa., for No. 3 corn; with the Lake Shore Elevator Co., of Cleveland, O., for No. 2 yellow corn; with Keister Milling Co., of Huntington, W. Va., for 5 cars of No. 3 yellow corn; and with the same company for 10 cars of No. 3 white corn, and that in the invoices the corn shipped to apply on these respective contracts was designated as "White corn, price \$1.64 3/4"; "Yellow corn, price \$1.85"; "Yellow corn, price \$1.73", and "3 white corn, price \$2.06 1/2." The official certificates on the cars tendered by the Merritt Grain Co. called for No. 4 white, No. 3 yellow, No. 4 yellow, and No. 5 white, in each case one to two grades below that called for by the several contracts.

The Parker Corn Co. is charged with having tendered one car inspected as No. 5 mixed to the Choctaw Sales Co. on the representation that it was No. 4 yellow.

When a company ships thousands of cars of corn in a year and the Buro of Markets is able to show up only one discrepancy in grade the presumption of doubt must be resolved in favor of defendant.



Hobbie's Grain Elevator

Hobbie Grocery Co.'s 80,000-bu. Elevator at Montgomery, Ala.

# ELLIS DRIERS

are always equal to the task. Whether you desire to remove 50 per cent of moisture from salvage grain or one per cent to make a corn grade, it is all in a day's work for an Ellis Drier.

Write for catalog No. 26, which is descriptive of our Portable Driers for small elevators, seed houses and mills.

Grain Driers. Rotary Driers.

## THE ELLIS DRIER COMPANY

332 So. LaSalle Street  
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Established 28 Years  
**H. M. STRAUSS & COMPANY**  
Receivers and Shippers  
**HAY and GRAIN**  
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**Give Your Ad a Chance to  
MAKE GOOD**

Run it in the  
**GRAIN DEALERS JOURNAL**

## Grain Shipping Ledger

**Form 24.** An indexed shipping ledger for keeping a perfect record of the shipments of 5,000 cars. Facing pages are given to each firm to whom you ship and name indexed. The pages are 10 1/2 x 15 1/2 inches, used double.

The book contains 100 double pages. The best linen ledger paper is used. The regular ledger index in front will accommodate all names necessary. The book is tight bound in heavy cloth covers with leather back and corners. Price, \$3.50.

**GRAIN DEALERS JOURNAL**  
305 So. La Salle Street CHICAGO, ILL.



## Seeds

CRAIG, COLO.—A wholesale seed house is being planned for this city by H. B. Kobey.

UNIONDALE, IND.—The Miller & Brickley Grain Co. will build a new seed and ware room this spring.

ST. LOUIS, MO.—C. E. Prunty, who has been ill for some time, has recovered and is now back at work.

EAST LANSING, MICH.—J. W. Nicolson has been named sales representatives of the Michigan Crop Improvement Ass'n, to promote the sale of Michigan grown pure varieties of seeds in other states.

GREAT FALLS, MONT.—The Barkemeyer Grain & Seed Co. which has conducted a wholesale grain and seed business in this city since 1915, has entered the retail field and opened a retail seed store.

ELIZABETH CITY, N. C.—Buxton White, formerly with the North Carolina Agricultural Experiment Station, has entered the seed business at this city with a complete line of field, garden and flower seeds.

POSTVILLE, IA.—Hall Roberts & Son are building a new seed warehouse in this city. They are also rebuilding an old hotel that adjoins their property into another warehouse. The plant when completed will be under the management of I. P. Putman.

GARFIELD, KAN.—The Prather Grain Co. of Larned, Kan., has purchased the elevator of the Gano Grain Co. at this city. They expect to take possession on April 1 and contemplate building a warehouse in which they will handle field seeds, feed and flour.

THE BURO of Crop Estimates announces that in Missouri the acreage being sown to clover is 10% more than last year. The high prices prevailing for seed are holding many farmers from seeding the desired acreage. It is the present intention of the farmers to increase their last year's acreage by 15%.

NAPIER GRASS, brot into this country in 1913 from Africa by the Department of Agriculture, has after extensive experiments been found to thrive in California and certain southern parts of the United States. The Department of Agriculture states that it is a valuable grass and is now distributing samples of this seed with instructions for its care while growing.

ROCHESTER, N. Y.—Three cars of seed corn had been ordered from the Suffolk County Seed Co. One car was delivered to the purchaser, Edw. F. Dibble, with a draft attached to the B/L. After paying the draft and opening the car Mr. Dibble found that the corn was unfit for use and sold it for feeding purposes. The other two cars arrived shortly afterward and they were in the same condition. The company took these last cars back but refused to take back the car of corn that had been paid for. In a decision given in the Supreme Court at this city Mr. Dibble was awarded \$1,566.43 with interest from Aug. 1918.

ALSIKE made a big gain last week. Hung around \$35.00 for a long time. Then somebody wanted it badly, shooting the price to \$36.00 Friday. Most of our reports indicate light stocks all over with prospects of it all coming out this spring. Season's receipts 12,047 bags against 6,459 last year, 10,031 two years ago, 7,868 three years ago, and 9,407 four years ago. Shipments for the same time 10,591 bags against 12,196 last year, 8,504 two years ago, 6,430 three years ago, and 5,852 four years ago. Based on receipts and shipments and carry-over stocks in Toledo are 5,258 bags against 5,893 week ago, none last year, 6,580 two years ago, 5,223 three years ago, and 10,375 four years ago.—J. F. Zahm & Co.

TOLEDO, O.—Few small deliveries have been made on March contracts, but cash has been commanding a premium most of the month, and this has naturally curtailed deliveries on contracts. Offerings have been light. Market advanced easily above \$35.00 level. New York reports imports red clover 2,344 sacks. October clover sold down to a new low level fore part of the week, though the general price range is the highest known for this time of year. Market later rallied sharply on short covering and investment buying, offerings were light. October represents the new crop, with all its uncertainties.—Southworth & Co.

JEFFERSON CITY, MO.—The Missouri State Board of Agriculture announces the following ruling. The percentage of "hard seed," not germinating in the stated number of days as listed under the Missouri "Rules for Seed Testing" under the State Seed Law, should be shown on an extra line under the "germination percentage" line on the seed label. If the seedsman adds to his "germination" total what he desires to guarantee as the portion of the "hard seed" that he is willing to declare will germinate after being planted, he is responsible therefor. The percentage of "hard seed" should be shown on the label or shipping tag.

THE TRYING of grain and its effect on germination as found in experiments made at the English Seed Testing Station are given by R. G. Stapleton and M. Adams in The Journal of the Board of Agriculture, London. In this work instances are given of drying samples of grain at 104 degrees for three days and comparing the results secured with grain that has been held without drying for three weeks. Observations have also been made on the effect of drying on poorly developed and sprouted grain. This work shows that kiln drying and the air drying is best for wheat, barley and rye and air drying for oats, and that the drying of a seed gives a much more representative idea of its commercial value. Results secured are believed to have solved many problems in seed testing and grain conditioning.

FORAGE plant seeds imported during February were as follows: alfalfa 1,588,500 lbs., blue grass 28,500 lbs., brome grass 15,100 lbs., clovers 4,075,400 lbs., millet 77,300 lbs., orchard grass 471,500 lbs., red top 6,000 lbs., rye grass 336,900 lbs., vetch 202,500 lbs., compared with imports during February, 1919, of alfalfa none, blue grass 47,000 lbs., brome grass none, clovers 848,100 lbs., orchard grass none, millet 6,900 lbs., rape 121,000 lbs., red top 100 lbs., rye grass 156,700 lbs., timothy 8,700 lbs., and vetch 109,700 lbs. The total imports of these seeds for the eight months ending February, 1920 were 10,943,800 lbs., blue grass 403,200 lbs., brome grass 141,600 lbs., clovers 23,729,700 lbs., millet 328,500 lbs., orchard grass 2,338,300 lbs., rape 4,861,400 lbs., red top 6,800 lbs., rye grass 2,349,000 lbs., timothy 4,500 lbs., vetch 838,200 lbs. The total imports for the eight months ending February, 1919, were alfalfa 77,000 lbs., blue grass 333,800 lbs., clovers 5,720,900 lbs., millet 118,-

### Imports and Exports of Seeds.

January imports and exports of seeds, compared with January, 1919, and for the seven months ending January, were reported by the Buro of Foreign and Domestic Commerce as follows:

	IMPORTS.			
	— January —		7 months ending Jan.	
	1920.	1919.	1920.	1919.
Flaxseed, bus.	2,186,287	181,480	11,775,993	4,161,888
Castor beans, bus.	115,901	19,264	816,643	140,652
Red clover, lbs.	.....	2,456,287	8,443,028	3,047,221
Other clover, lbs.	2,643,519	784,122	14,814,361	5,092,518
Other grass seeds, lbs.	3,601,416	563,318	16,255,942	2,648,277
Sugar beet seeds, lbs.	1,639,843	1,050	11,086,505	604,320
EXPORTS.				
Flaxseed, bus.	.....	.....	33	.....
Clover seed, lbs.	.....	150	78,248	247
Other gr. sds., lbs.	2,016	.....	11,860	2,356

rye grass 764,500 lbs., timothy 75,000 lbs., 000 lbs., rape 436,800 lbs., red top 4,300 lbs., vetch 566,300 lbs.

### From the Seed Trade.

UNIONDALE, IND.—We will not sell half the grass seed we sold in former years on account of the high price of seed. The acreage sown to grass will only be about half normal.—Miller & Brickley Grain Co.

SPRINGFIELD, O.—Farmers in this section are buying the high priced clover seed to sow on land where the winter wheat was a failure. There has been considerable inquiry for spring barley and seed oats.—W. E. Tuttle Co.

ROCKFORD, ILL.—The demand for clover in this locality has been light. The farmers seem to be shopping and do not seem inclined to buy at the prevailing high prices. They are of the opinion that the prices will not hold up. Oats of good weight are scarce. There is no demand for corn yet as it is too early.—R. H. Shumway.

COLUMBUS, WIS.—I do not think that the high prices will prevent many of the farmers in this section from sowing clover this spring as usual, principally because most of the farmers have the seed that they have raised themselves and when they have it they do not look upon it as costing as much as if they had been obliged to pay the actual cash for it.—E. C.

COLUMBUS, WIS.—We cannot say just how much acreage in this locality is devoted to the production of peas. We believe that it is safe to say that within a radius of 20 miles of Columbus there are more peas raised than in any other section of the state. The canning factories use a large acreage for seed purposes. There are several seed houses operating in this section that contract acreage for peas. There has been a tendency of the farmers of late to decrease the acreage devoted to peas. They have been putting up silos and now raise more corn on account of the high prices paid for milk, pork, etc. We figure that every silo means 10 acres less peas.—Leonard Seed Co.

MADISON, WIS.—We have been doing extensive work with peas, especially at our northern branch stations, and have got out two pure bred varieties, pedigreed Scotch and green which are being disseminated largely thruout Wisconsin. Wisconsin in former years had a great reputation for growing field peas and our peas were so perfect that they found a ready market in New York, Chicago, Philadelphia, and the large cities. These were used then as soup peas. Our farmers later, being enthusiastic about new varieties, soon allowed some of these to creep in, and consequently got their crops mixed so that every farmer was growing seven or eight different varieties of peas in a single field. Peas require a different time for cooking in order to make soups from them, and these mixed strains were unsatisfactory and consequently Wisconsin lost her reputation.

Realizing what was going to happen several years ago we immediately started the breeding of peas. Our pedigreed stock was bred up from single plants taken about ten years ago and were bred especially for high yield and uniformity. These peas are being disseminated widely this year.—R. A. Moore, Agronomist, University of Wisconsin, College of Agri.

WINTER WHEAT may be a "flu" victim. Reports sound sad. Kansas predicts big acreage abandonment. Missouri ditto. Southwest needs rain. Stage is set for very sad April report. Hessian fly reports are numerous and cover large territory. Northwest wheat acreage may be cut due to high cost of farm labor. Missouri crop report shows considerable acreage reduction and condition 68 against 93 last year.—Southworth & Co.



## Supply Trade

CINCINNATI, O.—The third annual convention of the Community Millers Ass'n and an exhibit of milling machinery will be held in this city June 1, 2, 3, 4, at Music Hall.

C. BENTHAM, a director of Henry Simon, Ltd., of Manchester, Eng., will soon visit the United States to call on the trade with regard to grain handling work. His firm is one of the largest in Great Britain manufacturing grain handling and mill machinery.

CHICAGO, ILL.—The Monolith Builders, Inc., have opened an office at Kansas City, Mo., to better take care of their fast growing business in Kansas, Missouri and adjacent territory. They will also have a warehouse at Independence, Mo., for the storing and repairing of steel equipment.

BALTIMORE, MD.—John C. Carter recently purchased the interest of H. Deverell in the firm of Deverell, Spencer & Co., Inc., and will be actively associated with J. H. Spencer in the conduct of the business in the future. The name of the company will shortly be changed to The Spencer Construction Co.

BALTIMORE, MD.—H. Deverell, formerly head of Deverell, Spencer & Co., will represent the Weller Mfg. Co. in the Southeastern States. Mr. Deverell is a competent engineer and is well equipped to take care of any machinery problems with which he may be confronted. His offices will be located in the Lexington Bldg.

OWENSBORO, KY.—The annual meeting of the stockholders of the Anglo-American Mill Co. was held at its offices on January 20. The following officers and directors were elected: L. F. Little, pres.; J. W. McCulloch and L. Reno, vice-pres.; A. Y. Allen, treas., and B. E. Munson, sec'y; L. Reno, W. L. Reno, J. W. McCulloch, R. S. Hughes, C. Rogers, L. F. Little and C. W. Folds, directors.

WASHINGTON, D. C.—A bill to abolish the office of Postmaster General and to provide for creation of a postal commission to direct the business of the Post Office department was introduced recently by Rep. Igoe, of Missouri. The purposes of the bill, as explained by Mr. Igoe, is to put the post office department on the same basis with big business institutions, removing it from politics forever.

MINNEAPOLIS, MINN.—The following Iowa elevators have installed the Benson Combination Auto Truck Dump: Farmers Elevator Co., Rockwell City, Jolly, Glidden (2); Minburn, Ellsworth, Stratford, Cylinder, Whittemore, Rembrandt, Ringsted, Lamont; Farmers Produce Co., Schaller, Webster City Grain Co., Webster City; D. Witmyer, Villisca; W. L. Shimly, Lewis; Kiron Grain Co., Kiron; Thos. Lacy, Spencer, and Henry Lex, Tara.

THE BASIS for success in business is service, and real service must of necessity be based upon genuine interest in the other fellow—upon love, if you please. Every straight business man is a preacher, employing deeds as the chief medium of his expression, rather than words. Some people live for what they can get out of life. Others live for what they can put into it, and these are the salt of the earth.—Carl Hunt in Advertising and Selling.

SILVER CREEK, N. Y.—Catalogue No. 57 of the S. Howes Co. is now ready for distribution. This catalogue contains a vast amount of technical information pertaining to the manufacture of feeds. The grain dealer who also conducts a feed business in connection with his grain business and the feed manufacturer will find this catalogue invaluable to them. Special attention is called to articles on the necessity of balanced rations, the ever increasing demands for feeds and the uniform feeds required by law. A copy of this cata-

logue will be sent Journal readers who write requesting it.

BAGS will soon be sold by the government. Informal bids will be received until Apr. 16 by the surplus property division, Office of the Quartermaster General of the Army, for 44,360 new grain bags, 28,200 used grain bags, 93,550 burlap bags, 812,446 used cotton bags and 134,371 new cotton bags. The bags are listed and described on Clothing and Equipage List No. 7, which shows the quantity of each kind, the location and the minimum number which may be purchased.

BOSTON, MASS.—The open shop was upheld by the entire bench of the Massachusetts Supreme Court on the 22d. The court ordered a permanent injunction to issue against photo engravers' local and international unions, restraining them from interfering with the business of two Boston engraving companies who operate an open shop. Refusal of the companies to comply with a union demand to conduct their shops on a closed basis had resulted in a strike declaration. Large employers of labor will be glad to learn of this partial breaking of the autocracy of the labor unions.

OMAHA, NEB.—The following have installed Trapp Dumping Systems: E. W. Johnston, Pond Creek, Okla.; Morrison Bros. Co., Jefferson, Okla.; Burke Grain & Live Stock Co., Little River, Kan.; W. C. Bailey, Brandon, Neb.; Van Dusen Co., St. Lawrence, S. D.; Exchange Co-Op. Grain & Merc. Co., Hardtner, Kan.; Davidson Grain Co., Macksville, Kan.; Cerro Gordo Grain & Coal Co., Cerro Gordo, Ill.; Erling & Son, Wentworth, S. D.; Independent Farmers Elvtr Co., Sac City, Ia.; A. B. Gillette and Farmers Grain Co., Dell Rapids, S. D.; H. Lee Flour Mills Co., Ellis, Kan.; J. F. Wolf, Sheldon, Ia.; Sac City Canning Co., Storm Lake, Ia.; Cones Grain Co., Enid, Okla.; Rock Mill & Elvtr Co., Hutchinson, Kan.; A. H. Betts, Humboldt and Alexandria, S. D.; Farmers Union Co-Op. Ass'n, Missionhill, S. D.; New Era Mfg. Co., Ark. City, Kan.; Deer Creek Elvtr Co., Blackwell, Okla.; Carson Farmers Elvtr Co., Delft, Minn.; Colman Elvtr Co., Colman, S. D.; Abilene Flour Mills Co., Abilene, Kan.; Woodhull Grain & Elvtr Co., Woodhull, Ill.; Farmers Gr. & Sup. Co., Yewed, Okla.; J. E. Sweedberg and Farmers Elvtr Co., Boxholm, Ia.

### Canadian Milling Industry.

The extent of the flour milling industry in Canada is well set forth in figures compiled by the Canadian Bureau of Statistics for 1918. These show that there are 1,183 mills in the Dominion, divided among the provinces as follows: Alberta 53, British Columbia 5, Manitoba 46, New Brunswick 52, Nova Scotia 37, Ontario 622, Prince Edward Island 27, Quebec 284 and Saskatchewan 57.

Although the prairie provinces now produce 90 per cent. of all the wheat grown in the Dominion still they are not the center of the milling industry. Of the total 1,183 mills, only 161 are located west of the Great Lakes. However, some of the western mills are among the largest units in the Dominion, which makes their grinding capacity larger than that number of mills might indicate. Over 50 per cent of the mills are located in Ontario.

The total capital investment in the mills during 1918 was \$78,144,461 divided as follows: Land, buildings and fixtures, \$25,845,826; machinery and tools, \$14,620,318; materials on hand, stocks in process, etc., \$22,606,266; cash, trading and operating accounts and bills receivable, \$15,072,051. The number of employees is 7,365, the wages and salaries paid being \$7,525,070 for the year.

The value of the materials used during 1918 was \$218,237,250, classified as follows: Wheat \$174,454,518; oats, \$18,738,514; corn, \$3,883,987; barley, \$3,118,845; other and mixed grains, \$15,987,986; materials for containers, \$1,853,000.

## ELLIS OAT PURIFIERS

There are now no restrictions on the purifying of oats other than they must be labeled as such. The demand for them is rapidly increasing and we are convinced they will be more popular than ever before. Purified oats have a better color, are sweeter, they keep better, and feed better than the natural oats. Furthermore, they are purified or sterilized and make a more appetizing and healthful feed.

Write for descriptive matter.

## THE ELLIS DRIER COMPANY

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CHICAGO, U. S. A.

## CIFER CODES

Use a good Telegraph Cipher Code, Prevent Errors, Reduce the Cost of Sending Messages and Prevent Contents Becoming Known to Agents.

**Universal Grain Code**, the most complete and up-to-date code published for the use of the grain dealers and millers. Its use will reduce your tolls one-half. Its 150 pages of bond paper contain 14,910 expressions for present-day terms, and no two of them are near enough alike to cause confusion. Bound in flexible leather. Price, \$3.00; Book paper, board cover, \$1.50.

**Robinson Cipher Code** with 1912 and 1917 Supplements for domestic grain business. Bound in flexible leather, price \$2.25; cloth, \$1.75.

**A. B. C. Improved 5th Edition**, contains a complete set of five letter code words for every expression in the former edition. Any two of these words may be combined and sent as one word, reducing telegraph tolls 50 per cent. Price in English, \$18.00.

**Miller's Code** (1917), for milling and flour trades, 3 1/4 x 6 inches, 77 pages, \$2.00.

**Riverside Code**, fifth edition, for millers and flour dealers. Bound in flexible leather, 228 pages, \$3.00.

**Baltimore Export Cable Code**, third edition, the latest, simplest and most popular code used in the export grain trade. Bound in leather, 412 pages. Price \$12.50.

**Cross Telegraphic Cipher Code**, for provision and grain trade. 145 pages, bound in flexible leather, \$2.00.

**Your Name** in gilt letters stamped on front cover of any of the above books for 35 cents extra. Any code upon short notice.

## GRAIN DEALERS JOURNAL

305 S. La Salle Street, Chicago, Ill.



## Changes in Rates

As shown by tariffs recently filed with the Interstate Commerce Com'n the carriers have made the following changes in rates:

**E. B. Boyd**, agt. Western Freight Tariff Bureau, in Sup. 29 to 1-N gives rules, regulations and exceptions to classifications, effective March 20.

**C. & E. I.** in Sup. 3 to 8625-A gives joint rates on grain and grain products from C. & E. I. stations to points in Ala., Ark., Fla., La., Miss., and Tenn., shown in the tariff, effective Feb. 29.

**C. & A.** in Sup. 5 to 1629-B gives joint and proportional rates on grain products, carloads, minimum weight 30,000 lbs., from stations in Illinois to Cairo and Evansville, Ind., effective Feb. 29.

**C. B. & Q.** in 7072-G gives distance rates on classes and commodities between Ill., C. B. & Q. stations, west bank Miss. River stations on the C. B. & Q. and Illinois stations on the C. B. & Q., effective Feb. 29.

**C. & A.** in Sup. 3 to 2-F issues a Sup. to freight tariff showing switching and other terminal charges, also rules governing absorption of switching, drayage and transfer charges applying at C. & A. stations, effective Feb. 29.

**A. T. & S. F.** in Sup. 4 to 7481-F gives joint freight tariff on grain products and seeds from stations in Colo., Kan., Mo., and Okla., also Superior, Neb., on A. T. & S. F. L. & T., and the K. S. to points in Ala., Ark., Kan., La., Mo., Miss., Okla., Tenn., and Tex., effective Feb. 29.

**A. T. & S. F.** in Sup. 3 to 5655-Y gives joint and proportional rates on grain, grain products and seeds from points in Mo., Kan., and Okla.; also Superior, Neb., to Galveston, Houston, Ft. Bolivar, Texas City, Beaumont, Orange and Pt. Arthur, Tex., when for export, effective Feb. 29.

**A. T. & S. F.** in Sup. 5 to 5588-K gives local, joint and proportional rates on grain and grain products, etc., between points in Kan., Colo., Mo., and Okla., also Superior, Neb., and Joplin, Mo., and points in Ill., Ind., Ia., Kan., Mich., Minn., Mo., Neb., Okla., and Wis., effective Feb. 28.

**L. Lowry** in Sup. 30 to 20-J gives local and joint terminal charges, rules and regulations from or to points in the Chicago district on outbound and inbound freight traffic, also rules governing intermediate service on freight traffic passing thru the Chicago district, effective Feb. 29.

**C. & A.** in Sup. 6 to 1581-F gives joint and proportional rates on grain and grain products also by-products, beet pulp, dextrine and starch from C. & A. stations in Illinois and Hannibal and Louisiana, Mo., to Atlantic seaboard and eastern interior United States and Canadian points, effective Feb. 29.

**C. B. & Q.** in 1346-F gives local, joint and proportional rates between Chicago, Peoria, Quincy, E. St. Louis, Ill., St. Louis, Mo., etc., and stations on the C. B. & Q. and R. I. S. in Ill., and C. B. & Q. Miss. River points, Dubuque, Ia., and south thereof, also stations on the M. B. & S., effective Feb. 24.

**C. B. & Q.** in Sup. 2 to 6786-E gives local, joint and proportional rates on grain and grain products from C. B. & Q. stations and C. A. & D. K. stations, also Miss. River points, Dubuque, Ia., to Louisiana, Mo., incl., to Cairo, Metropolis, Mounds, Ill., Evansville, Ind., Cincinnati, O., Paducah and Louisville, Ky., etc., effective Feb. 29.

**C. B. & Q.**, in G. F. O. 1800-C, gives local joint and proportional rates on grain, grain products, broom corn and seeds between C. B. & Q. stations west of the Mo. River also R. C., B. H. & W. stations and Chicago, Peoria, Ill.; St. Louis, Mo., St. Paul, Duluth, Minn., and stations taking the same rates or arbitraries higher effective Feb. 24.

**C. B. & Q.** and Q. O. & K. C. in Sup. 40 to G. F. O. 3457-G and circular 112-H gives rules and regulations affecting freight traffic including reconsigning rules, restrictions as to acceptance of freight and freight requiring prepayment or guarantee, mileage payments and rental charges on private cars and refrigeration, etc., effective Feb. 29.

**C. B. & Q.** in 3200-D gives local, joint and proportional rates on grain, grain products and seeds between Mo. River points and Chicago, Peoria, Ill., St. Louis, Mo., St. Paul, Minneapolis, Duluth, Winona, Minn., and La Crosse, Wis., and stations taking same rates or arbitraries higher, effective Feb. 29.

**C. & A.** in Sup. 6 to 1609-D gives local, joint and proportional rates on grain, grain products, seeds, hay and straw and articles taking the same rates between Chicago, Peoria, Ill., and St. Louis, Mo., and points taking the same rates, also Rondout, Waukegan and stations in Mo. on the C. & A., effective Feb. 29.

**C. R. I. & P.** in Sup. 10 to 19687-K gives joint and proportional rates on grain, grain products, seeds, hay, and broom corn from Missouri River stations and other stations in Ill., Ia., Minn., Mo., and So. Dak., on the C. R. I. & P., to Miss. Valley points and other stations in Ala., Ark., Fla., La., Miss., and Tenn., effective Feb. 28.

**C. & A.** in Sup. 13 to 1604-E gives local, joint and proportional rates on grain and grain products between Chicago, Joliet, Peoria, Pekin and E. St. Louis, Ill., also St. Louis, Mo., and stations on the C. & A., C. & I. M., and C. & N. W. in Ill., and St. Louis, Mo., also to Detroit, Mich., and Toledo, O., and stations on connecting lines in Ill., effective Feb. 29.

**C. R. I. & P.** in Sup. 11 to 10389-E gives local, joint and proportional rates on grain, grain products and seeds between St. Louis, Mo., E. St. Louis, Alton, Quincy, Ill., Hannibal, Mo., and stations in Ill., Minn., Mo., and S. Dak., to Cairo, Thebes, Ill., Evansville, Ind., and Louisville, Ky., when destined S. E. and Carolina territories, effective Feb. 28.

**C. & A.** in Sup. 12 to 1604-E gives local, joint and proportional rates on grain and grain products between Chicago, Joliet, Peoria, Pekin, and East St. Louis, Ill., also St. Louis, Mo., and stations on the C. & A., C. & I. M., and C. & N. W. in Ill., and St. Louis, Mo., also to Toledo, O., and Detroit, Mich., and stations on connecting lines in Ill., effective Feb. 29.

**C. & A.** in Sup. 6 to 1596-E gives joint and proportional rates on grain and grain products from stations on the C. & A., C. & I. M. and C. N. W., also from Louisiana, Mo., to Memphis, Tenn., New Orleans, La., Helena, Ark., and points in Ala., Ark., Fla., La., Miss., and Tenn., also to Key West, Fla., Mobile, Ala., New Orleans, La., Pensacola, Fla., when for export, effective Feb. 29.

**C. R. I. & P.** in Sup. 13 to 29329-C gives local joint and proportional rates on grain, grain products and seed between Albright, Omaha, So. Omaha, Neb., Armourdale, Atchison, Leavenworth, Kan., Council Bluffs, Ia., Kansas City, St. Joseph, Sugar Creek, Mo., stations in Ia., Kan., Mo., and Neb., on the C. R. I. & P., C. B. & Q. and the M. P. and stations in Ill., Ind., Ia., Minn., Mo., So. Dak., and Wis., effective Feb. 29.

**C. B. & Q.** in Sup. 2 to 1362-K gives local, joint and proportional rates on grain, grain products and seeds between St. Paul, Minneapolis, Duluth, Stillwater, Winona, Minn., and La Crosse, Wis., etc., and stations in Ill., Ind., Ky., west bank Miss. River points (Dubuque, Ia., to St. Louis, Mo., incl.); also Green Bay, Kewaunee, Wis., also to points east of the Ill.-Ind. state line or south of the Ohio River, as shown in the tariff, effective Feb. 29.

**C. R. I. & P.** in Sup. 10 to 10389-E gives local, joint and proportional rates on grain, grain products and seeds, between St. Louis, Hannibal, Mo.; E. St. Louis, Alton, Quincy, Ill., and stations in Ill., Ia., Minn., Mo., and S. Dak., also Armourdale, Atchison, and Leavenworth, Kan., also on grain and grain products, carloads from stations in Ia., Minn., and S. Dak., to Cairo, Thebes, Ill., Evansville, Ind., and Louisville, Ky., when destined Southeastern and Carolina territories, effective Feb. 29.

**W. J. Kelly**, agt., Western Freight Tariff Bureau, in Sup. 5 to 245, gives local, joint and proportional rates on grain, grain products and by-products of grain from points in Ill., Ind., Ia., Ky., Mich., Mo., N. Y., O., Pa., W. Va., and Wis., to Albany, Baltimore, Boston, Montreal, New York, Philadelphia, Rochester, Syracuse, Utica, also to points taking same rates or arbitraries higher, as per eastbound basing tariffs and eastbound billing instruction and guide books referred to on pages 78-81 of tariff and as provided on pages 69-77 of the amended tariff, also bases for rates to Baltimore, New York, Philadelphia, etc., on export traffic, effective Feb. 29.

# A Toll Saver

Here is a letter from a firm you all know, based on its experience over a period of five years with the Universal Grain Code.

POPE & ECKHARDT CO.

Grain  
Commission Merchants  
West. Un. Bldg.

Chicago, June 2, 1915.

Grain Dealers Journal,  
Chicago, Ill.

Gentlemen:—

Since its publication we have made use of the Universal Grain Code continually in communicating with our customers, and we cheerfully subscribe to the fact that it is, in our opinion, a high class medium for the use of the Grain Trade and other lines incident to it. We find it not only useful, but that it is also a very efficient medium in saving telegraphic tolls. Most of our customers agree with us in the opinion we express herein covering the utility and usefulness of the book. Naturally, it requires a little study to become familiar with and to be able to make use of it to best advantage, but we cheerfully recommend it as a reliable and useful code for the purpose for which it is intended.

Yours truly,  
POPE & ECKHARDT CO.  
WNE:S

This is only one letter. Dealers everywhere who have used it pronounce the "Universal" to be far superior to any code ever published for the Grain and Milling Industries.

Its 146 pages of policy bond paper contain 13,745 expressions for modern, present day, trade terms, every one of which means a saving in tolls to you.

Follow the example of Pope & Eckhardt Co. Stop the leaks in YOUR profit account by sending us your order today. Price \$3.00.

Grain Dealers Journal  
315 So. La Salle St. Chicago, Ill.



## Supreme Court Decisions

**Written Contract Not Modified by Parol Negotiations.**—A written contract cannot be modified by the oral negotiations leading up to its execution or by subsequent or contemporaneous oral agreement in contradiction of its terms.—*Union Oil Co. v. Pacific Surety Co.* Supreme Court of California. 187 Pac. 14.

**Service on Carrier Under Federal Control.**—Under General Orders No. 50 and No. 50a, requiring service of process to be made upon operating officials operating for the Director General the carrier in respect of which the cause of action arises, service upon a railroad's agent having no connection with the railroad in respect of which the cause of action arose is insufficient and must be set aside.—*Harmon v. Hines.* Supreme Court of South Carolina. 101 S. E. 925.

**Contract Not Affected by Rejected Confirmation.**—Plaintiff's letter to defendants stating plaintiff's understanding of their oral contract for sale of corn has no bearing on and does not modify the original contract, defendants immediately stating that they would not accept that statement of the contract, and plaintiff replying that they need not, and defendants then shipping corn of the kind they claim was originally stipulated.—*Parker Corn Co. v. Sexton.* Kansas City Court of Appeals, Missouri. 217 S. W. 616.

**Partial Delivery Makes Contract Valid.**—In an action for damages for failure to deliver one of two carloads of wheat purchased, a petition alleging that defendant delivered "one carload of said grain within the time, under the terms, for the consideration, and at the place specified in said contract, confirmation, and agreement," was sufficient upon which to base an inference of acceptance and receipt, taking the contract out of the statute of frauds, particularly in view of the time of the attack on the petition, and of

waiver of demurrer by answer.—*Fox-Miller Grain Co. v. Stephans.* Kansas City Court of Appeals, Missouri. 217 S. W. 994.

**Valid Contract for Future Delivery.**—A valid contract of sale may be made for future deliveries of grain, even though seller has no grain on hand and will have to provide himself with the requisite quantity and quality before time of delivery; such contracts being illegal only where there is no intention to procure and deliver, or receive and pay for the grain; the contract under such circumstances being a mere wager. Under Rev. St. 1909, § 4780, option contracts for future delivery of grain are valid, unless made by parties who did not intend to actually receive and pay for or to deliver the grain.—*Yontz v. McVean.* Kansas City Court of Appeals, Missouri. 217 S. W. 1000.

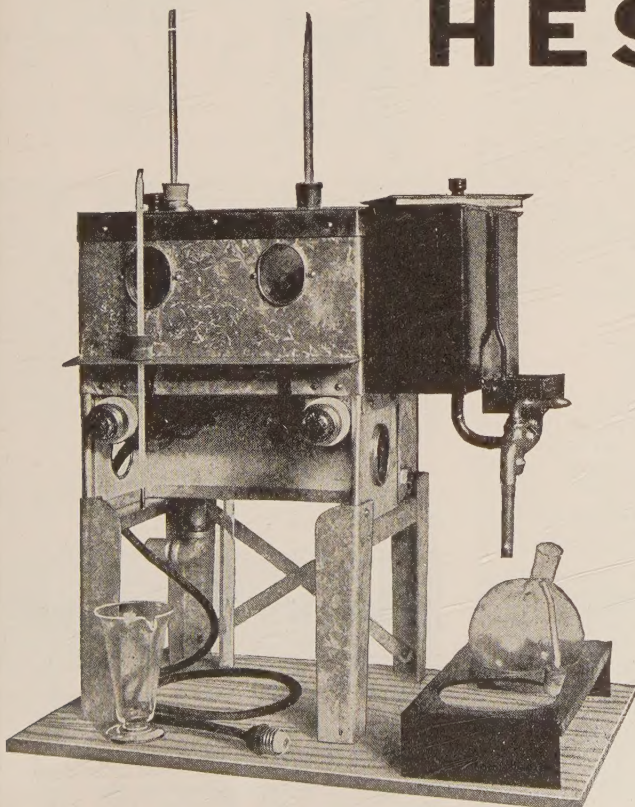
**Title Under B/L.**—A B/L is a muniment of title quasi negotiable, and at common law transferable so as to pass title to the goods in transit, when such is the intention of the parties, as effectually as if the goods themselves had been delivered. Where consignor draws on consignee for the purchase money, and the draft with the B/L attached is indorsed or transferred to a bank which discounts the draft and places the proceeds to the credit of the consignor, a special property in the goods passes to the bank, subject to be divested by acceptance and payment of the draft, and superior to the rights of attaching creditors of the consignor.—*Frontier Natl. Bank of Eastport v. Salinger.* Appellate Court of Indiana. 126 N. E. 40.

**Technical Terms in Contracts.**—Words in a contract may be shown by parol to have a technical or trade meaning, provided it be shown such meaning is not local, but is generally known to and recognized by the trade; and this though the contract would be intelligible if the words were given their customary meaning, particularly when all the parties are connected with the trade, and so presumed to know the technical meaning in the trade. One dealing in a business or trade must be conclusively presumed to know and understand the language thereof, shown to be generally used and known therein; and his ignorance will not protect him, unless shown to be known by the party dealing with him.—*Gile v. Tsutakawa.* Supreme Court of Washington. 187 Pac. 323.

**North Dakota Grain Elevator Statute.**—It is held that the building, owning, and operating of state-owned elevators, flouring mills, and other state industries in question is for a public purpose as defined in paragraph 6. The building, owning, and operating of such state-owned industries and utilities being for a public purpose, and the profits from such inuring to all the people of the state, and payable as such into the state treasury for the equal use and benefit of all the people of this state, it is held that such ownership and operation of such industries and utilities constitute a public purpose, to carry out which the state may issue bonds as prescribed by law and may levy a tax on all property of this state not exempt from taxation under the laws of this state or of the United States, for the purpose of paying the principal and interest of such bonds; and that in levying and collecting of such tax there is no violation of either the state Constitution or the Constitution of the United States nor the Fourteenth Amendment.—*Green v. Frazier.* Supreme Court of North Dakota. 176 N. W. 11.

**Conditions on Telegram Blank Valid.**—The act of Congress of June 18, 1910 (U. S. Comp. St. § 8563), declaring telegraph companies to be common carriers and subject to federal statutes regulating interstate commerce, and authorizing them to make a reasonable and just classification of messages transmitted by them, into day, night, repeated, unrepeated, letter, commercial, press, government, and to charge different rates for the different classes of messages, warrants such company in inserting, as a condition of its contract with the sender of a message, that, in no event shall it be liable for damages "for any mistakes or delays in the transmission or delivery, or for the nondelivery, of this telegram, whether caused by the negligence of its servants or otherwise, beyond the sum of fifty dollars, at which amount this telegram is hereby valued, unless a greater value is stated in writing hereon at the time the telegram is offered to the company for transmission, and an additional sum paid or agreed to be paid based on such value equal to one-tenth of one per cent. thereof." Such a condition is a reasonable regulation within the purview of the federal statute.—*Dunham v. W. U. Tel Co.* Supreme Court of Appeals of West Virginia. 102 S. E. 123.

# HESS DRIERS



The Brown-Duvel Moisture Tester (official) for electricity

Insure Safety in Storage and in transit, of all kinds of grain or seed. They raise the grade by reducing the moisture content. Dispel foul and sour odors, mustiness and mold.

They are STANDARD—are used everywhere and embody all that is desirable in grain drying apparatus. Your inquiry is requested.

## Brown-Duvel Moisture Testers

all sizes, for gas, gasoline, alcohol and electricity—glass or copper flasks. Conform strictly to government requirements. Also scales, dockage sieves, bucket testers and all other grain sampling and testing apparatus. Free booklet—illustrated.

**Hess Warming & Ventilating Co.**  
907 Tacoma Bldg., Chicago, Ill.



## Grain Carriers

THE COMMUNITY Millers Ass'n will hold its third annual convention at Cincinnati, O., June 1 to 4.

ABOUT one fifteenth of the freight cars on government controlled roads were unfit for service on March 1, says a report of the Ass'n of Railway Executives.

POSTPONEMENT of the expiration of minimum grain weight tariffs from Mar. 31, 1920, to Aug. 31, 1920, was authorized Mar. 17 by the Interstate Commerce Commission.

SUIT has been started by the Early & Daniel Co., Cincinnati, O., against former Director General Hines for \$2,418 because a shipment of wheat was delayed and deteriorated in transit.

UPLAND, NEB.—The Farmers Union Elevator at this point is full of corn with no cars in sight. They say that they have had but 10 cars since Jan. 1 and could have used 60 cars.

THE CAR service commission has recommended the re-establishment of machinery for the exchange of embargo information such as was in effect before the railroads went under federal control.

THE GRAIN Corporation investigation committee of Congress was named Mar. 20, with Senator LaFollette of Wisconsin as chairman. The other members of the committee are Senators Fernald, Gronna, Reed and Walsh.

THE SOUTHERN Railway thinks that the prosperity of the south will continue indefinitely. It will rely on its own earnings instead of accepting the return of 5½ guaranteed under the new transportation act.

A SCHOONER carrying 5,000 bus. of corn recently sunk after having collided with ice in the river near Annapolis, Md. The ship was raised and attempts are now being made to save the corn by drying. Dudley & Carpenter, Baltimore, were the consignees.

THE INTERSTATE COMMERCE Commission, Division No. 2, has ordered that carriers file with the commission a supplement to their tariffs explaining that the minimum weights on grain and grain products that were to be canceled on April 1 will be canceled instead on Sept. 1.

EXAMINER BUTLER has recommended to the Interstate Commerce Commission that the Northern Grain & Warehouse Co. be granted reparation from the Northern Pacific on a car of oats to the extent that the rate of 76 cents exceeded 61 cents from South Dakota points to Helix and Portland, Ore.

THE TAX on export freight would be done away with under a bill, S4027, introduced by Senator McCumber abrogating the regulations of the commissioner of internal revenue levying tax on export freight stopped for any commercial purpose. Declaration of intention to export and filing of export B/L shall be sufficient proof for exemption.

As a preliminary towards de-control, shippers and exporters have been advised by the Canadian Wheat Board that they can make their own arrangements with individual vessel agents for lake tonnage. The chartering committee will be dissolved and chartering agents will be allowed to charter to individual firms without reference to the Board.—B.

THE WARRIOR RIVER, a navigable waterway that connects Birmingham and Mobile, will have two new \$500,000 barges that have recently been launched in St. Louis. Each barge is 280 feet long, 50 feet wide and will draw 7 feet of water. In addition to a bulk coal carrying capacity of 1,600 tons there are accommodations for 400 tons of package freight.

THE SHIPPING board has taken under consideration the proposed promulgation of regulations requiring "interstate water carriers" to file complete schedules of their maximum rates. The purpose is to make the rates immediately available to shippers and to prevent discrimination or rebates. The regulations, however, would not apply to carriers operating between American and foreign ports.

THE FARMERS GRAIN & SUPPLY Co. at Hildreth, Neb., protested to the Nebraska Railway Commission that the C. B. & Q. is not furnishing cars for grain shipments. They say that the nearby towns of Kane, Franklin and Bloomington have been receiving cars and that for that reason 50,000 bushels of grain has been sold by the farmers to dealers in these towns which should have been brought to Hildreth.

"No. 2 West of the Mississippi River Rates" is a carefully compiled volume containing rates and minimum weights on grain from Iowa, Minnesota, Missouri, North Dakota and South Dakota to Chicago. This publication contains the readjustments of rates up to Feb. 28, 1920, and supersedes "No. 1 West of the Mississippi River Rates." Compiled by the Chicago Board of Trade, Transportation Department. Price, \$5.

AT THE HEARING Mar. 23 and 24 at Washington on the valuation of the railroads for the payment of the guaranteed return the state railroad commissioners opposed giving much weight to the investment accounts of the carriers. John E. Benton, general solicitor for the Ass'n of State Railroad Commissioners, declared Congress has not given the Interstate Commerce Commission discretion to be governed by other than the true cost.

SOME COURTS and some attorneys are rather slow to inform themselves of the latest decisions controlling. This is true in Bowman-Kranz Lumber Co. v. Bush, receiver of the Missouri Pacific, where the Supreme Court of Nebraska on Jan. 31, 1920, cut down shipper's recovery from \$162.94, destination value, to \$90.90, invoice value, for conversion of a car of coal, totally oblivious of the decision of the U. S. Circuit Court of Appeals in the McCaull-Dinsmore case, allowing destination value.

THE FOUR MONTHS limit in which to file claim for loss on shipments made during 1914 and 1915 was not unreasonable even though the limit later was extended to six months, says the Interstate Commerce Commission in the claim of C. P. Blackburn vs. The Ann Arbor R. R. "We are of opinion and find that the four months' limitation upon the filing of claims appearing in the uniform B/L in effect at the time the shipments specified in the complaint moved, has not been shown to have been unreasonable as applied to the claims listed by complainants."

### Insurance Notes.

THE CHICAGO Local Agents Club is considering action to discontinue the Underwriters Grain Ass'n, now that the war emergency no longer exists. It is suggested that the Ass'n be abolished when the year ends in August, 1920, as the Ass'n is inimical to the interests of local agents and might spread to other industries.

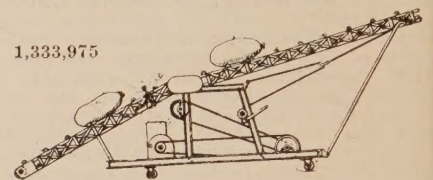
GEORGE H. RICE, assistant engineer grain dust explosions, U. S. Bureau of Chemistry, Washington, D. C., will soon leave the bureau and affiliate himself with the Hartford Fire Insurance Co., Chicago, Ill., as an inspector in the mill and elevator dept. Mr. Rice while with the federal government made his headquarters in Chicago, Ill., and traveled the middle central west investigating dust explosions and fire losses in mills and elevators jointly for the Bureau and the U. S. Grain Corporation of New York City. Mr. Rice will travel the States of Iowa and Nebraska for the Hartford.

## Patents Granted

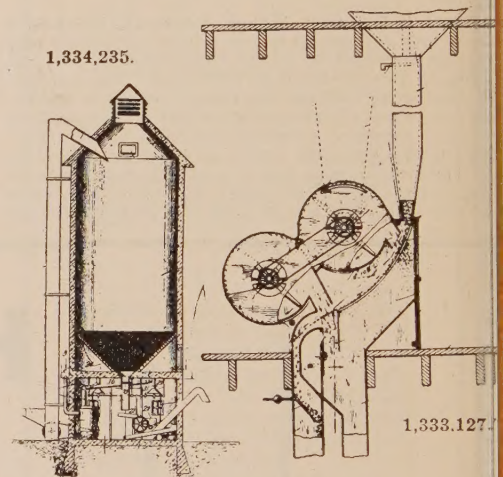
1,334,235. Means for Handling Ear Corn. W. G. Eckhardt, De Kalb, Ill. This is an ear corn handling apparatus into which the corn is fed at the top, then dried by a current of air forced thru the apparatus from the bottom. At the bottom the corn is directed into a sheller from where the shelled corn and by-products are delivered. This apparatus produces its own heat and power by the combustion of the cobs and by-products received from shelling the corn.

1,333,127. Grain Separator. Chas. P. Nall, Minneapolis, Minn. This separator has a casing which incloses an inclined screen. A hopper is at the top of this casing from where the grain is directed thru a delivery tube to the screen. A fan delivers a current of air thru the screen. At the end of the screen is a gap thru which the current of air passes. The impurities in the grain pass down with the current of air and the cleaned grain passes over the gap, down a spout and into a receptacle therefor.

1,333,975



1,334,235.



1,333,975. Counter Attachment for Conveyors. Henry Harrington, Waukegan, Ill. This is an attachment to be used with conveyors or elevator systems. It comprises a pair of vertical supports adapted to be placed against the sides of a conveyor, shafts rotatable in the supports, arms on the upper ends of the shafts, cranks on the lower ends of the shafts, links operatively connecting the cranks, a counter and an arm on one of the shafts for operating the counter, the parts being so constructed that the counter will be actuated by an article traveling along the conveyor regardless of its position transversely of the conveyor.

THE MILLERS NATIONAL FEDERATION will hold its annual meeting at the Blackstone Hotel, Chicago, on April 9.

## Books Received

WEEDS POISONOUS TO LIVESTOCK, identifies weeds poisonous to livestock, gives method of handling to prevent stock from eating these weeds and explains the symptoms and treatment of animals poisoned therefrom. The writer of this book estimates that more livestock is lost from poisoning than from every other source. Paper, 40 pages. By Dr. P. S. Hooper and J. C. Hooper. Published by Department of Agriculture, Edmonton, Alta.



## Grain Scale Book

Form 23 is an Indexed Journal, keeps a record of 10,000 wagon loads. Each man's loads are entered on his page. It keeps a record of scale weights. From it both debits and credits are posted to ledger, crediting the customer with the amount received and charging it to the grain's account.

The book is ruled with column headings as follows: Date; L. F.; L. F.; Kind of Grain; Remarks; Gross; Tare; Net; Bushels; Pounds; Price; Amount.

The book contains 240 pages, size 10½x15½ inches, of Atlas linen ledger paper. A 28-page index in front. Extra heavy cloth covers with leather back. Price, \$3.50

### GRAIN DEALERS JOURNAL

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**Tell the Grain Dealers—  
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If you have spent your good money without satisfactory results, it's a case of wrong article or wrong advertising.

The easiest, most direct, most popular and most effective way to the grain dealer is The Grain Dealers Journal route.

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## Only One Way

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## THE RECORDS

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## Flour Mill and Grain Elevator Mutual Insurance Companies

show that 80% of all fires in Mills and Elevators start in elevators. The installation of an automatic feeding device which will positively prevent an elevator choking from over-feeding and an automatic relief which will prevent an elevator choking from bin getting full or spout choking, will annually save millions of dollars in fire loss and add at least 50% to efficiency of any stand of grain elevators. These devices are perfected and are available for mill and elevator owners, are very inexpensive and absolutely practical. Full particulars will be furnished, free of charge, by this office, on request.

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INCORPORATED 1877

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Insurance in Force \$48,050,335.36 Cash Surplus \$655,363.13

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HOME OFFICE: Alton, Illinois

SOUTHWESTERN AGENCY: Railway Exchange, KANSAS CITY, MO.

## Insurance Worries

Would you like to be relieved of the worry of watching your grain insurance every day to see that you are properly covered? Let us tell you about our Premium Adjustment Policy. You are charged only for actual values contained in your elevator. Premium adjusted with you once a year. Simple; saves time and worry. Write for information.

C. R. McCotter

Western Manager  
Omaha, Neb.



C. A. McCotter

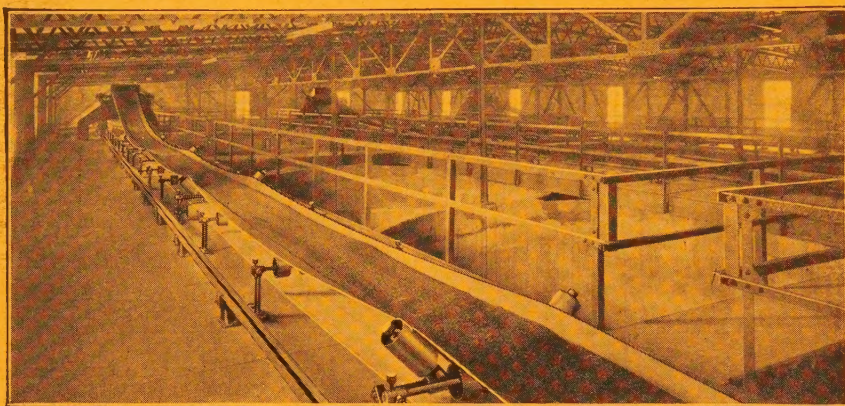
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Save Time by Speeding Up the Work  
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### By Installing Weller Grain Handling Equipment

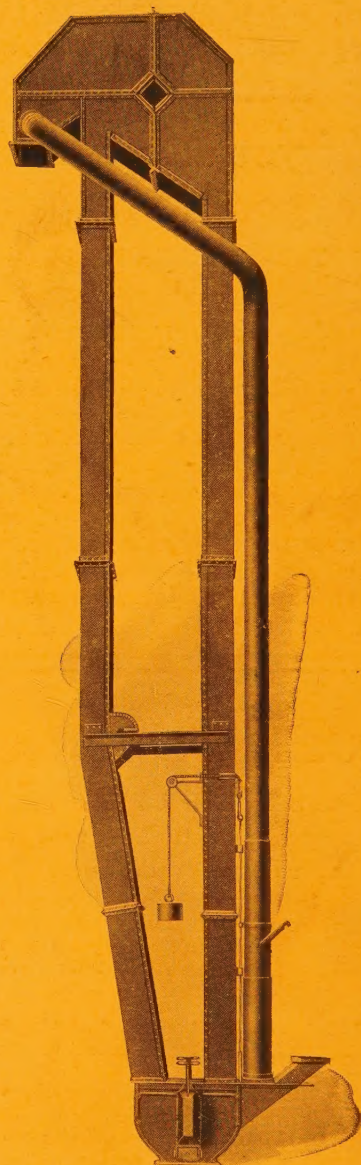
For more than thirty years the Weller Manufacturing Co. have specialized in the design and manufacture of machinery adapted to the handling of grain. During that time we have equipped some of the largest elevators in the world and there are very few of the larger elevators that do not have some Weller Equipment.

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BULLETIN No. B115

*"What if Your Elevator Should Burn Today?"  
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